

Revision 7

July 2025

C: Standard Drawings

We acknowledge the Gadigal of the Eora Nation
as the Traditional Custodians of our local area.

CITY OF SYDNEY



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C: Standard Drawings

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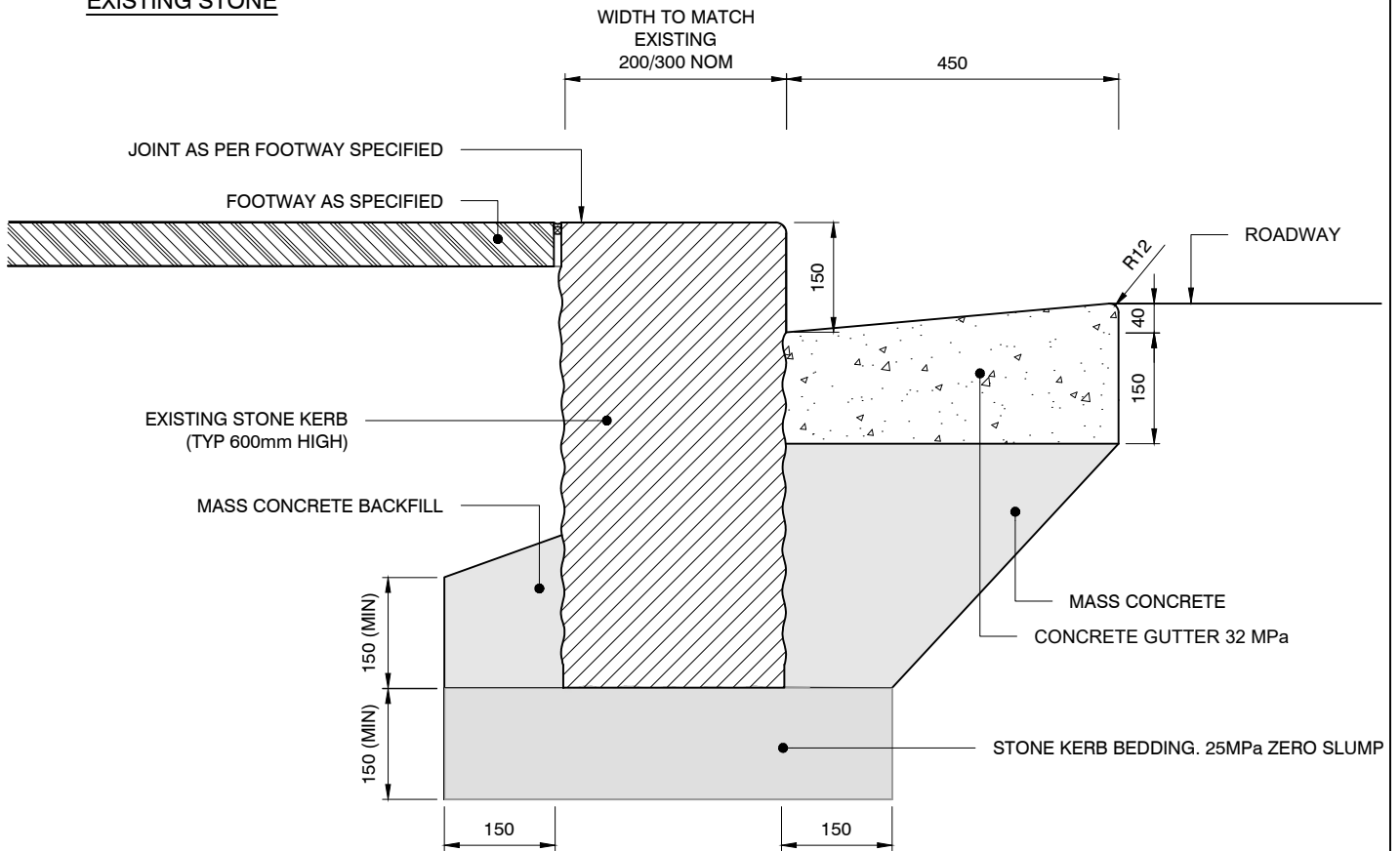
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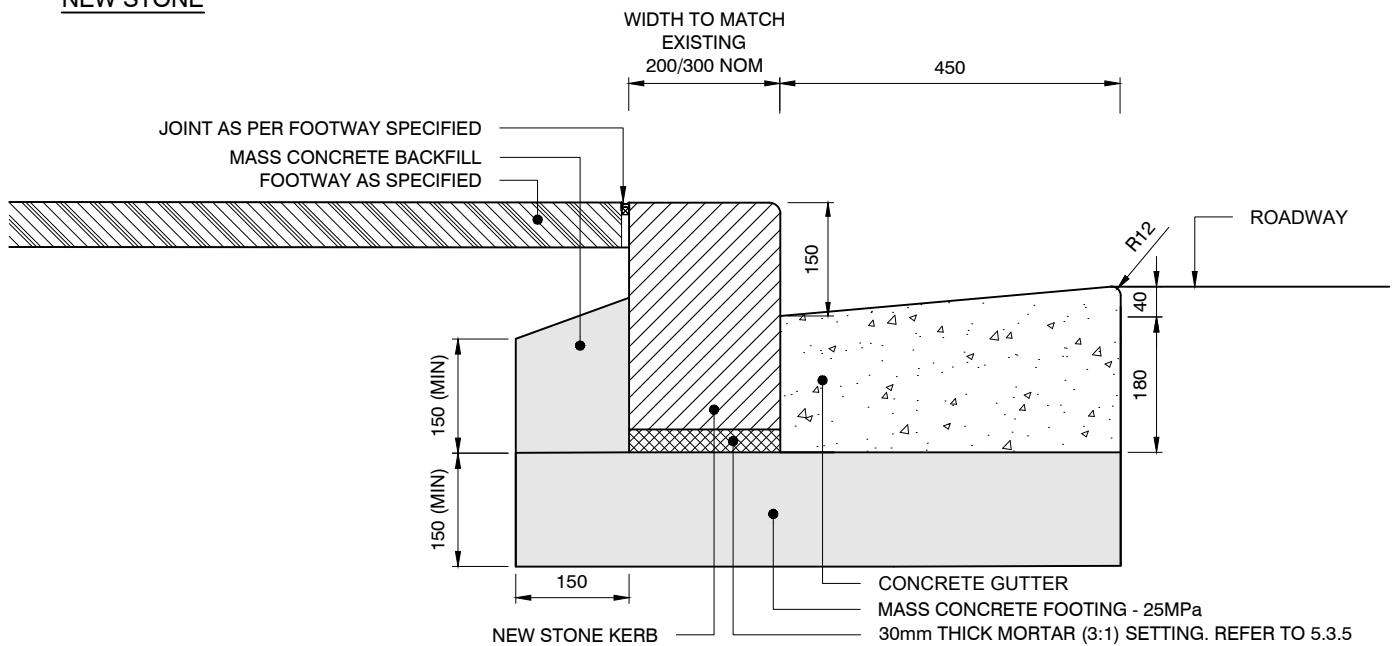
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EXISTING STONE



NEW STONE

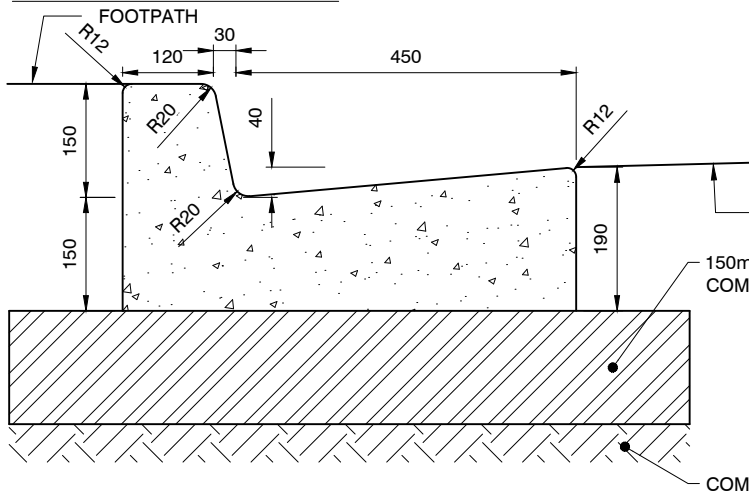


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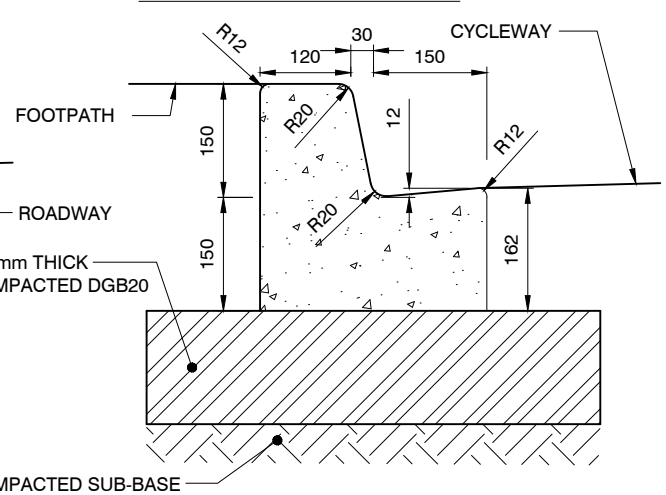
- JOINTS IN CONCRETE GUTTERS:
 - CONTRACTION JOINT: WIDTH 5 mm, 20 mm DEPTH, AT 3 m INTERVALS
 - EXPANSION JOINT: WIDTH 15 mm, FULL DEPTH OF GUTTER, AT 15 m INTERVALS
- FOR ROAD RESTORATION ADJACENT TO KERB REFER TO STD DRG # 1.1.16
- WHERE THE GUTTER IS SUBJECTED TO HIGHLY REPETITIVE MEDIUM AND HEAVY TRAFFIC, THE GUTTER SHALL BE 200mm THICK IN ACCORDANCE WITH B4 - 4.3.3
- ALL CONCRETE SHALL BE MIN 32 MPa UNLESS NOTED OTHERWISE.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:10

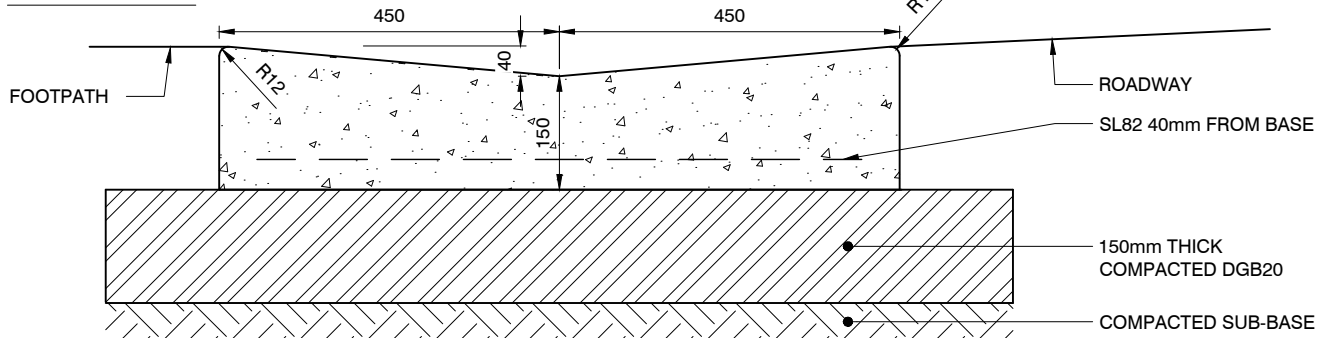
BARRIER KERB - ROADWAYS



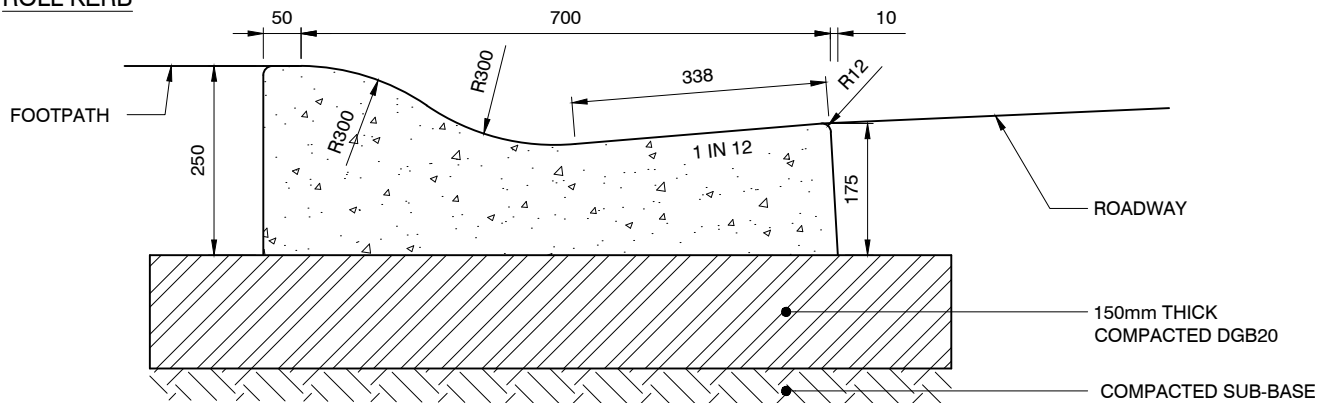
BARRIER KERB - CYCLEWAY



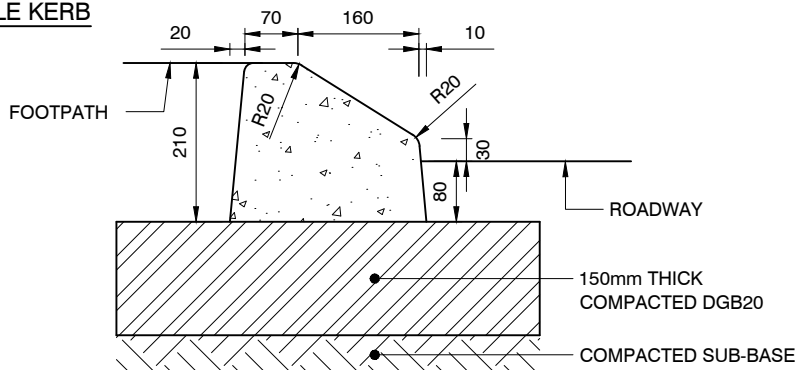
DISH CROSSING



ROLL KERB



MOUNTABLE KERB

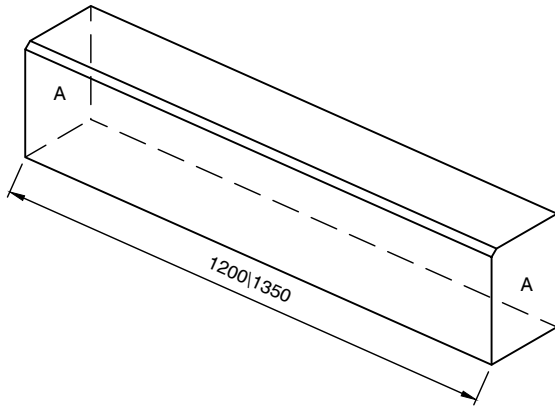


NOTES:

1. ALL CONCRETE SHALL BE MIN 32 MPa UNLESS NOTED OTHERWISE.
2. WHERE THE GUTTER IS SUBJECTED TO HIGHLY REPETITIVE MEDIUM AND HEAVY TRAFFIC, THE GUTTER SHALL BE 200mm THICK IN ACCORDANCE WITH B4 - 4.3.3
3. ALL EDGES SHALL BE TOOL FINISHED WITH 12mm RAD 50mm WIDE EDGING TOOL.
4. EXPANSION JOINTS SHALL BE PLACED AT 15m INTERVALS AND THE INTERFACE WITH OTHER FIXED STRUCTURES.
5. FOR ROAD RESTORATION ADJACENT TO KERB REFER TO STD DRG # 1.1.16
6. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:10

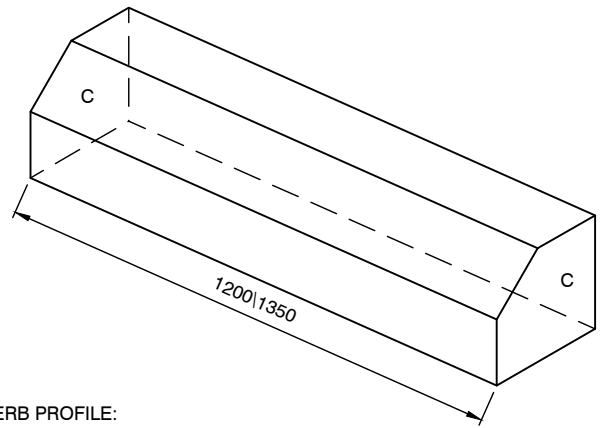
BARRIER KERB



KERB PROFILE:

- TYPE K(F) - FULL HEIGHT
- TYPE K(FR) - FULL HEIGHT/ TO KERB RADIUS (SIMILAR)

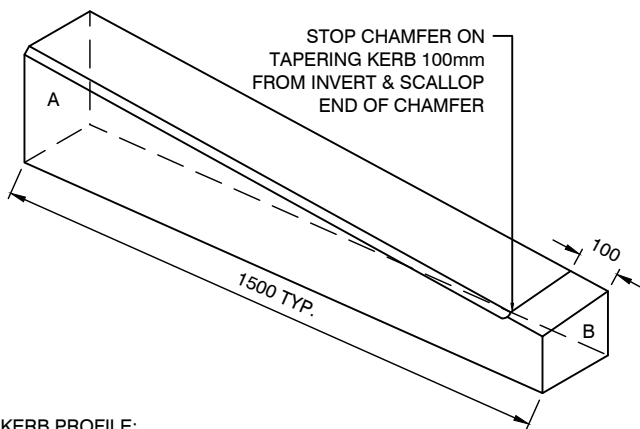
MOUNTABLE KERB



KERB PROFILE:

- TYPE MK(F) - FULL HEIGHT (SHOWN)
- TYPE M(FR) - FULL HEIGHT/ TO KERB RADIUS (SIMILAR)

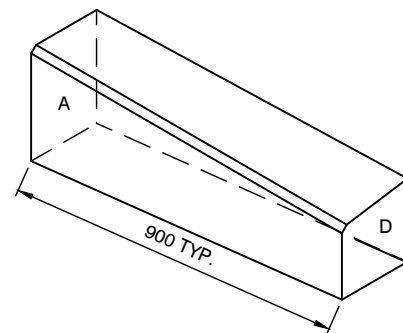
KERB RAMP WING



KERB PROFILE:

- TYPE K(KRF) - PEDESTRIAN CHAMFERED TO FALL
- TYPE K(KRFR) - PEDESTRIAN CHAMFERED TO FALL/TO KERB RADIUS (SIMILAR)

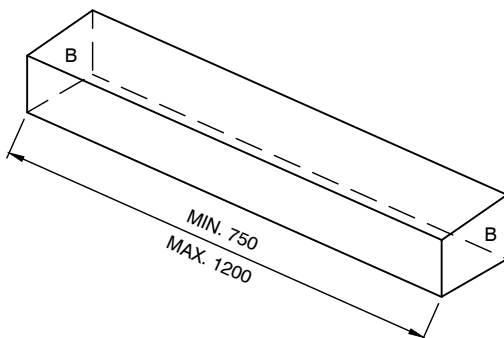
VEHICULAR CROSSING WING



KERB PROFILE:

- TYPE K(VCF) - CHAMFERED TO FALL
- TYPE K(KRFR) - CHAMFERED TO FALL/TO KERB RADIUS (SIMILAR)

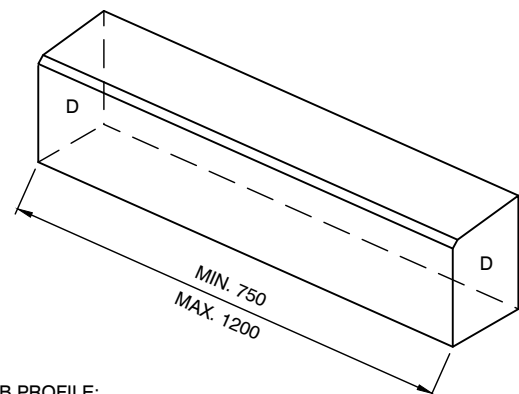
KERB RAMP



KERB PROFILE:

- TYPE K(KR) - PEDESTRIAN CROSSOVER TYPE
- TYPE K(KRR) - PEDESTRIAN CROSSOVER TYPE/TO RADIUS (SIMILAR)

VEHICULAR CROSSING



KERB PROFILE:

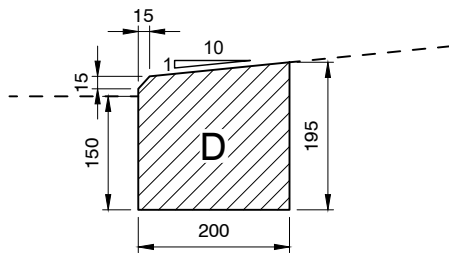
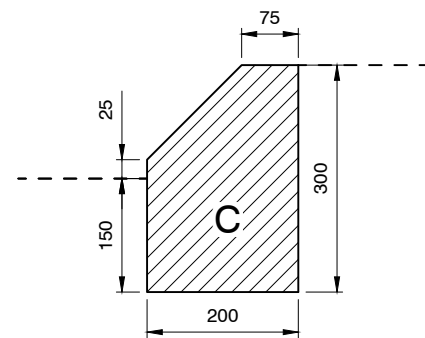
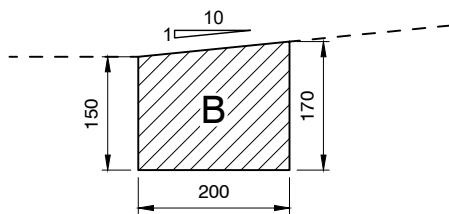
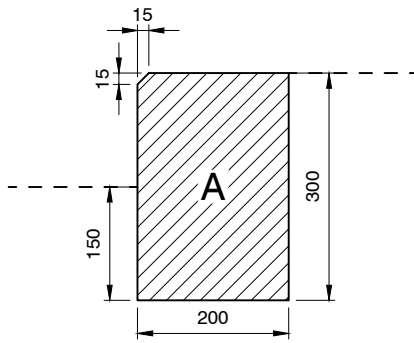
- TYPE K(VC) - VEHICULAR CROSSOVER TYPE
- TYPE K(VCR) - VEHICULAR CROSSOVER TYPE/TO RADIUS (SIMILAR)

NOTES:

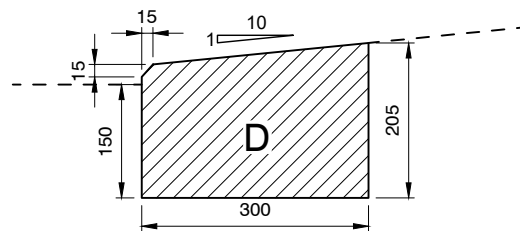
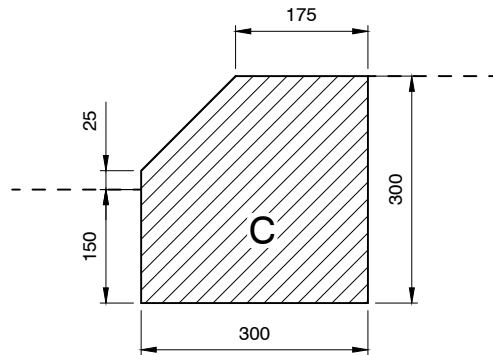
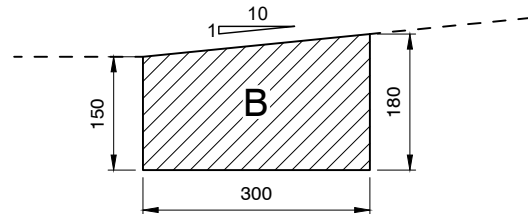
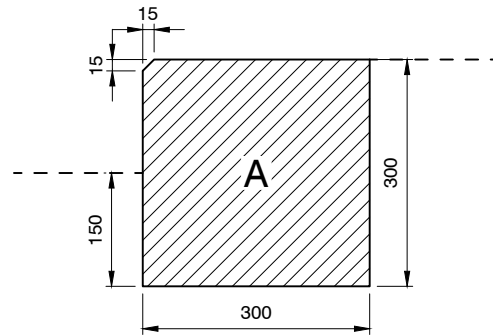
1. FOR KERB RADII OF LESS THAN 7.5m, STONE TO BE CUT TO MATCH ALIGNMENT. USE RADII OF EITHER 750mm, 1m, 3m, 6m OR 7.5m WHERE POSSIBLE.
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:20

200mm WIDE KERB



300mm WIDE KERB

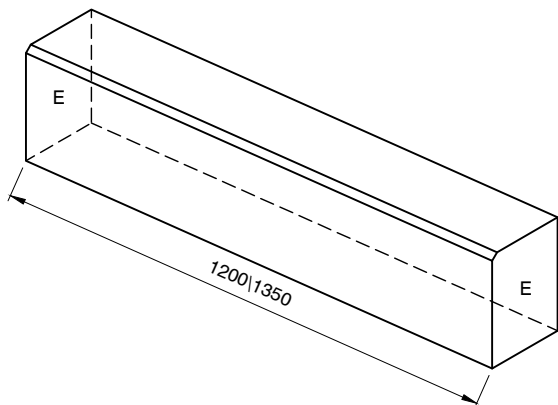


NOTES:

1. FOR SANDSTONE KERBS, A 25mm BULL NOSE ARRIS IS REQUIRED INSTEAD OF CHAMFERED CORNER.
2. ALL CORNERS TO HAVE 1mm ARRIS.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SECTION 1:10

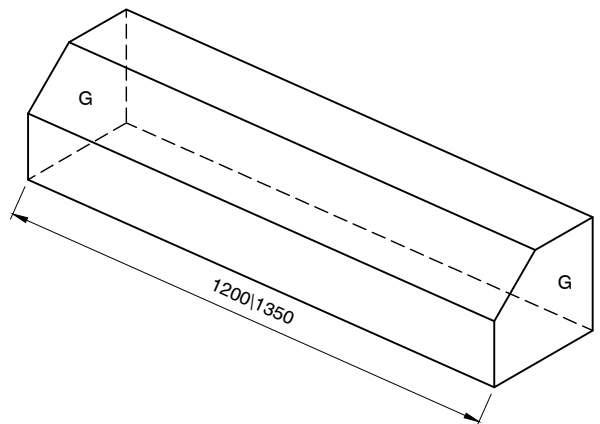
BARRIER KERB



KERB PROFILE:

- TYPE K(F) - FULL HEIGHT
- TYPE K(FR) - FULL HEIGHT/TO KERB RADIUS (SIMILAR)

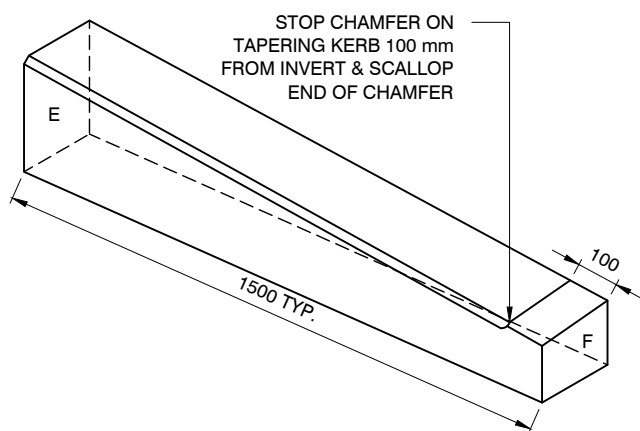
MOUNTABLE KERB



KERB PROFILE:

- TYPE MK(F) - FULL HEIGHT (SHOWN)
- TYPE MK(FR) - FULL HEIGHT ON KERB RADIUS (SIMILAR)

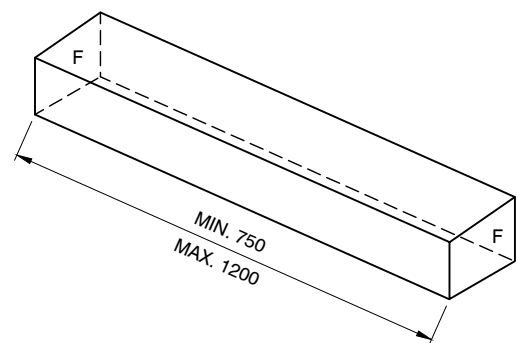
KERB RAMP WINGS



KERB PROFILE:

- TYPE K(KRF) - PEDESTRIAN CHAMFERED TO FALL
- TYPE K(KRFR) - PEDESTRIAN CHAMFERED TO FALL/TO KERB RADIUS (SIMILAR)

KERB RAMP



KERB PROFILE:

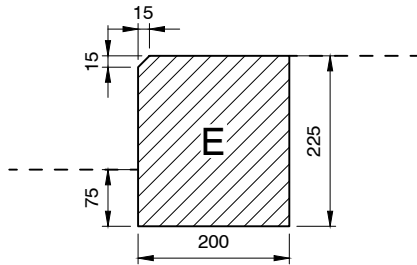
- TYPE K(KR) - PEDESTRIAN CROSSOVER TYPE
- TYPE K(KRR) - PEDESTRIAN CROSSOVER TYPE/TO KERB RADIUS (SIMILAR)

NOTES:

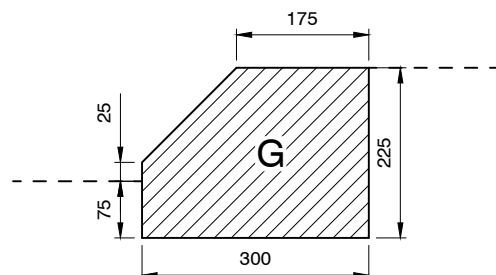
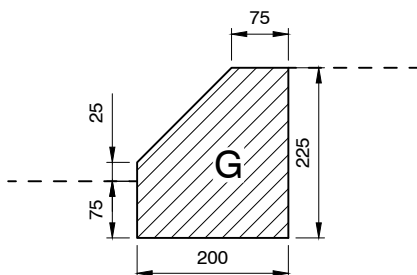
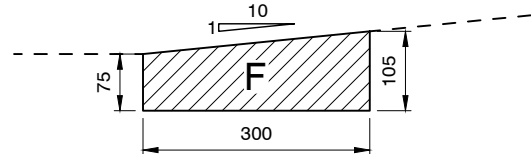
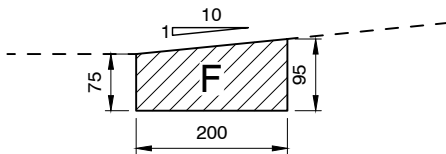
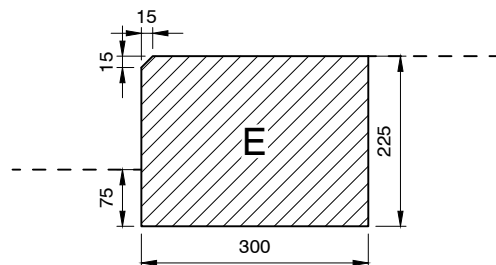
1. FOR KERB RADII OF LESS THAN 7.5 m, STONE TO BE CUT TO MATCH ALIGNMENT. USE RADII OF EITHER 3 m, 6 m OR 7.5 m WHERE POSSIBLE.
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:20

200 mm WIDE KERB



300 mm WIDE KERB

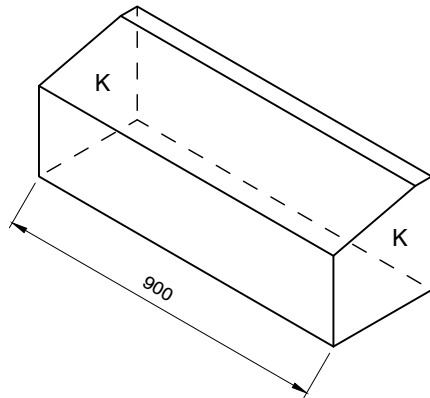


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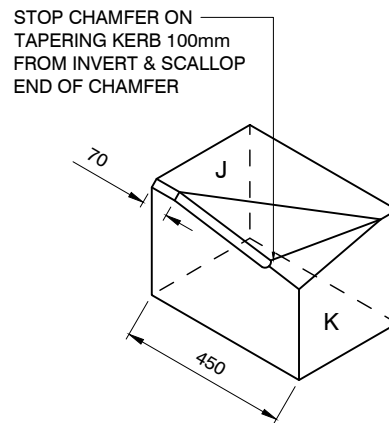
1. SANDSTONE KERBS ARE NOT TO BE USED.
2. ALL CORNERS TO HAVE 1 mm ARRIS.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:10

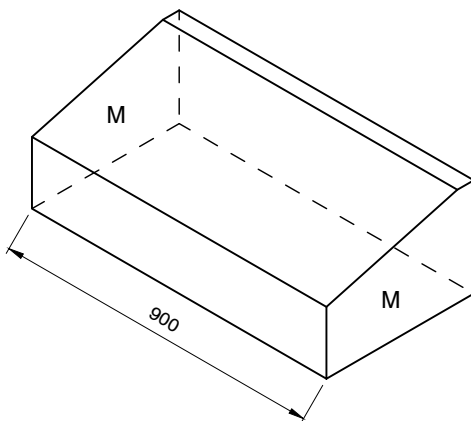
LAYBACK KERB 01 TO CYCLEWAY/
EMERGENCY VEHICLE (LK1VC)



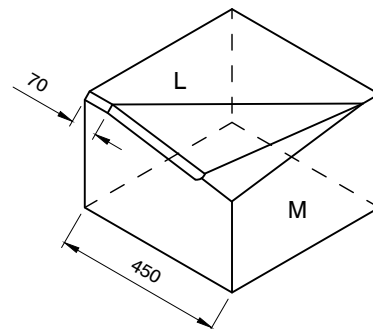
LAYBACK KERB 01 TO CYCLEWAY
TRANSITION (LK1VCT)



LAYBACK KERB 02 TO CYCLEWAY/
EMERGENCY VEHICLE (LK2VC)



LAYBACK KERB 02 TO CYCLEWAY
TRANSITION (LK1VCT)

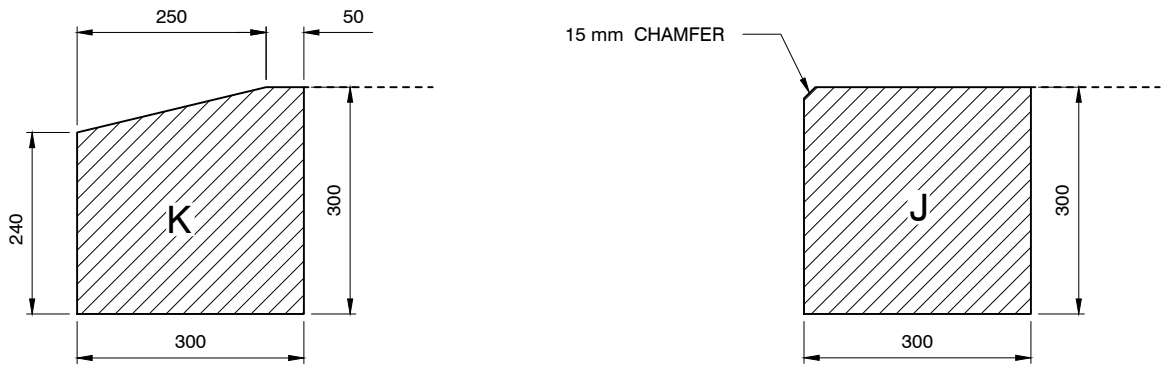


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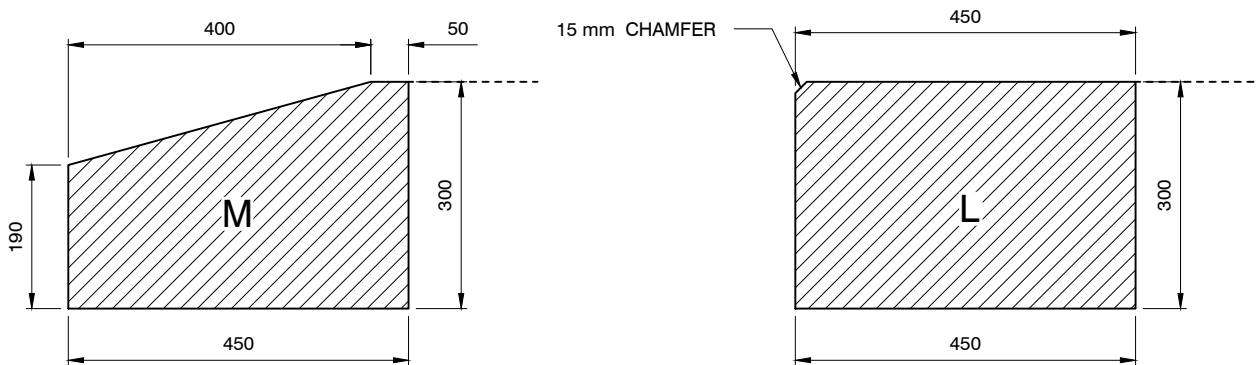
NOTES:

1. TOP TO HAVE EXFOLIATED FINISH.
2. VERTICAL EDGES TO HAVE SAWN FINISH.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

LAYBACK KERB 01 TO CYCLEWAY : LK1VC

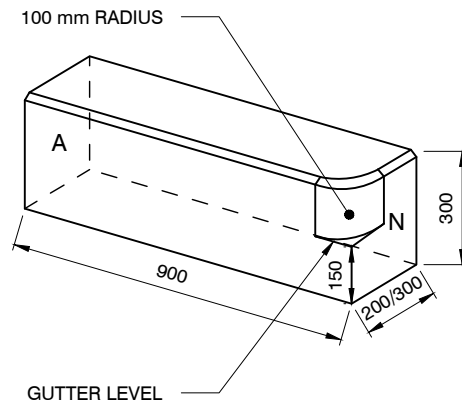


LAYBACK KERB TO CYCLEWAY 02: LK2VC

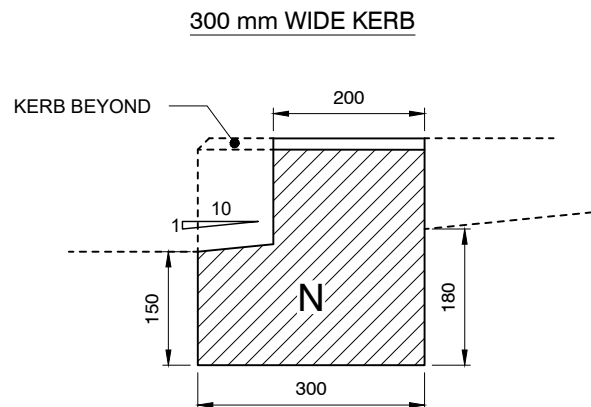
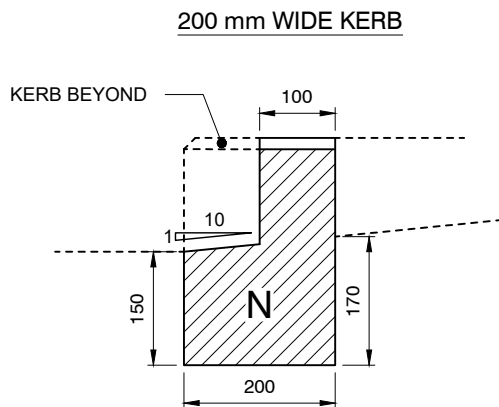


SECTION 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



SECTION 1:20

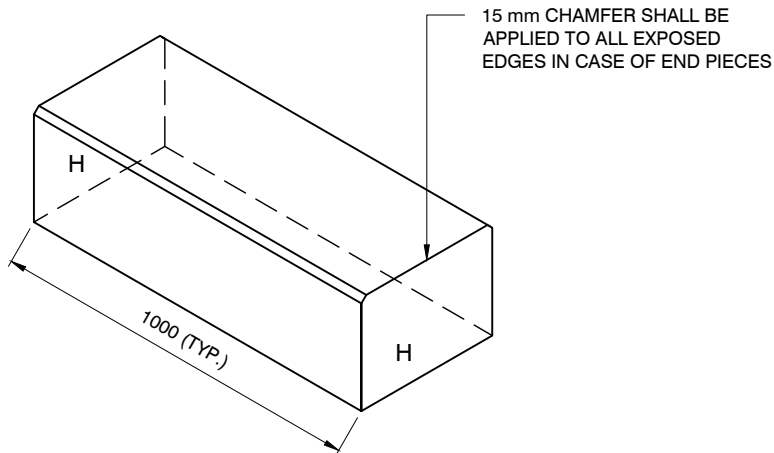


SECTION 1:10

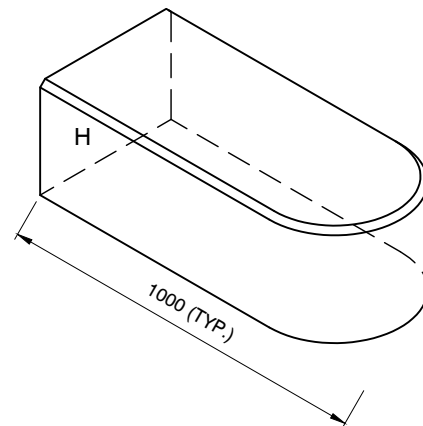
NOTES:

1. FOR SANDSTONE KERBS, A 25 mm BULL NOSE ARRIS IS REQUIRED INSTEAD OF CHAMFERED CORNER.
2. ALL CORNERS TO HAVE 1 mm ARRIS.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

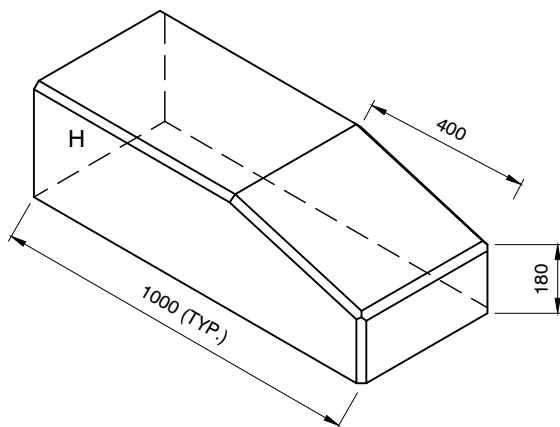
MEDIAN KERB (MK2)



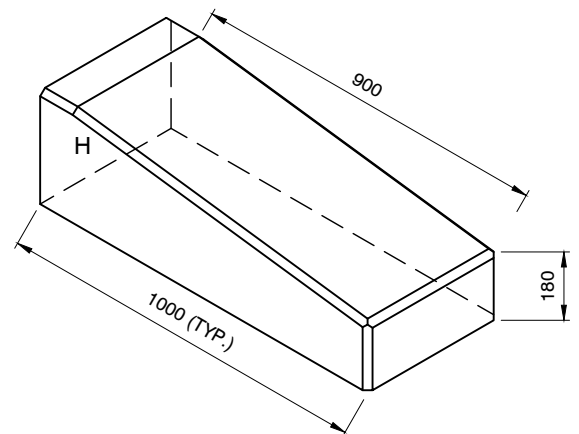
MEDIAN KERB WITH BULLNOSE (MK2BN)



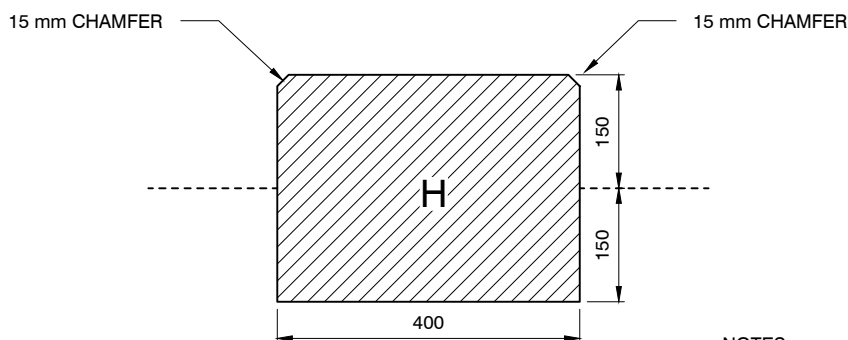
MEDIAN KERB END CHAMFER (MK2EC)



MEDIAN KERB DRIVEWAY CHAMFER (MK2DC)



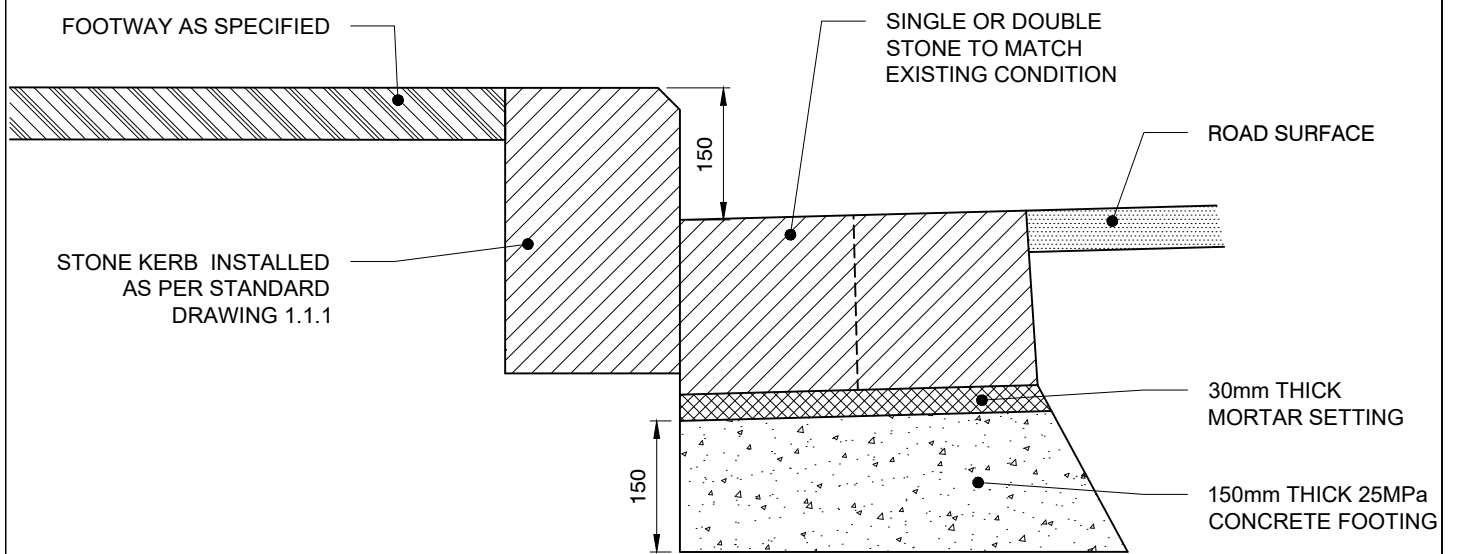
SCALE 1:20



SECTION 1:10

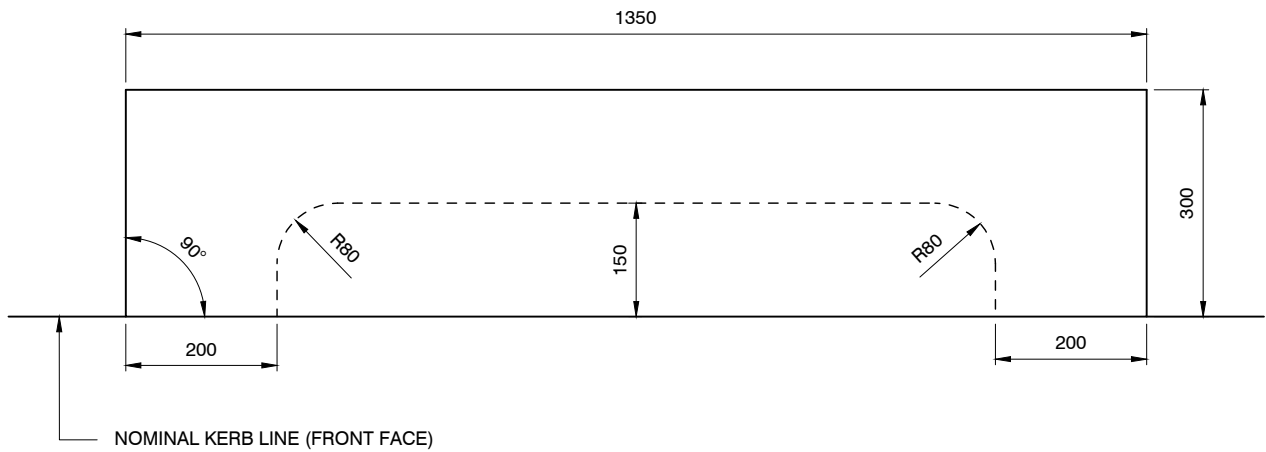
NOTES:

1. TOP TO HAVE EXFOLIATED FINISH.
2. VERTICAL EDGES TO HAVE SAWN FINISH.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

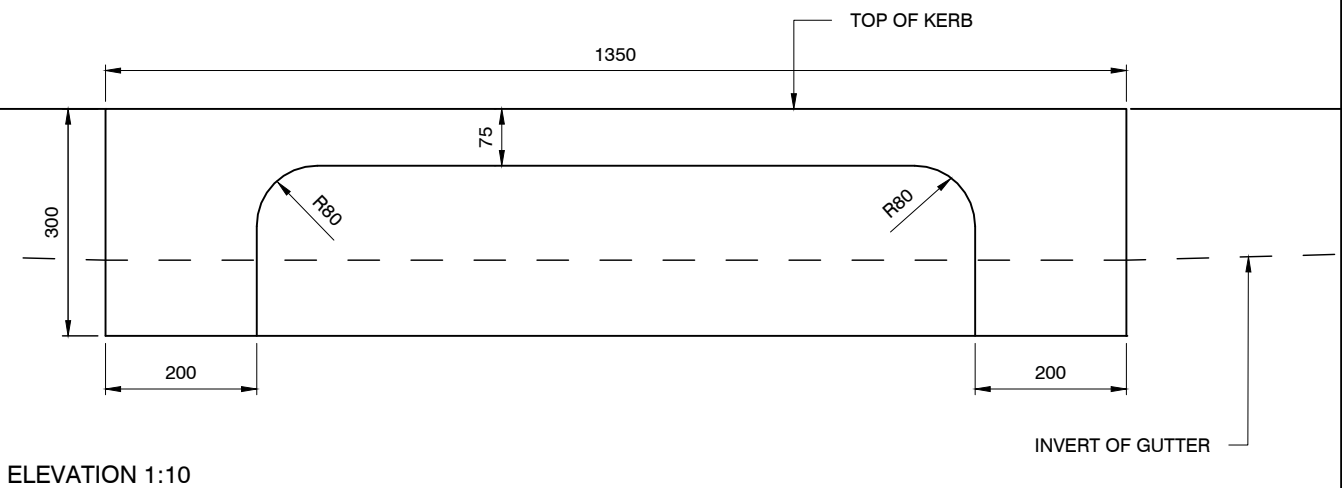


SECTION 1:10

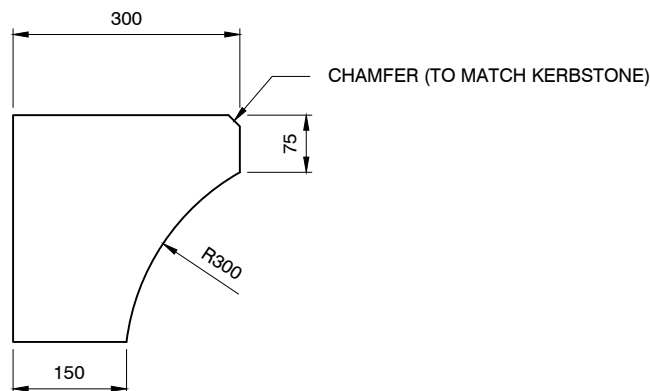
NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN 1:10



ELEVATION 1:10



SECTION 1:10

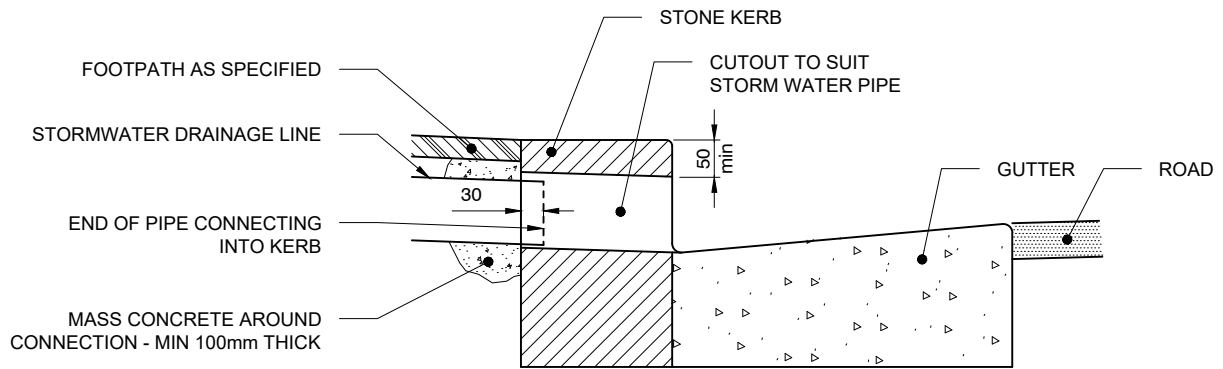
NOTE:

1. LINTEL TO BE ONE COMPLETE STONE (IE. NO JOINTS).
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

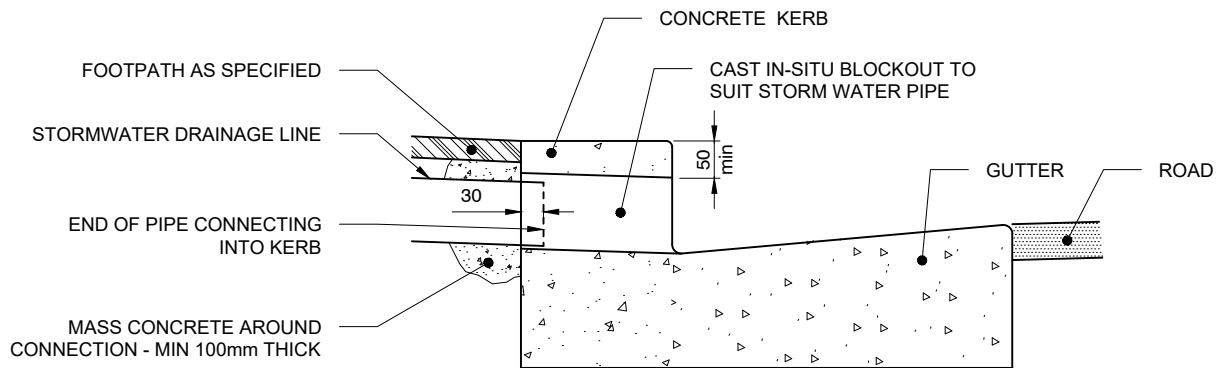


1. LINTEL TO BE ONE COMPLETE STONE (IE. NO JOINTS)
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

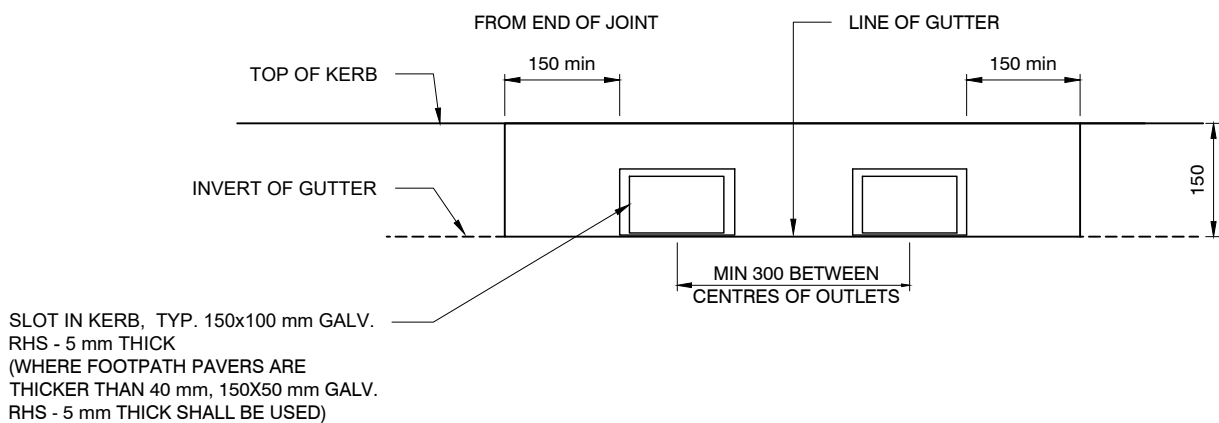
STONE KERB SECTION



CONCRETE KERB SECTION

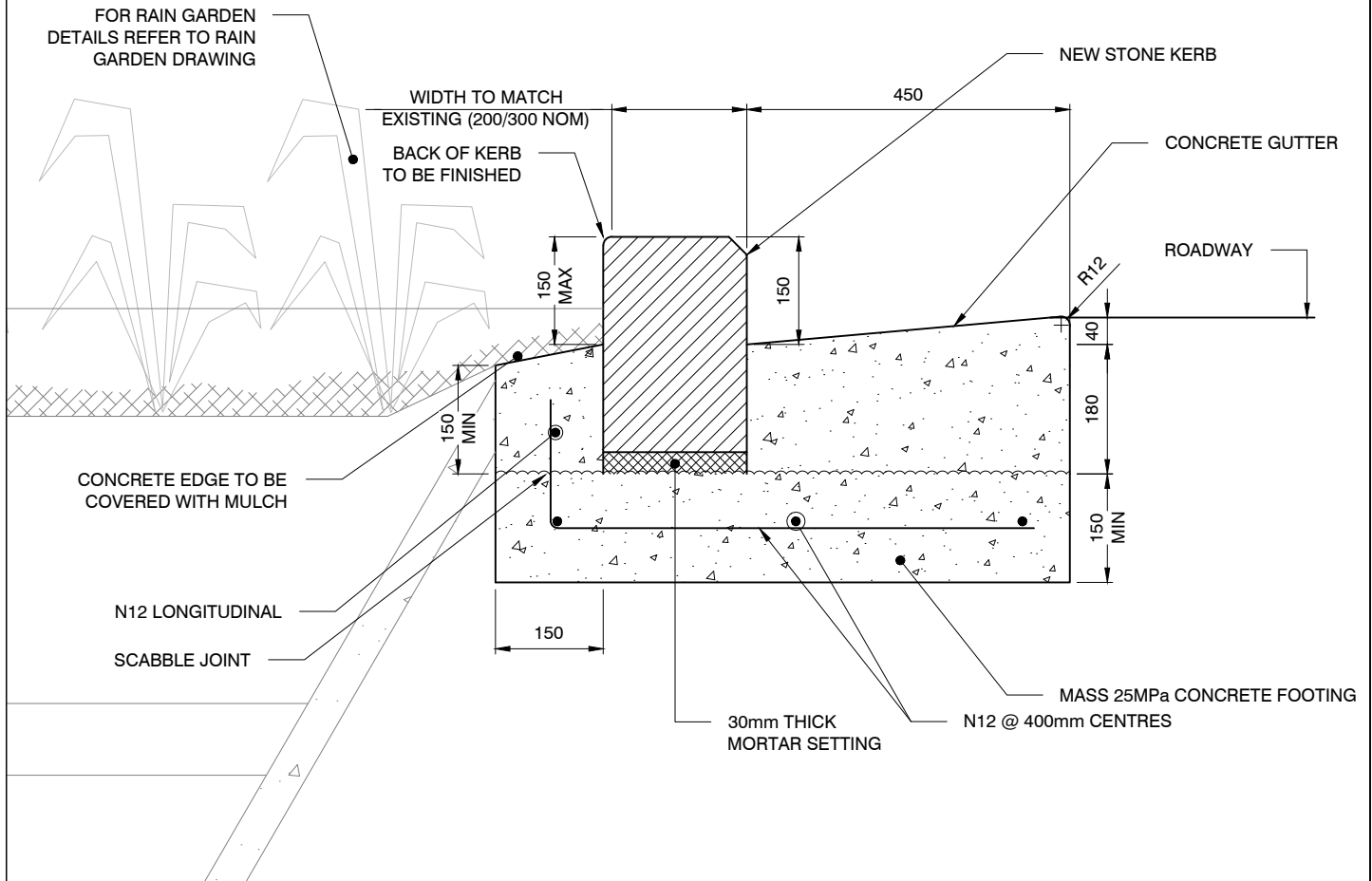


ELEVATION



SCALE 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

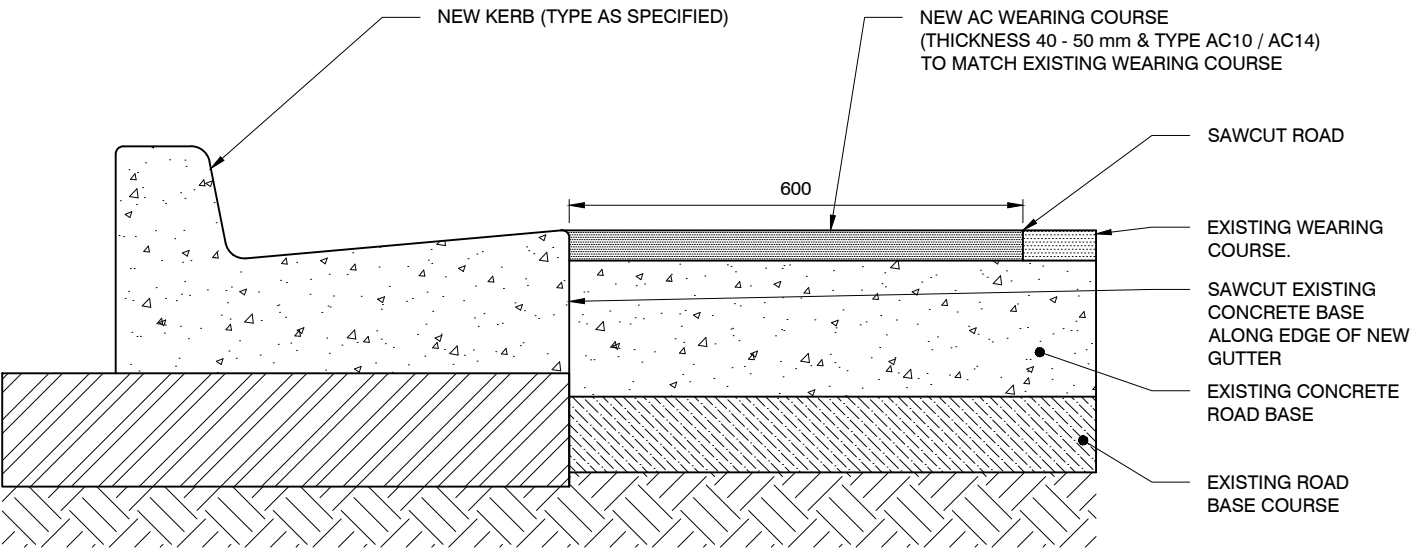


NOTES:

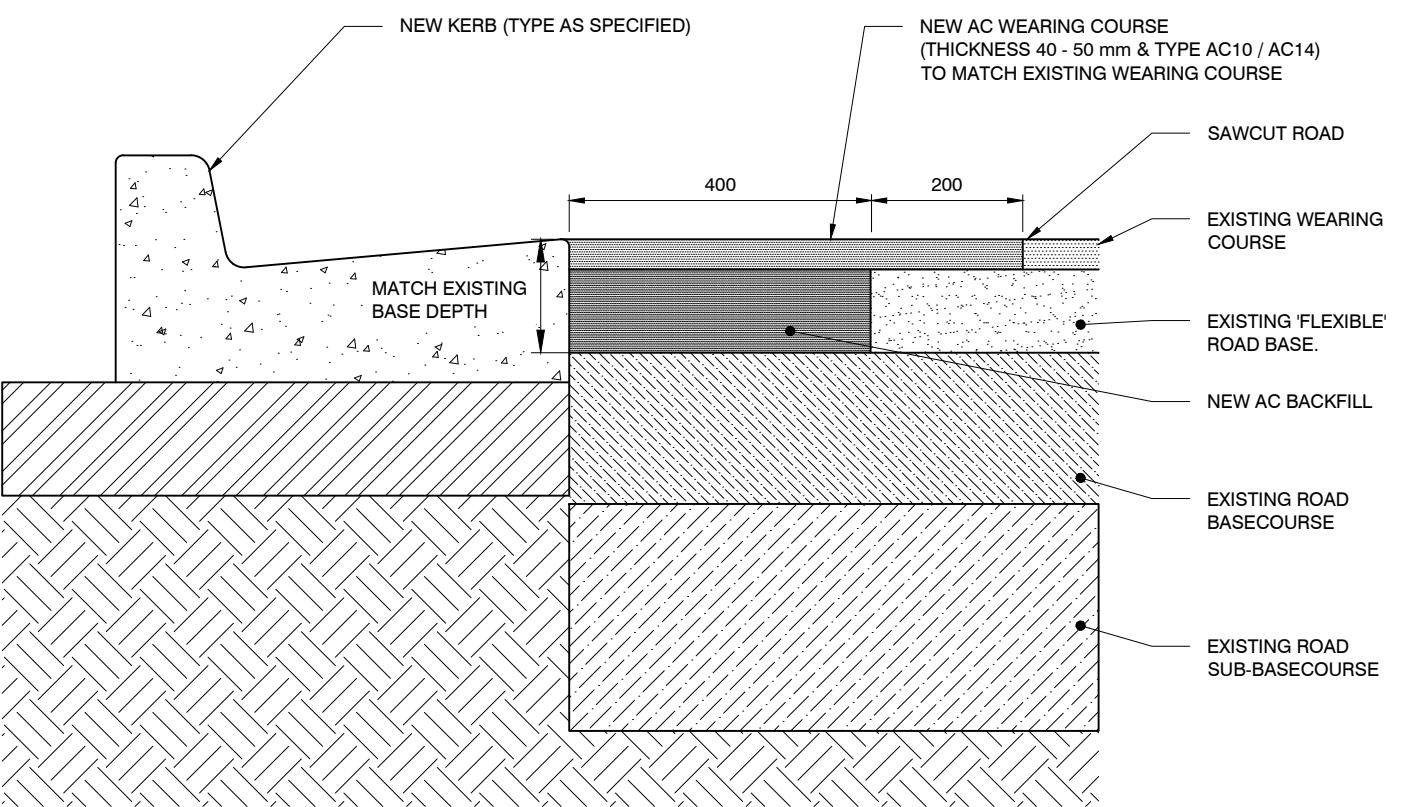
1. DEEP EMBEDMENT KERB ONLY TO BE USED.
2. FOR ROAD RESTORATION ADJACENT TO KERB REFER TO STD DRG # 1.1.16
3. WHERE THE GUTTER IS SUBJECTED TO HIGHLY REPETITIVE MEDIUM AND HEAVY TRAFFIC, THE GUTTER SHALL BE 200mm THICK IN ACCORDANCE WITH B4 - 4.3.3
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

SACLE 1:10

RIGID PAVEMENT RESTORATION



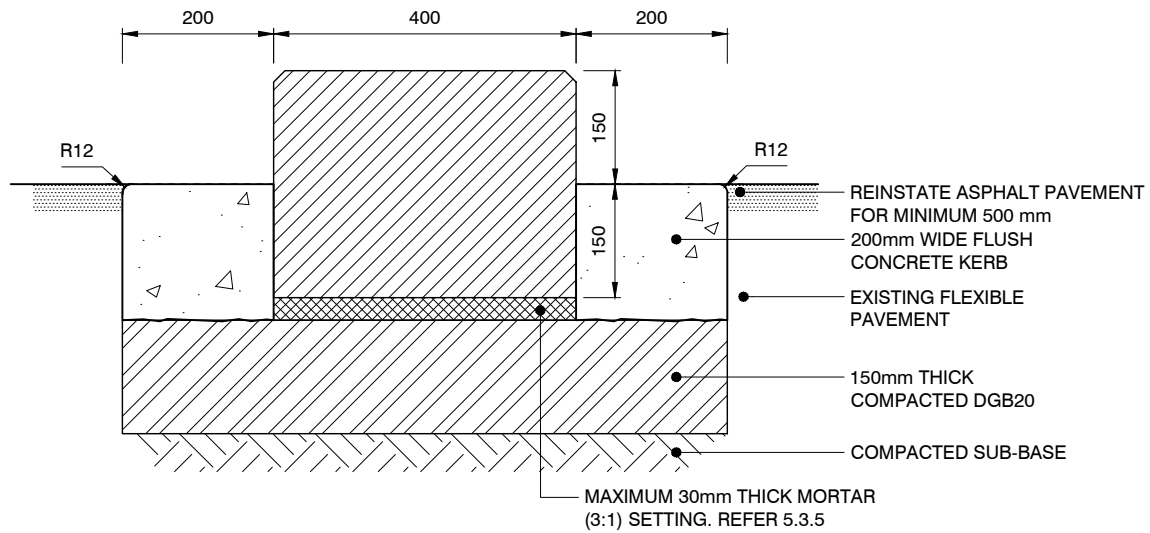
FLEXIBLE PAVEMENT RESTORATION



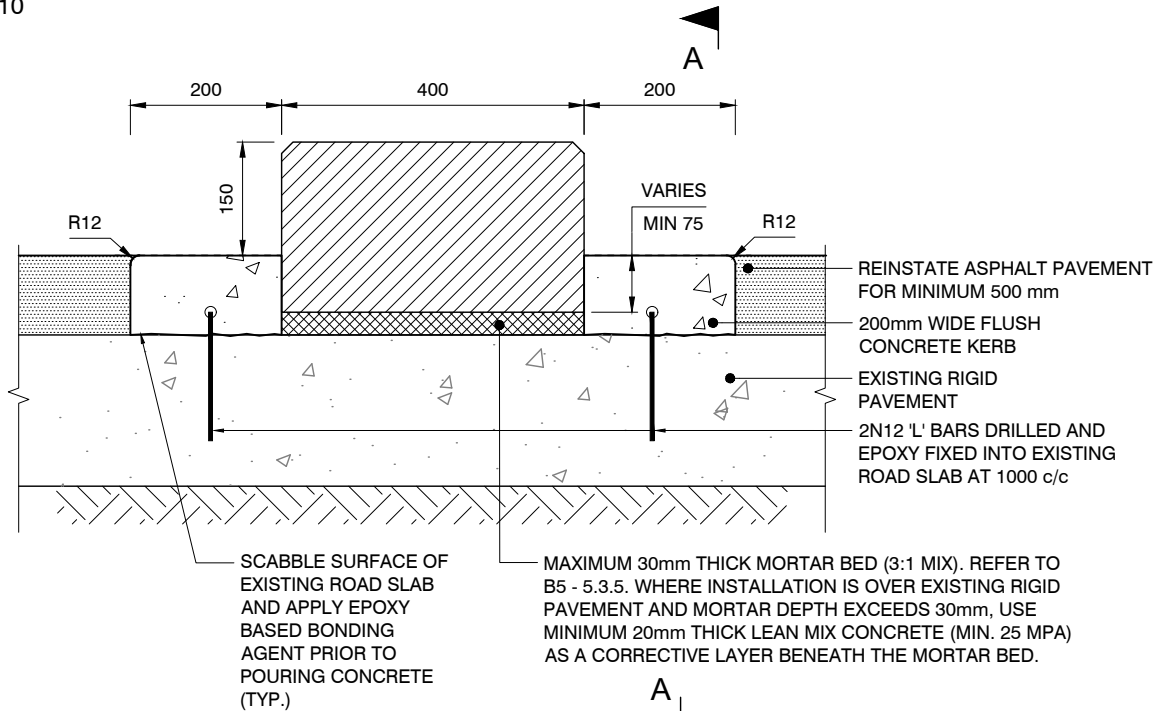
SECTION 1:10

NOTES:

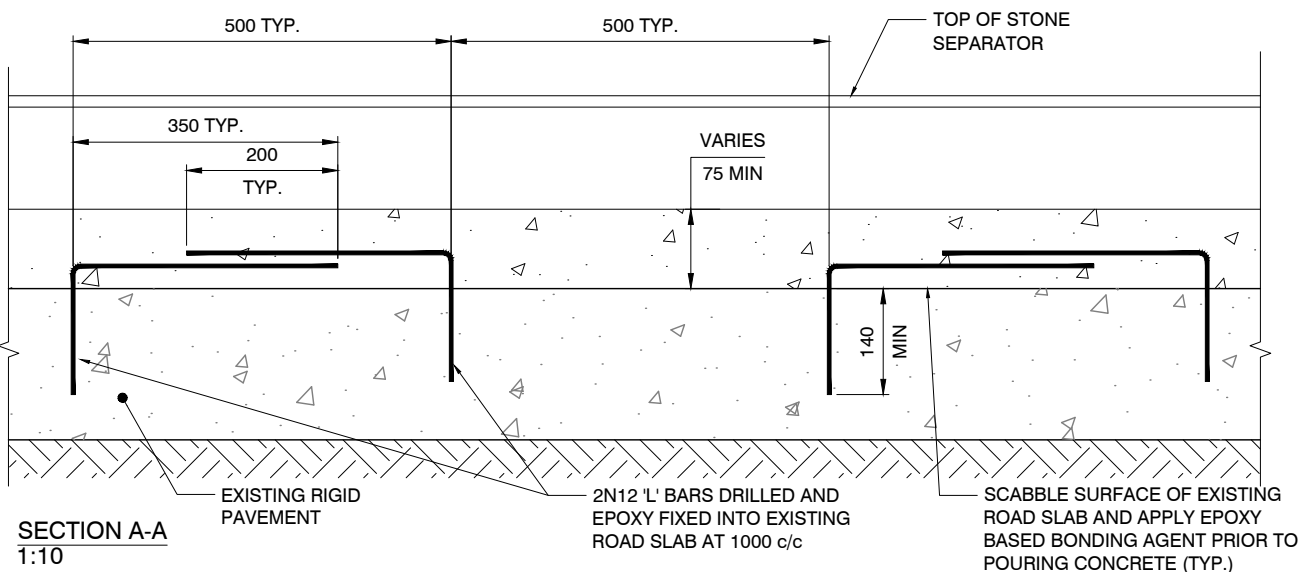
1. FOR KERB AND GUTTER DETAILS REFER TO STANDARD DRAWING # 1.1.1, 1.1.2 & 3.1.5
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



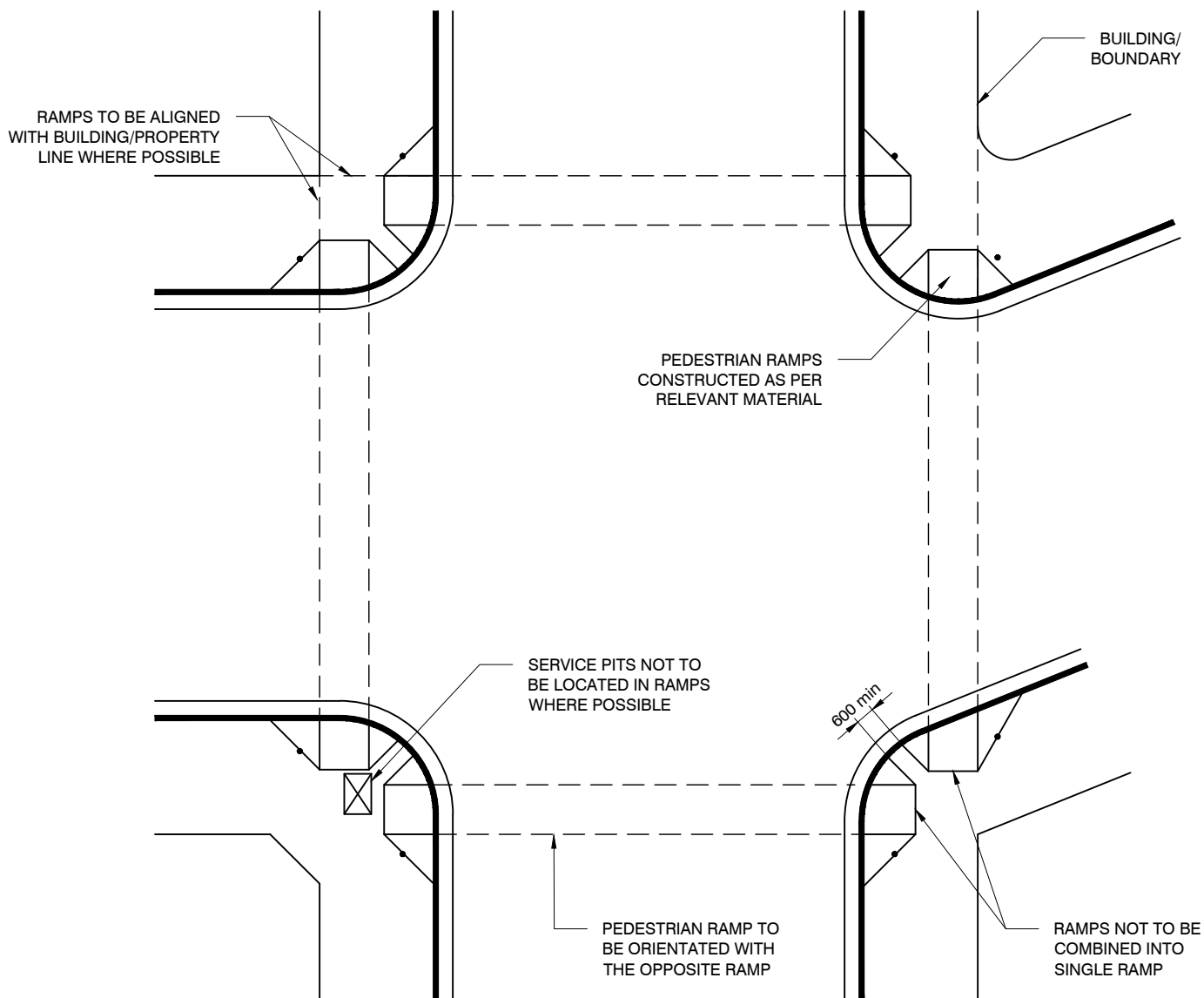
STONE SEPARATOR INSTALLATION - FLEXIBLE PAVEMENT
1:10



STONE SEPARATOR INSTALLATION - RIGID PAVEMENT
1:10



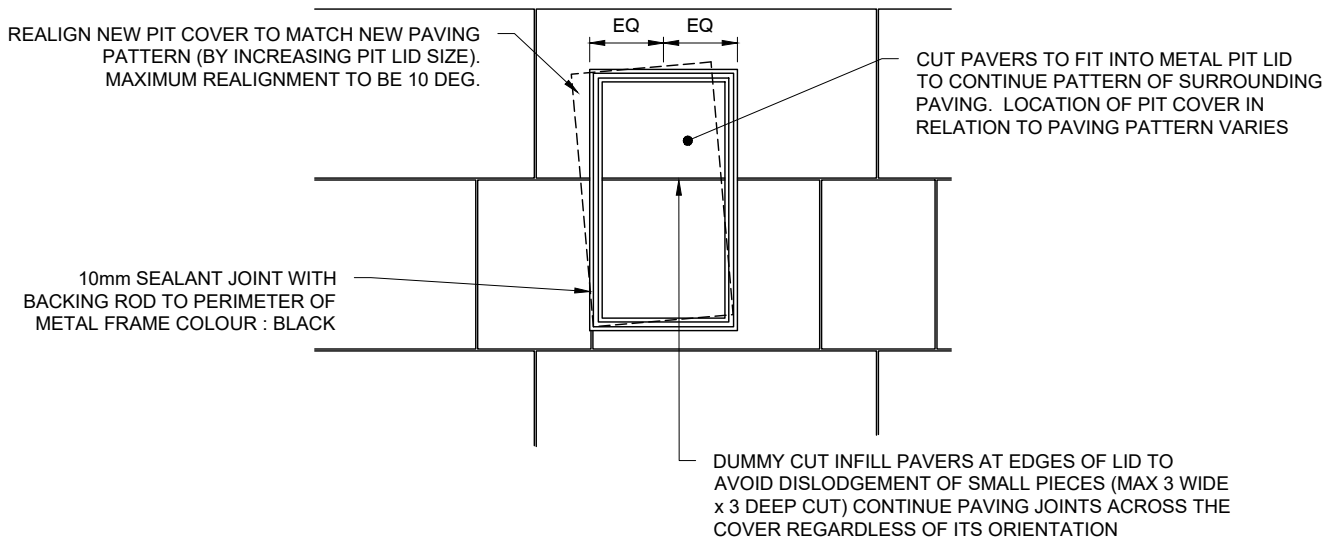
SECTION A-A
1:10



PLAN
1:200

NOTES:

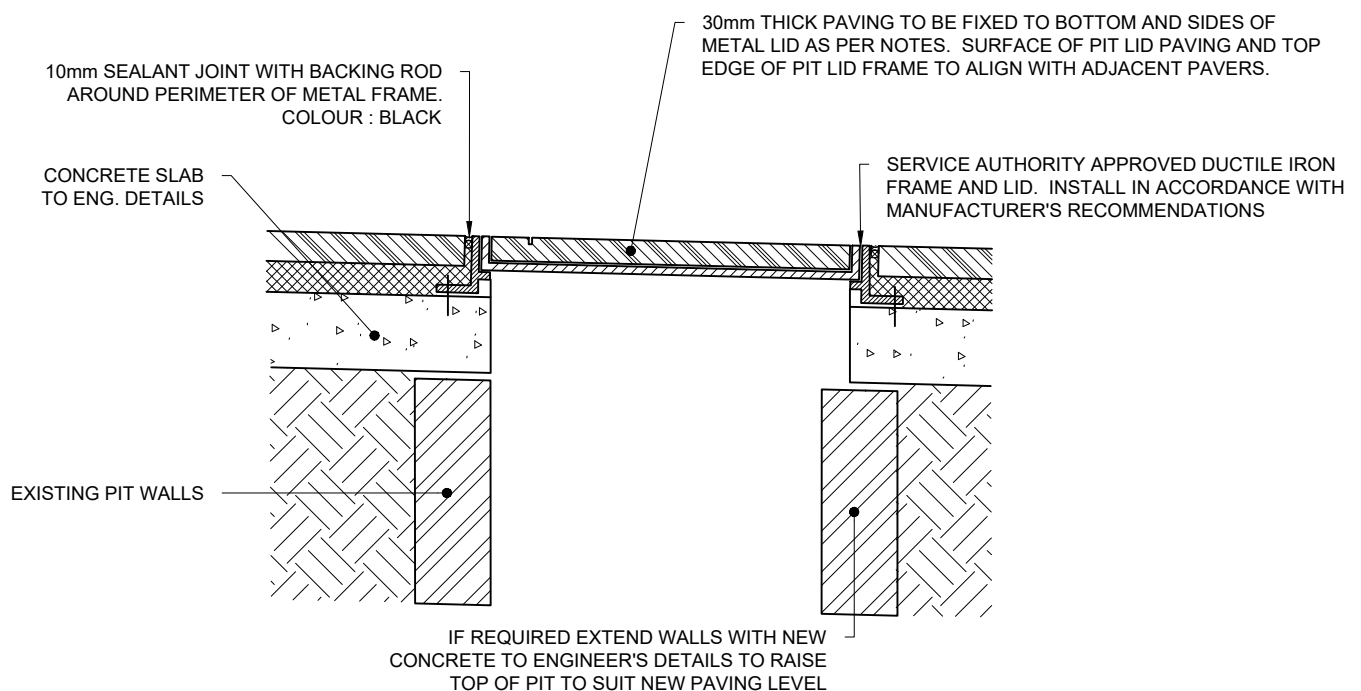
1. FOR SIGNALISED INTERSECTION TYPE AND LOCATION OF THE POLES, DIRECTION OF THE PEDESTRIAN CROSSING AND RAMPS SHALL BE APPROVED BY CITY OF SYDNEY TRAFFIC OPERATION TEAM PRIOR TO ANY CHANGES. CONSULTATION AND OBTAINING APPROVAL FROM TfNSW ALSO MAY BE REQUIRED.
2. FOR ANY PEDESTRIAN CROSSING AT THE INTERSECTION SITE SPECIFIC LIGHTING DESIGN SHALL BE PREPARED AND APPROVAL SHALL BE OBTAINED FROM CITY OF SYDNEY'S INFRASTRUCTURE PLANNING.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



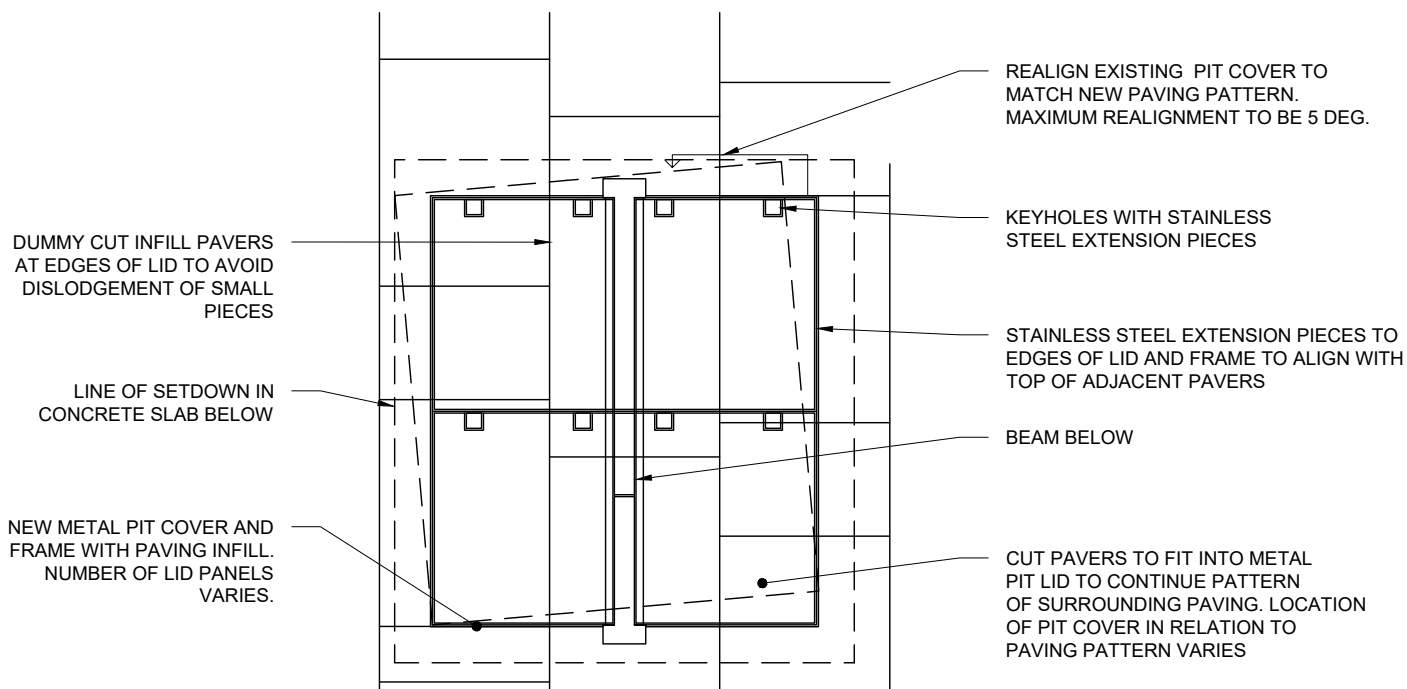
NOTES:

1. OBTAIN APPROVAL FROM AUTHORITY FOR COVER REALIGNMENT.
2. CLEAN PIT LID WITH WIRE BRUSH AND THEN WITH SOFT BRUSH TO REMOVE RUST FROM LID.
3. MOISTEN PAVER AND LID TO AID HYDRATION OF MORTAR MIX.
4. USE CEMENT MORTAR WITH FORTIFYING COMPOUND (ARDEX OR APPROVED EQUIVALENT) AS JOINTING MATERIAL.
5. USE A RICHER MIX eg. 1:1 CEMENT:SAND THINNER (2-5mm) JOINTS AND 1:2.5 CEMENT:SAND MIX FOR THICKER (12-15mm) JOINT.
6. PIT LID INFILLS TO HAVE APPROX. 3mm GAP ON SIDES SO AS TO NOT TOUCH THE LID.
7. GAPS ON SIDES SHOULD BE GROUTED WITH RICH CEMENT SAND MIX WITH FORTIFYING COMPOUND.
8. TO ENSURE THE SERVICE PIT IS STILL ACCESSIBLE AND FIT FOR USE THE RELEVANT AUTHORITY IS TO PROVIDE SIGN OFF / APPROVAL.
9. PIT COVERS IN THE FOOTPATH SHALL COMPLY WITH CLASS 'C' LOADING AND FOR TRAFFICABLE AREA, PIT COVERS SHALL COMPLY WITH CLASS 'D' LOADING.
10. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

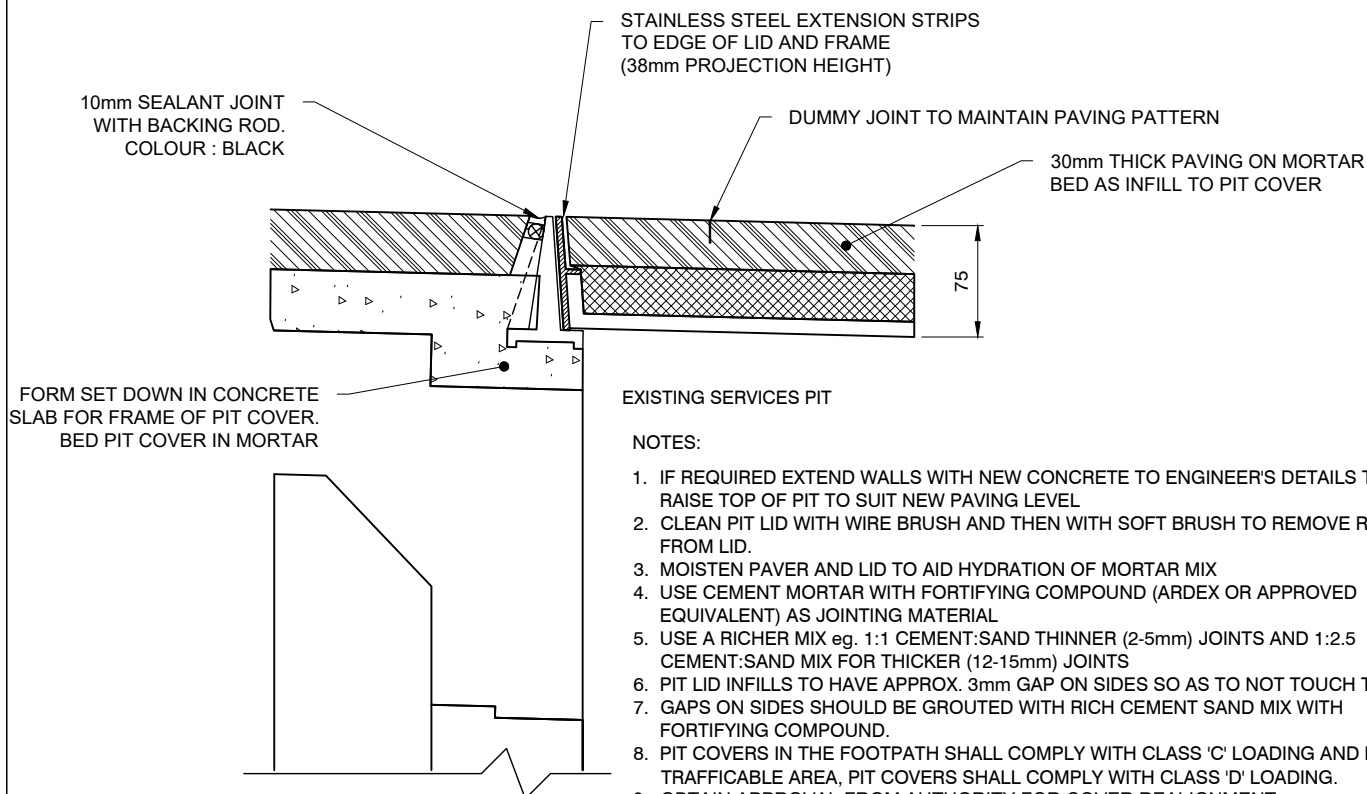
PLAN 1:50



SECTION 1:10



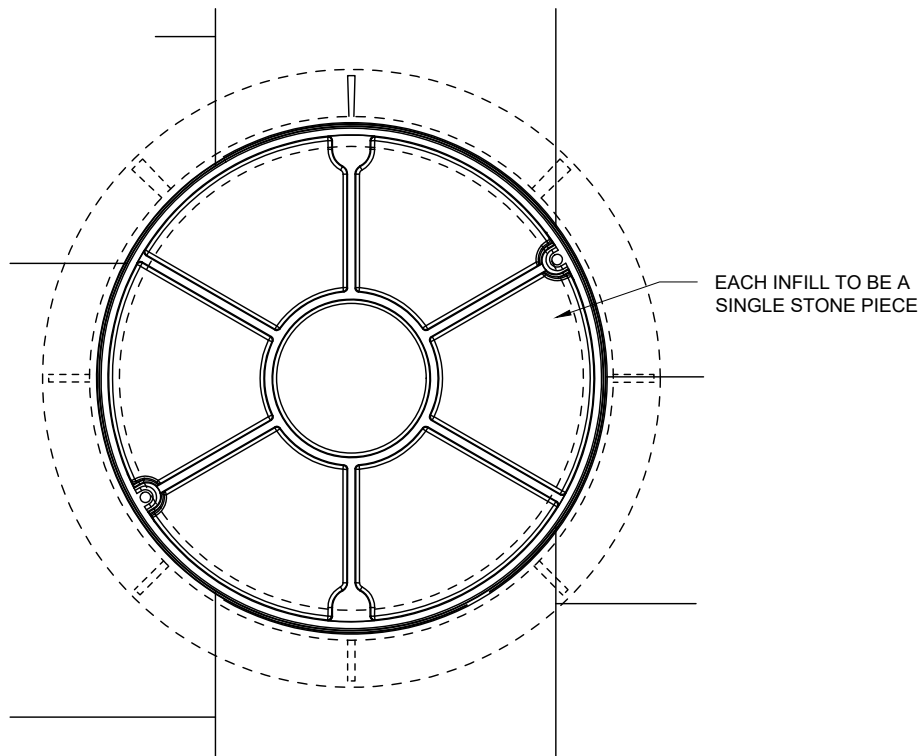
PLAN 1:20



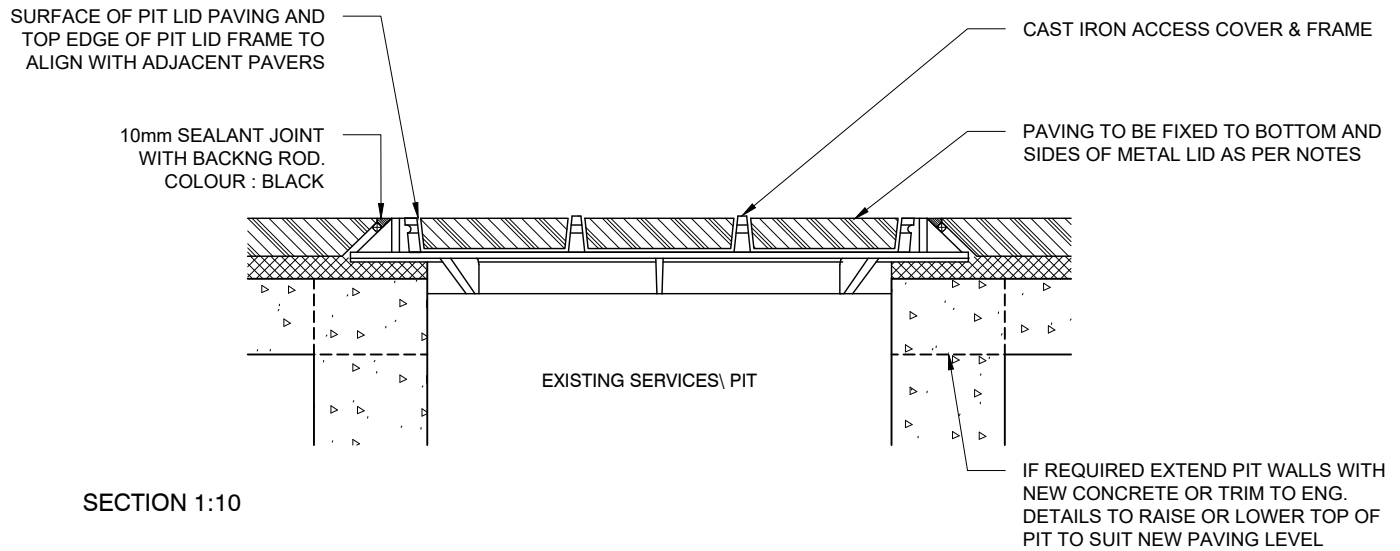
SECTION 1:5

NOTES:

1. IF REQUIRED EXTEND WALLS WITH NEW CONCRETE TO ENGINEER'S DETAILS TO RAISE TOP OF PIT TO SUIT NEW PAVING LEVEL
2. CLEAN PIT LID WITH WIRE BRUSH AND THEN WITH SOFT BRUSH TO REMOVE RUST FROM LID.
3. MOISTEN PAVER AND LID TO AID HYDRATION OF MORTAR MIX
4. USE CEMENT MORTAR WITH FORTIFYING COMPOUND (ARDEX OR APPROVED EQUIVALENT) AS JOINTING MATERIAL
5. USE A RICHER MIX eg. 1:1 CEMENT:SAND THINNER (2-5mm) JOINTS AND 1:2.5 CEMENT:SAND MIX FOR THICKER (12-15mm) JOINTS
6. PIT LID INFILLS TO HAVE APPROX. 3mm GAP ON SIDES SO AS TO NOT TOUCH THE LID
7. GAPS ON SIDES SHOULD BE GROUTED WITH RICH CEMENT SAND MIX WITH FORTIFYING COMPOUND.
8. PIT COVERS IN THE FOOTPATH SHALL COMPLY WITH CLASS 'C' LOADING AND FOR TRAFFICABLE AREA, PIT COVERS SHALL COMPLY WITH CLASS 'D' LOADING.
9. OBTAIN APPROVAL FROM AUTHORITY FOR COVER REALIGNMENT.
10. TO ENSURE THE SERVICE PIT IS STILL ACCESSIBLE AND FIT FOR USE THE RELEVANT AUTHORITY IS TO PROVIDE SIGN OFF / APPROVAL
11. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



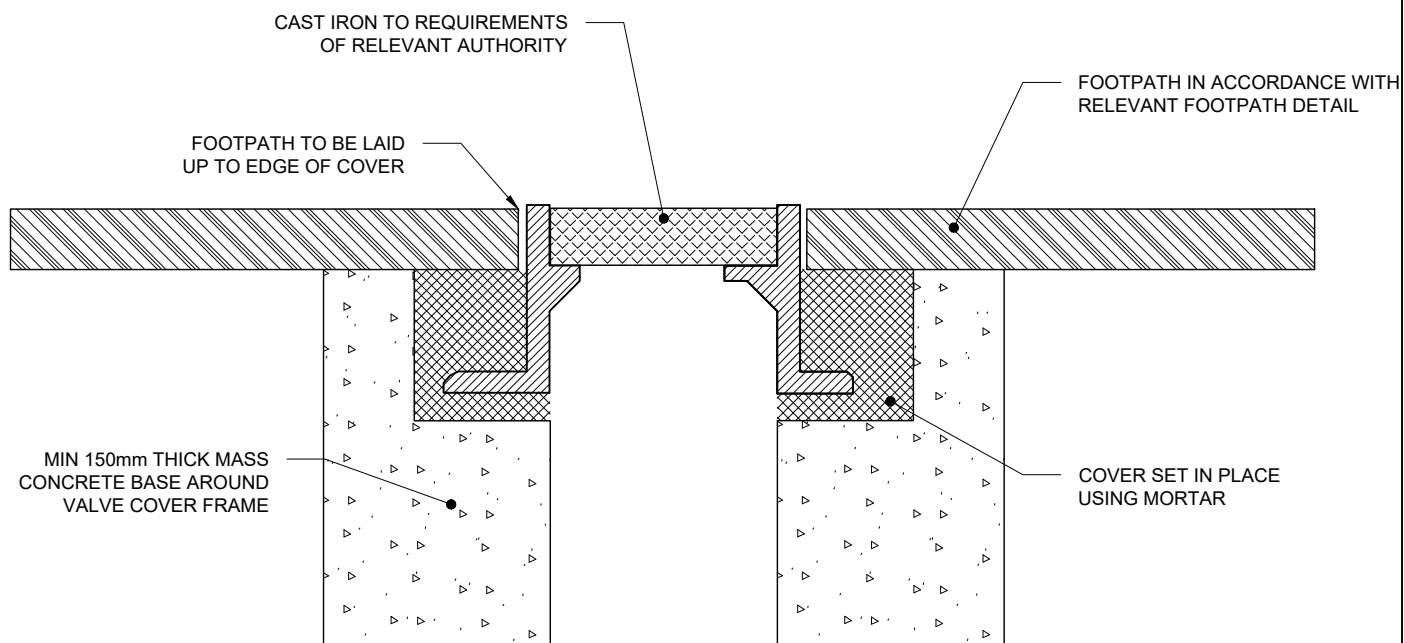
PLAN 1:10



SECTION 1:10

NOTES:

1. CLEAN PIT LID WITH BRUSH AND THEN SOFT BRUSH TO CLEAR RUST FROM LID
2. MOISTEN PAVER AND LID TO AID HYDRATION OF MORTAR MIX
3. USE CEMENT MORTAR WITH FORTIFYING COMPOUND (ARDEX OR APPROVED EQUIVALENT) AS JOINTING MATERIALS
4. USE A RICHER MIX eg: 1:1 CEMENT:SAND FOR THINNER (3-5mm) JOINTS AND 1:2.5 CEMENT:SAND MIX FOR THICKER (12-15mm) JOINTS
5. PIT LID INFILLS TO HAVE APPROX. 3mm GAP ON SIDES SO AS NOT TO TOUCH THE LID
6. GAPS ON SIDES SHOULD BE GROUTED WITH RICH CEMENT SAND MIX WITH FORTIFYING COMPOUND.
7. TO ENSURE THE SERVICE PIT IS STILL ACCESSIBLE AND FIT FOR USE THE RELEVANT AUTHORITY IS TO PROVIDE SIGN OFF / APPROVAL
8. PIT COVERS IN THE FOOTPATH SHALL COMPLY WITH CLASS 'C' LOADING AND FOR TRAFFICABLE AREA, PIT COVERS SHALL COMPLY WITH CLASS 'D' LOADING.
9. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



NOTE:

1. TO ENSURE THE SERVICE VALVE IS STILL ACCESSIBLE, THE RELEVANT AUTHORITY IS TO PROVIDE SIGN OFF / APPROVAL
2. FOR FIRE FIGHTING OPERATIONS, SERVICE VALVE COVERS ARE TO BE REINSTATED IN ACCORDANCE WITH THE WATER SUPPLY CODE OF AUSTRALIA WSA 03-2002-2.2 SYDNEY WATER EDITION VERSION, AND SPECIFICALLY DRAWINGS WAT-1305-V AND WAT-1306-V. ON COMPLETION THE NSWFB ZONE COMMANDERS EAST ONE (TEL: 0419 993 065) SHOULD BE CONTACTED TO ARRANGE TESTING TO ENSURE COMPLIANCE. TYPICALLY, THE CBD HAS SCREW HYDRANTS WHILST OUTSIDE CBD, SPRING HYDRANTS ARE USED.
3. WHERE PIT LID IS GREATER THAN 300mm IN ANY DIRECTION, AN INFILL LID IS TO BE USED.
4. PIT COVERS IN THE FOOTPATH SHALL COMPLY WITH CLASS 'C' LOADING AND FOR TRAFFICABLE AREA PIT COVERS SHALL COMPLY WITH CLASS 'D' LOADING.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

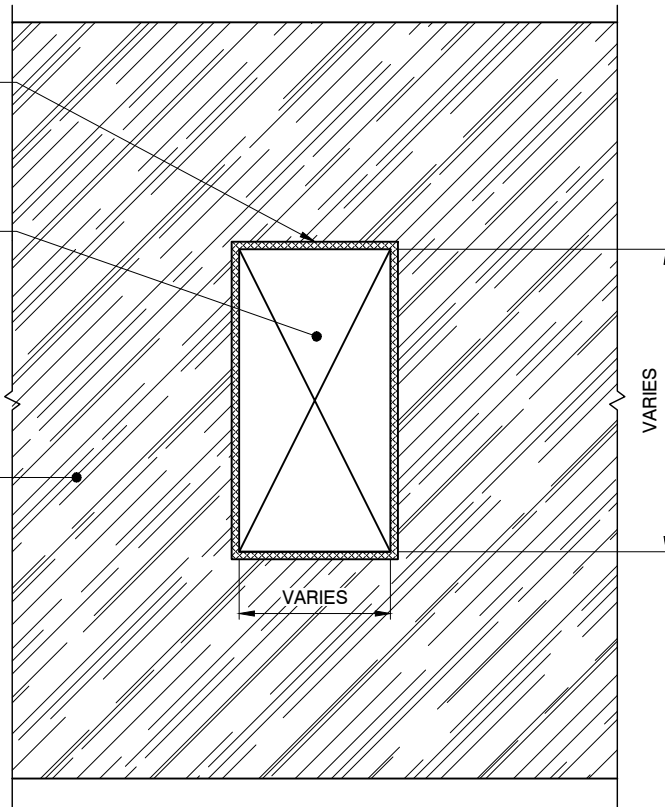
SCALE 1:5

TYPICAL PLAN

100mm DEEP SELF EXPANDING JOINT
FILLER IN CASE OF SURROUNDING AREA
BEING INFLEXIBLE PAVEMENT (eg
CONCRETE)

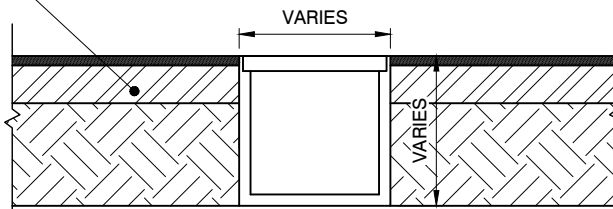
SERVICE PIT

FOOTPATH / ROAD AREA



FLEXIBLE PAVEMENT ELEVATION

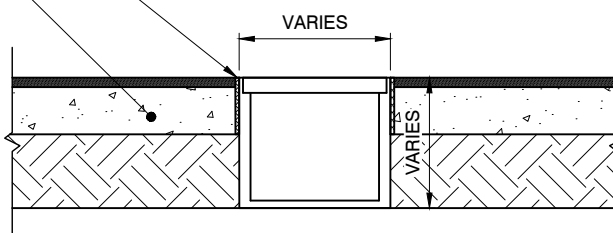
FLEXIBLE PAVEMENT



RIGID PAVEMENT ELEVATION

SELF EXPANDING JOINT FILLER
TO DEPTH OF RIGID PAVEMENT

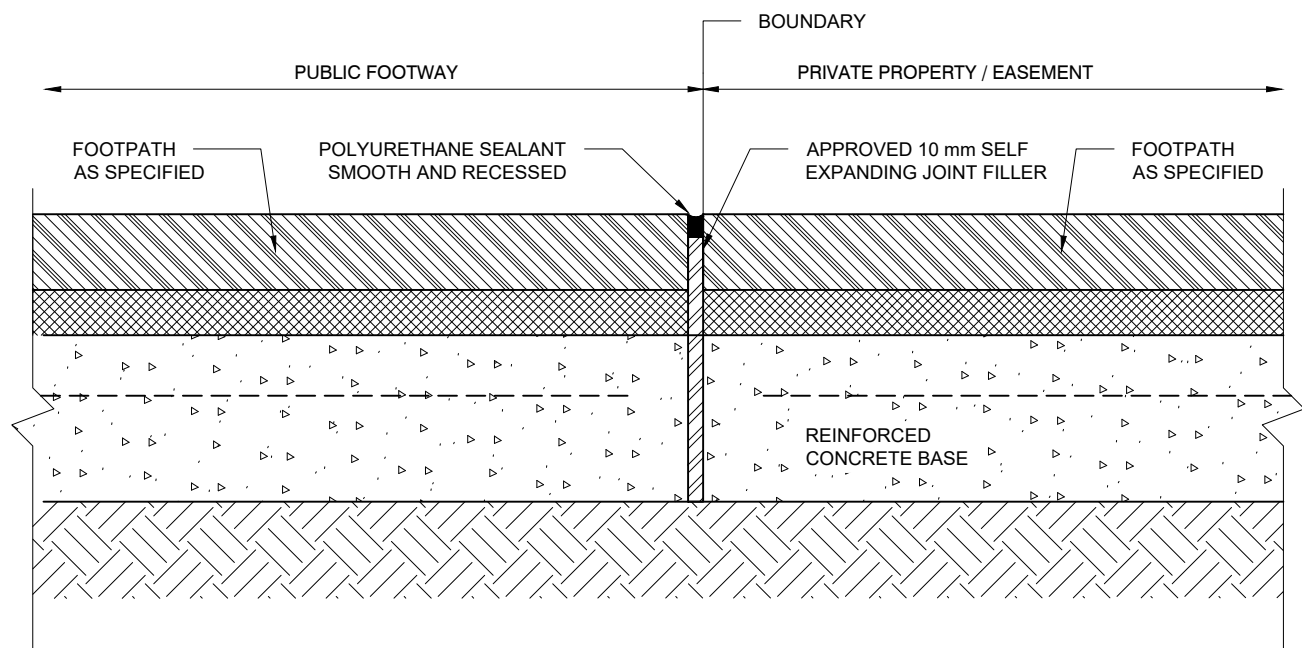
RIGID PAVEMENT



SCALE 1:20

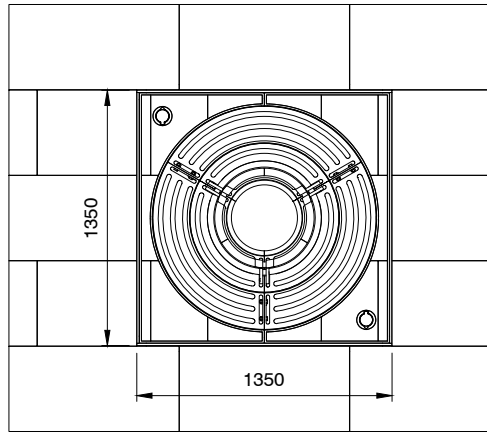
NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

JUNCTION ALONG PROPERTY BOUNDARY

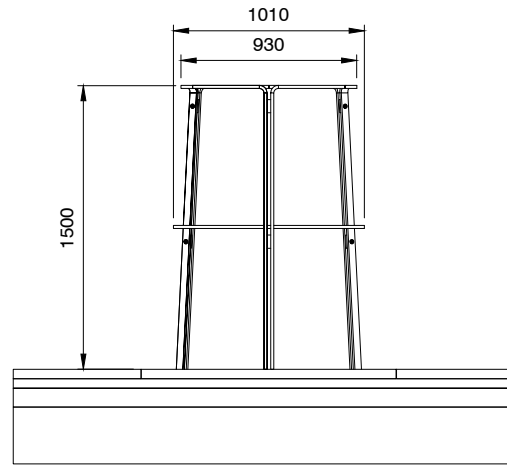


SECTION 1:5

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

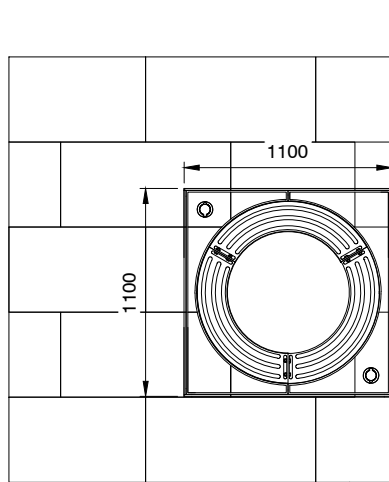


PLAN

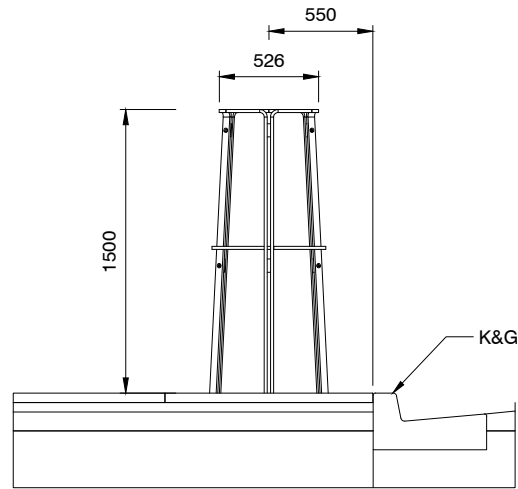


ELEVATION - FRONT

STANDARD TREE GUARD AND TREE GRATE 1:25



PLAN



ELEVATION - FRONT

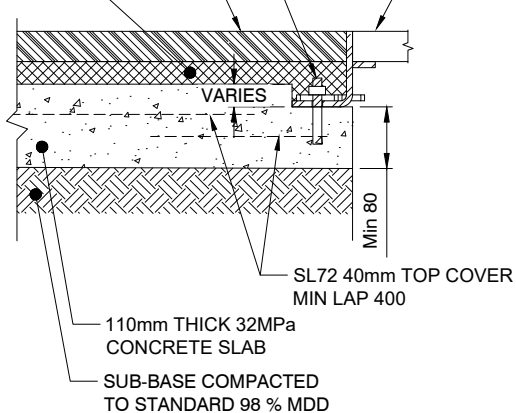
SLIMMED DESIGN TREE GUARD AND TREE GRATE 1:25

ANCHOR BOLT DETAILS REFER TO
MANUFACTURER'S SPEC

30mm THICK
MORTAR BED
MAXIMUM

PAVER

TREE
GRATE



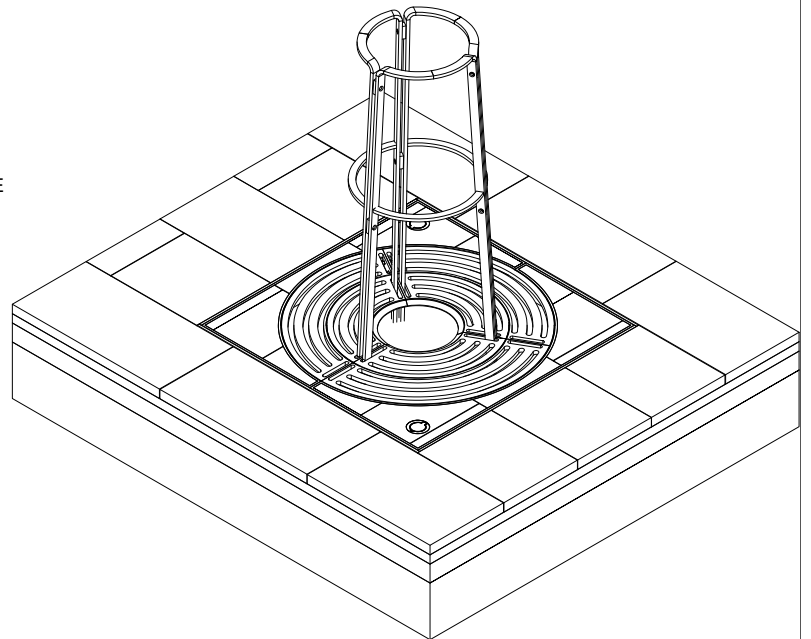
VARIES

SL72 40mm TOP COVER
MIN LAP 400

110mm THICK 32MPa
CONCRETE SLAB

SUB-BASE COMPACTED
TO STANDARD 98 % MDD

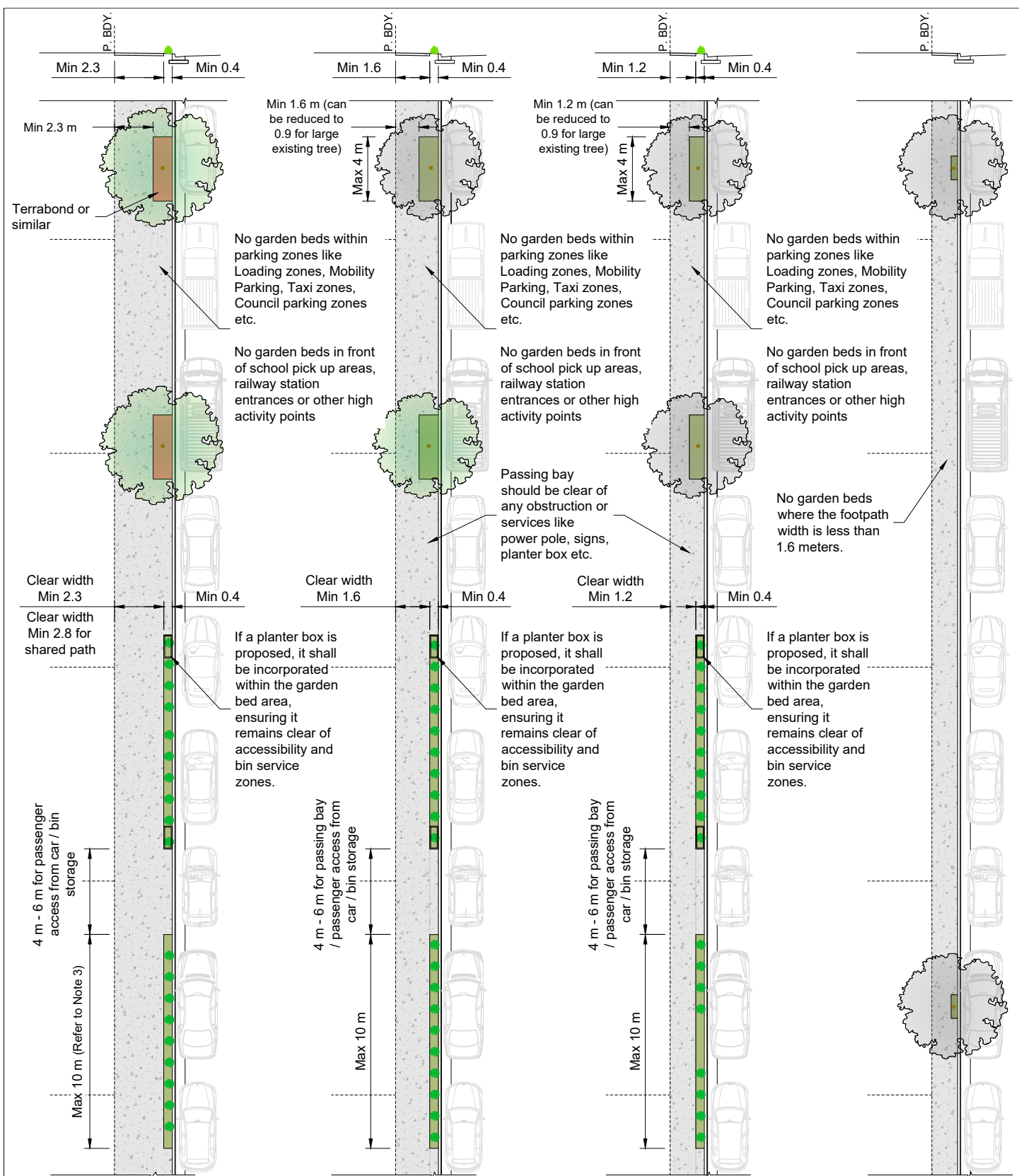
FRAME INSTALLATION DETAILS
1:10



ISOMETRIC VIEW
N.T.S.

NOTES:

1. FOR DESIGN DETAILS AND INSTALLATION GUIDES REFER TO MANUFACTURER'S SPECIFICATION
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



FOOTPATH ≥ 2.7 m WIDE ⁽¹⁾

1:250 - LOCAL STREET (Mid to High Activity Streets)

FOOTPATH ≥ 2 m < 2.7 m WIDE ⁽¹⁾

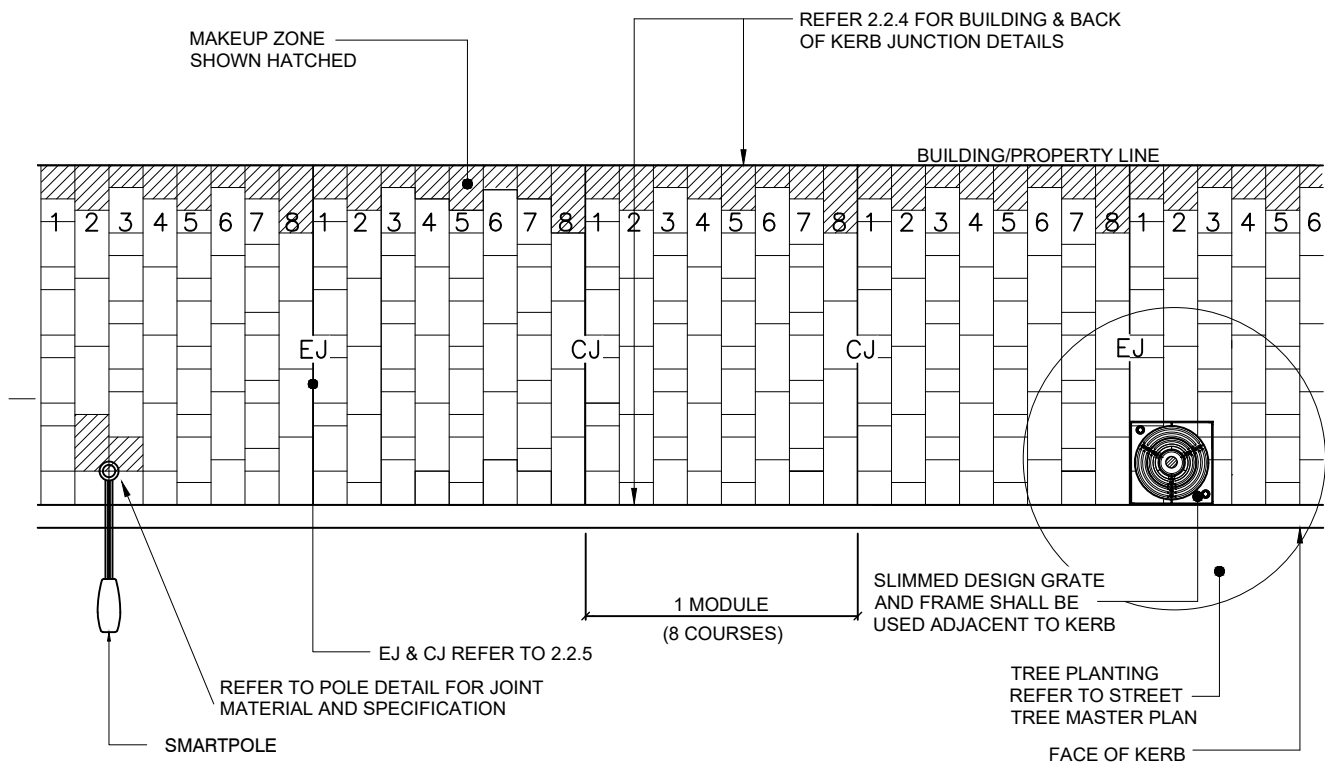
1:250 - LOCAL STREET (Low Activity Streets)

FOOTPATH ≥ 1.6 < 2 m WIDE ⁽¹⁾

FOOTPATH < 1.6 m WIDE ⁽¹⁾

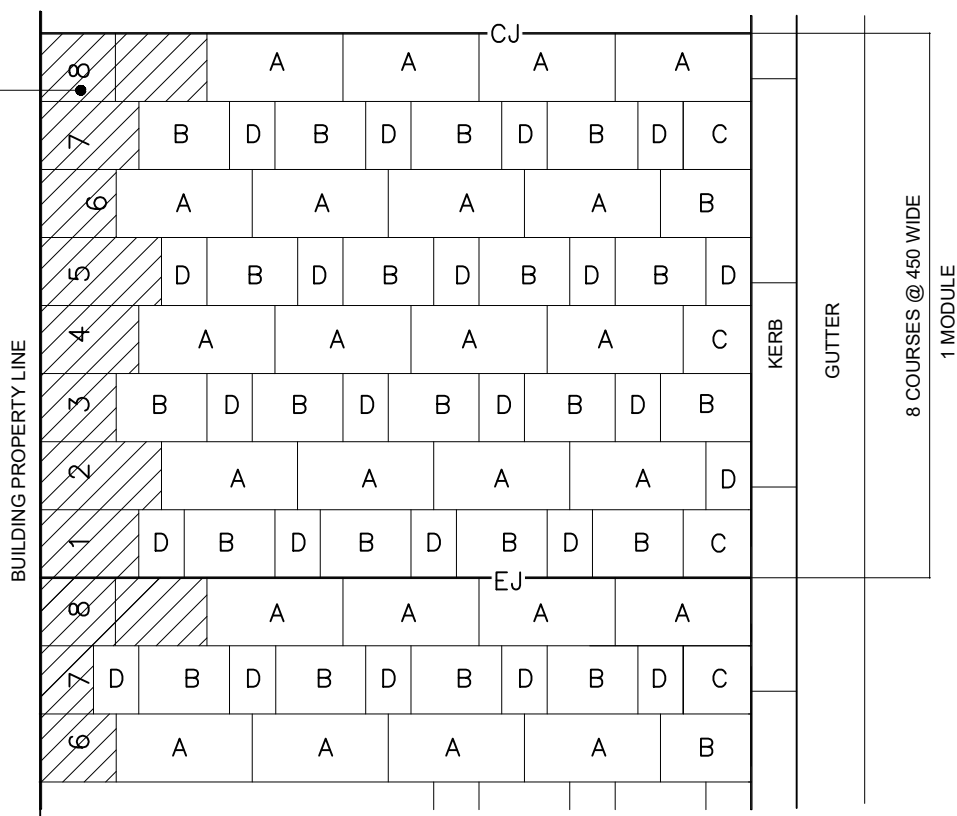
NOTES:

1. The footpath width shown is measured from the property boundary to the back of the kerb.
2. The minimum footpath width must comply with the Sydney Street Code & Inclusive and Accessible Public Domain Guidelines. If existing trees prevent this, the specified minimum width for various scenarios may be applied, with a priority on maximising footpath width.
3. In No Stopping zones, garden beds can be extended or made continuous, provided the clear footpath width complies with the Sydney Street Code and the section of the street is not serviced by a refuse vehicle.
4. Greening should be planned in consultation with the Infrastructure Services, City's Urban Forest Team, Waste Collection Team and City Greening and Leisure Operations to ensure that greening is viable, meets operational needs and provides direct access to kerb where required
5. Garden beds shall have a minimum depth of 500mm. If this depth cannot be achieved or if an awning is present, consultation with the Parks Team is required to ensure an appropriate design and planting solution is implemented.
6. In locations where a minimum 2.3m continuous and accessible path of travel is available and access to kerb is not required, then continuous planting may be considered



PLAN 1:100

CLOSURE UNITS AGAINST BUILDING / PROPERTY LINE VARY IN LENGTH AS DETERMINED BY PATTERN. MINIMUM UNIT LENGTH OF 300mm. NON-STANDARD CUT PAVERS



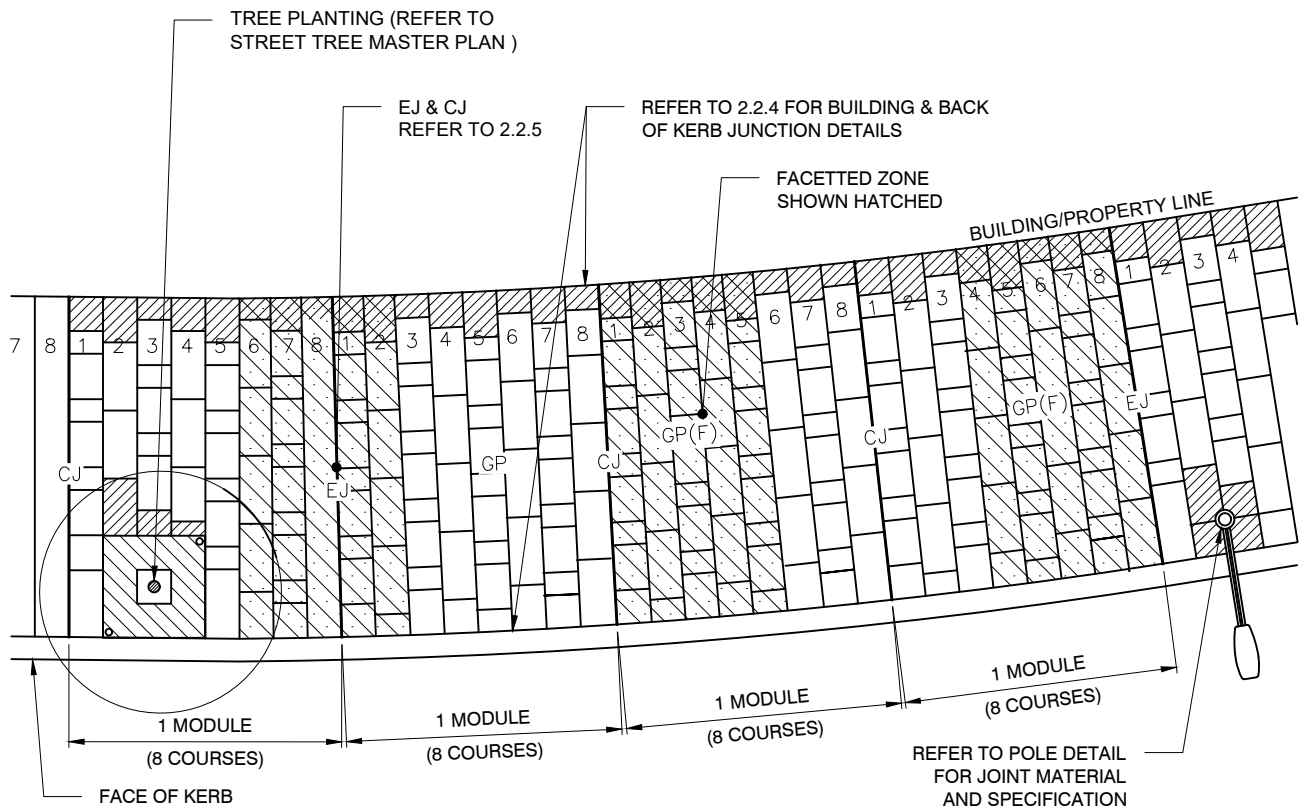
TYPICAL PAVING MODULE 1:50

NOTES:

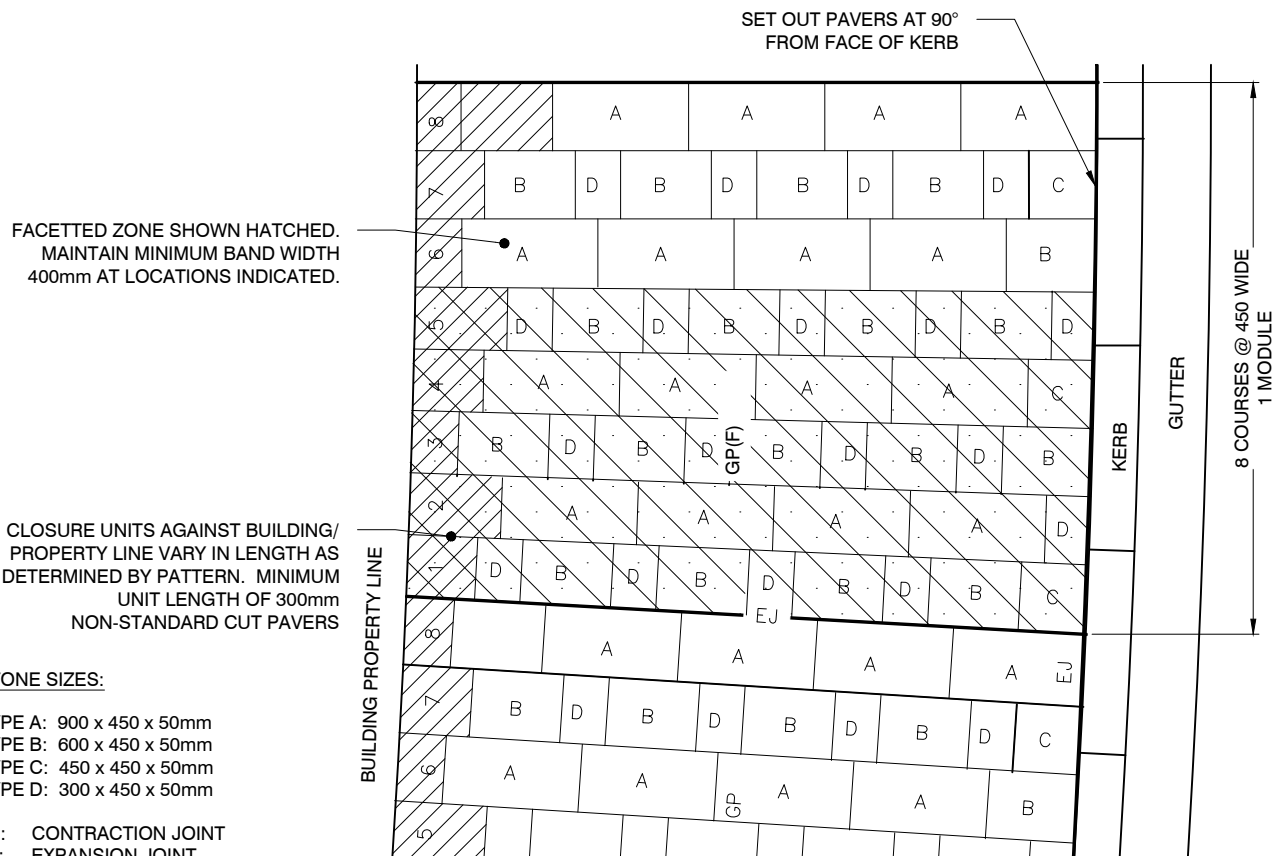
1. MAINTAIN PAVER BAND WIDTH OF 450 mm EXCEPT WHERE REQUIRED TO ADJUST PAVERS TO SUIT SITE CONDITIONS, THEN MIN 400 mm BAND WIDTH OVER MIN 5 COURSES IS ACCEPTABLE.
2. 1-3 mm GAP REQUIRED BETWEEN PAVERS.
3. 60 mm THICK PAVERS WILL BE REQUIRED IN DISTINCTIVE PLACES AS SPECIFIED (REFER TO FOOTWAYS SPECIFICATION FOR FURTHER DETAILS)
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

STONE SIZES: TYPE A 900x450x50
TYPE B 600x450x50
TYPE C 450x450x50
TYPE D 300x450x50

CJ CONTRACTION JOINT
EJ EXPANSION JOINT



PLAN 1:100



STONE SIZES:

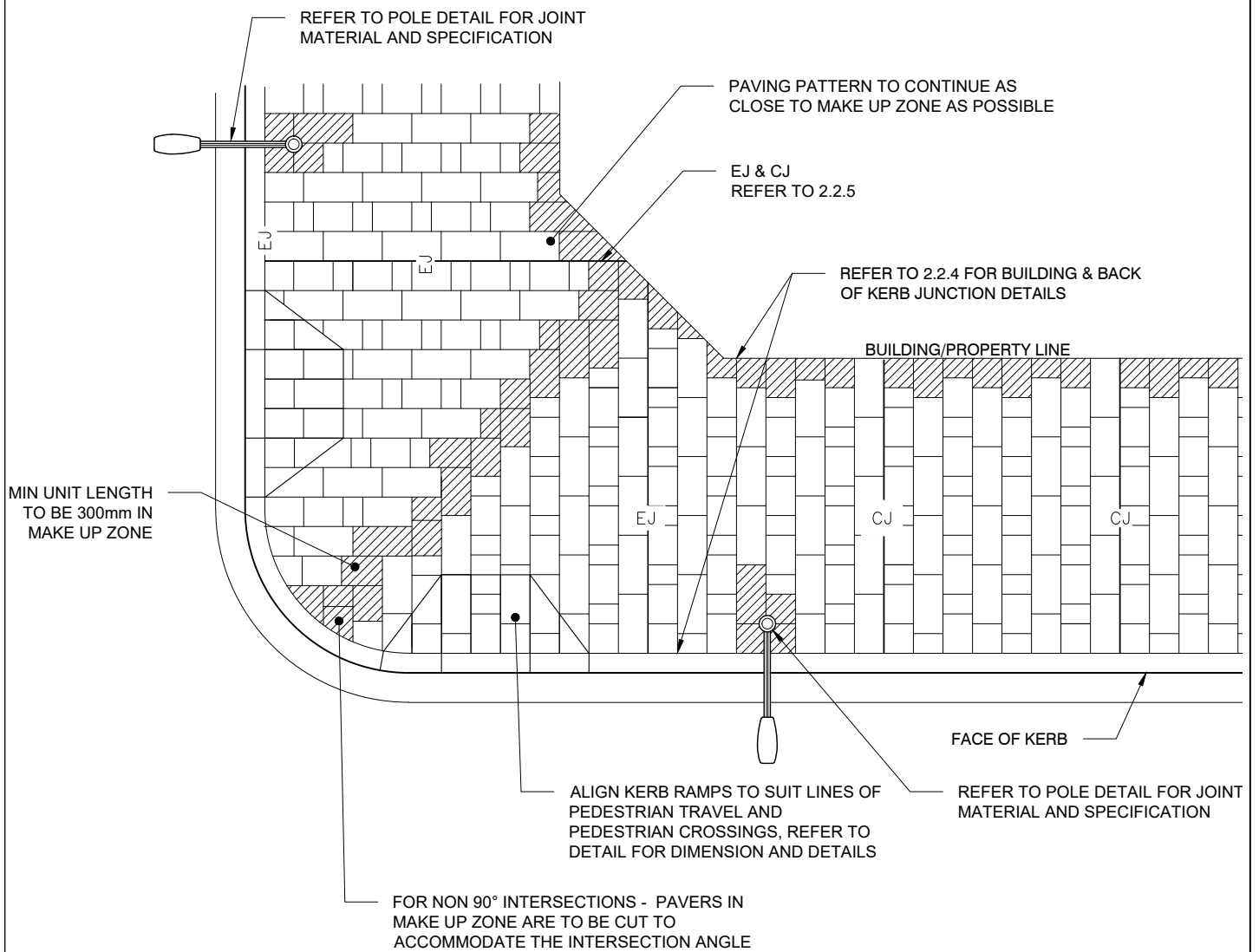
TYPE A: 900 x 450 x 50mm
 TYPE B: 600 x 450 x 50mm
 TYPE C: 450 x 450 x 50mm
 TYPE D: 300 x 450 x 50mm

CJ: CONTRACTION JOINT
 EJ: EXPANSION JOINT
 GP: GRANITE PAVING
 GP(F): GRANITE PAVING FACETTED

NOTES:

- 1-3mm GAP REQUIRED BETWEEN PAVERS.
- 60MM THICK PAVERS WILL BE REQUIRED IN DISTINCTIVE PLACES AS SPECIFIED (REFER TO FOOTWAYS SPECIFICATION FOR FURTHER DETAILS)
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

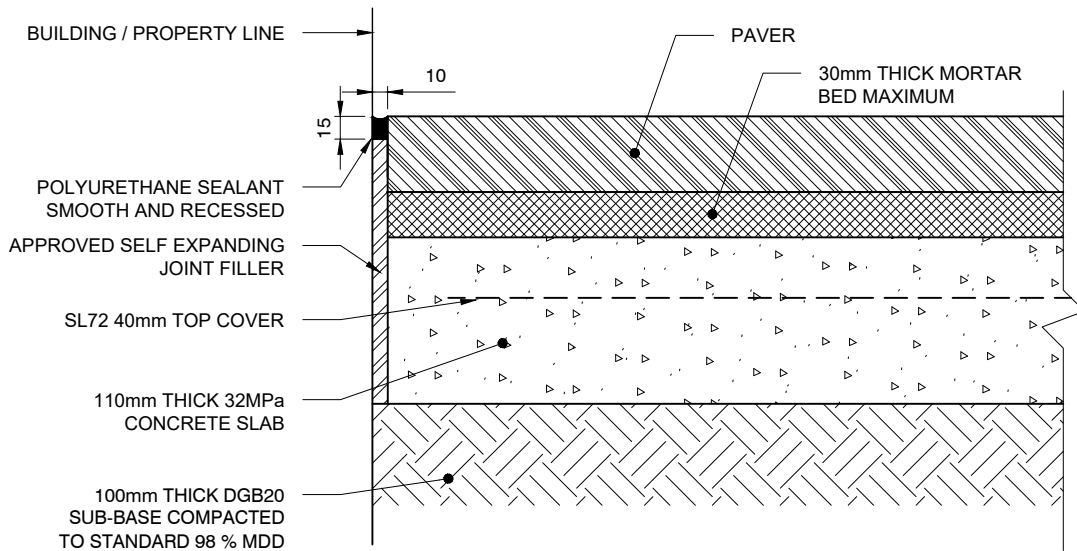
TYPICAL PAVING MODULE 1:50



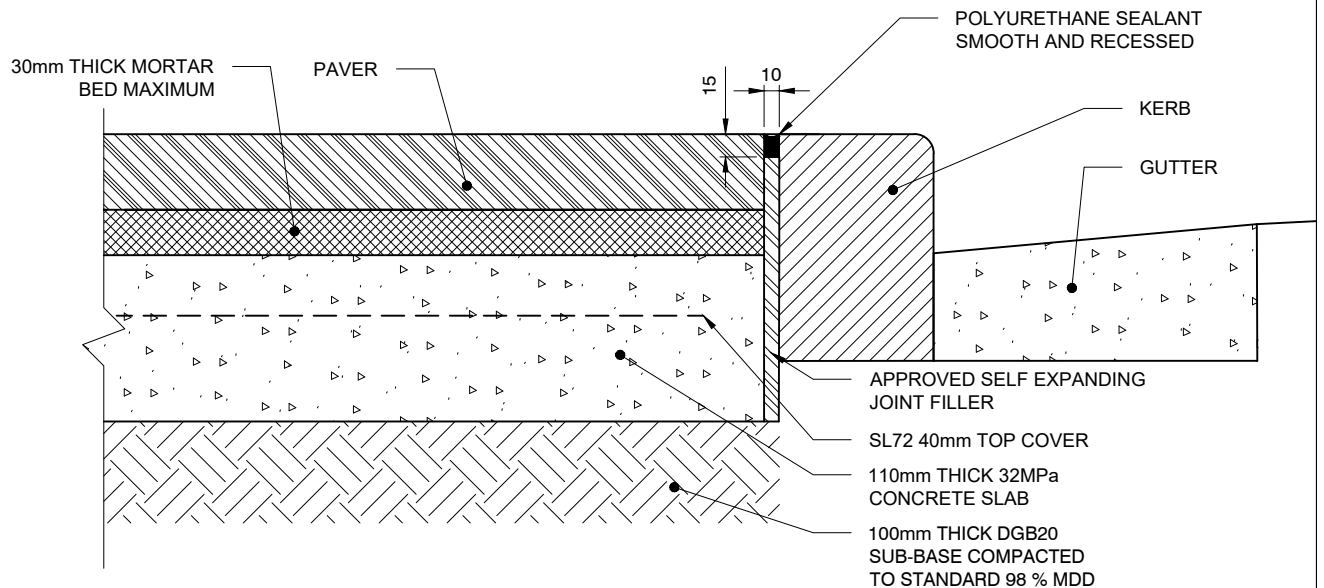
PLAN 1:100

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

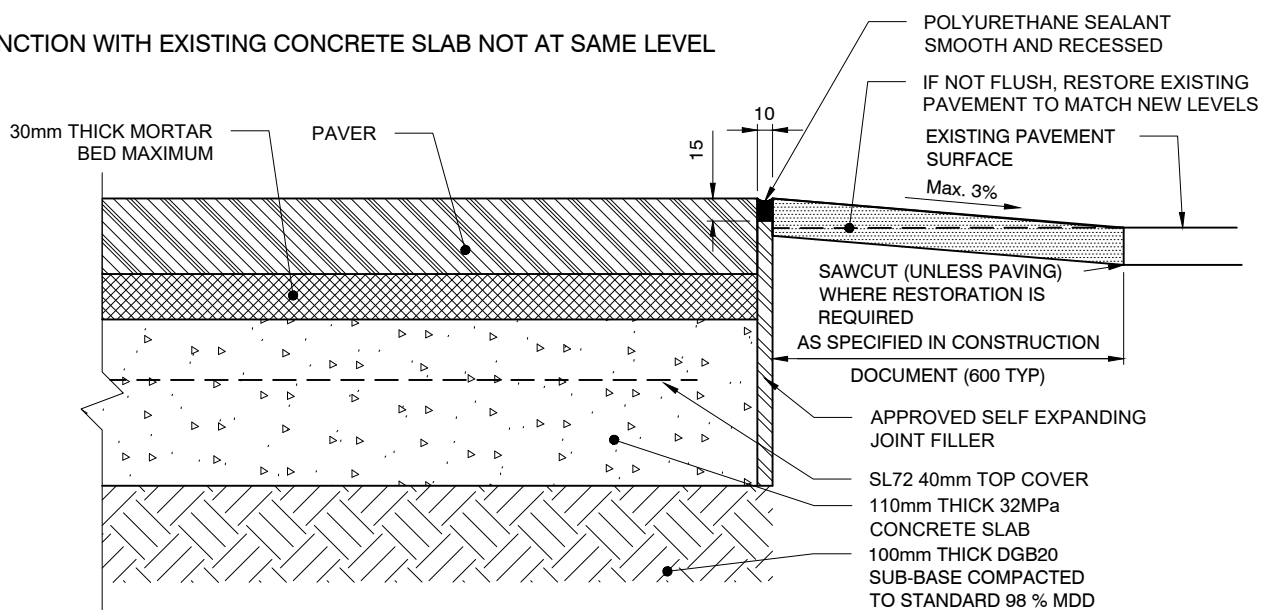
JUNCTION WITH BUILDING



JUNCTION WITH BACK OF KERB



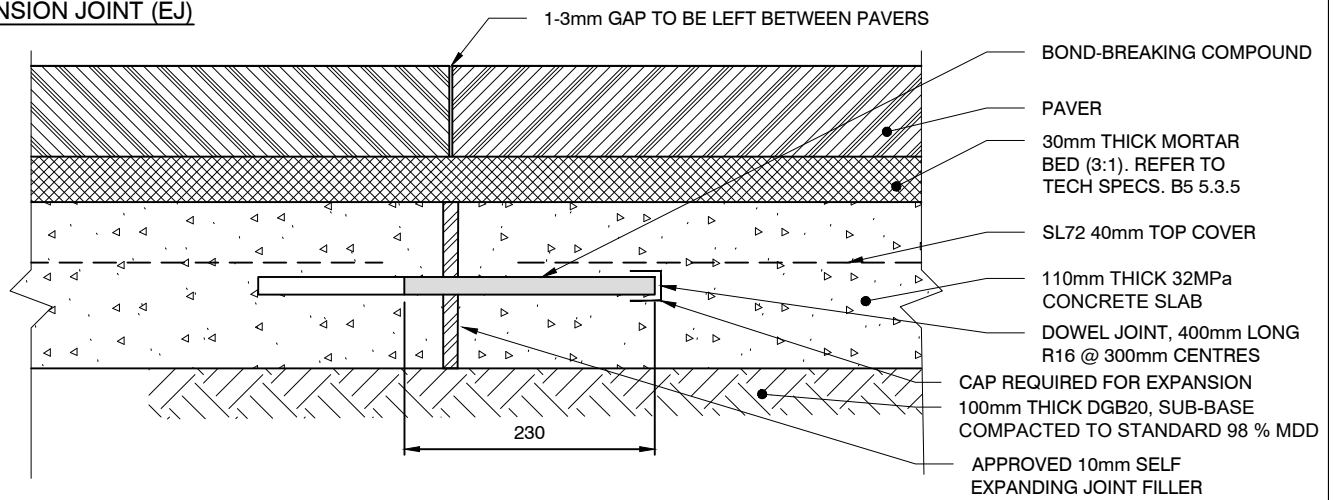
JUNCTION WITH EXISTING CONCRETE SLAB NOT AT SAME LEVEL



NOTES:
1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

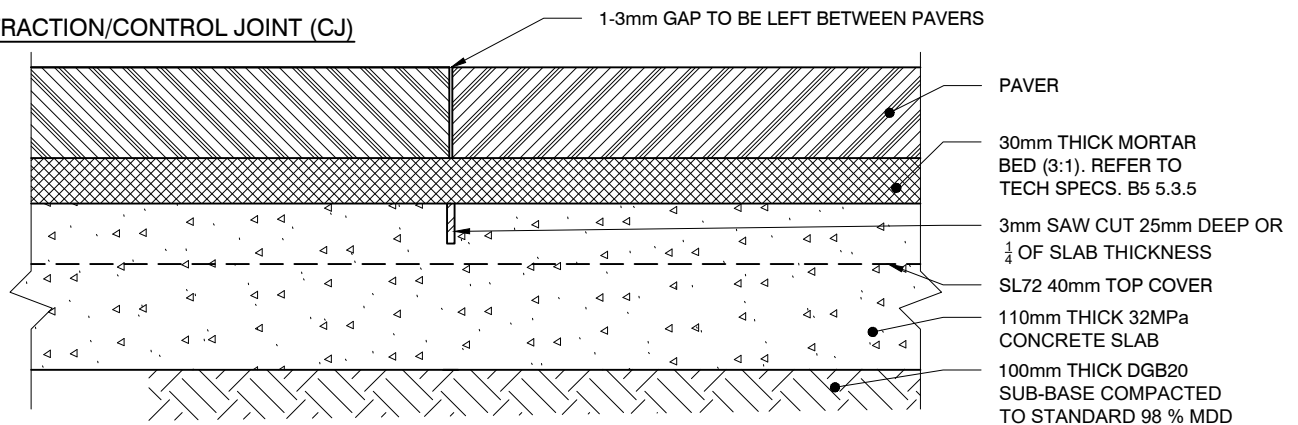
SECTION 1:5

EXPANSION JOINT (EJ)



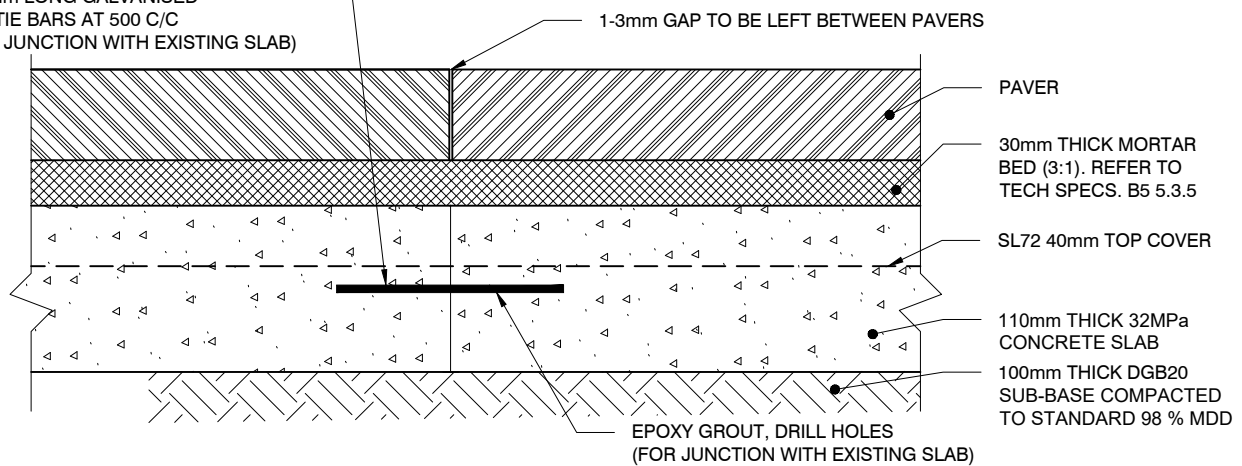
NOTE: BOND-BREAKING COMPONENT AND END CAP MAY BE REPLACED WITH A PURPOSE-MADE DOWEL SLEEVE.

CONTRACTION/CONTROL JOINT (CJ)

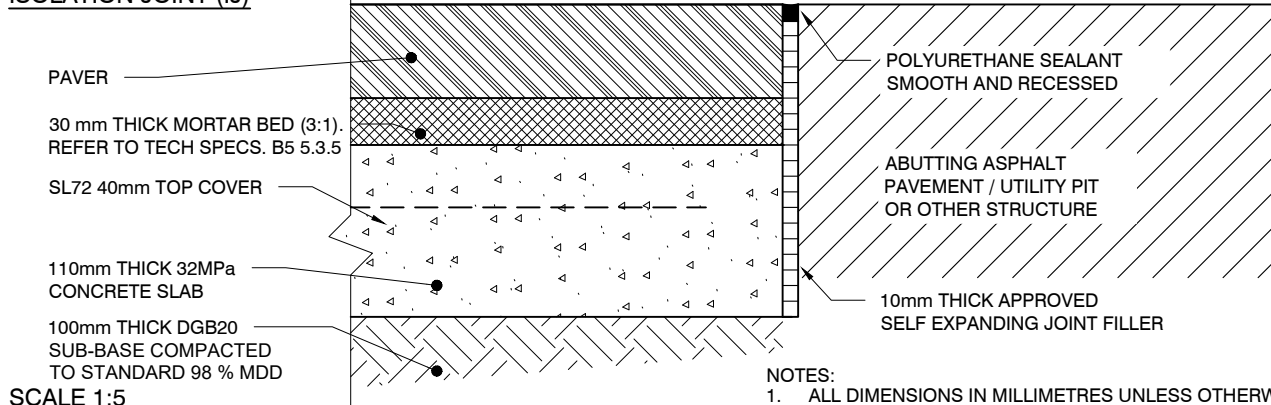


CONSTRUCTION JOINT / JUNCTION WITH EXISTING CONCRETE SLAB

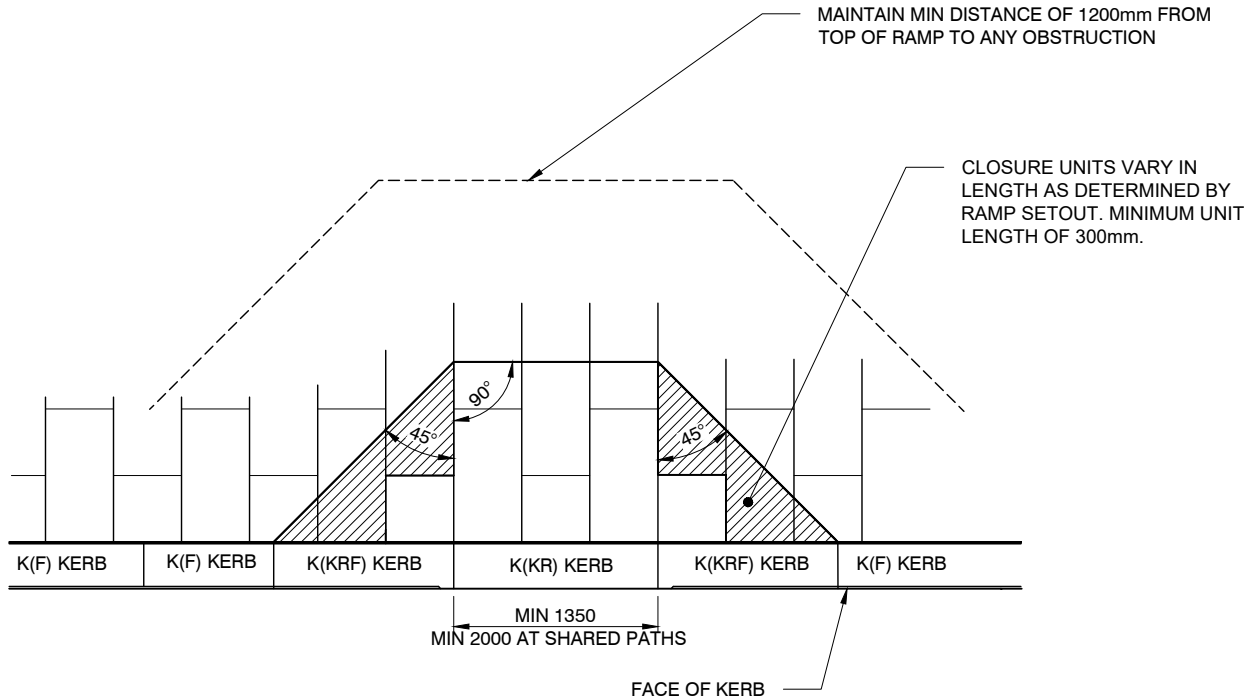
600mm LONG GALVANISED N12 TIE BARS AT 500 C/C (FOR JUNCTION WITH EXISTING SLAB)



ISOLATION JOINT (IJ)



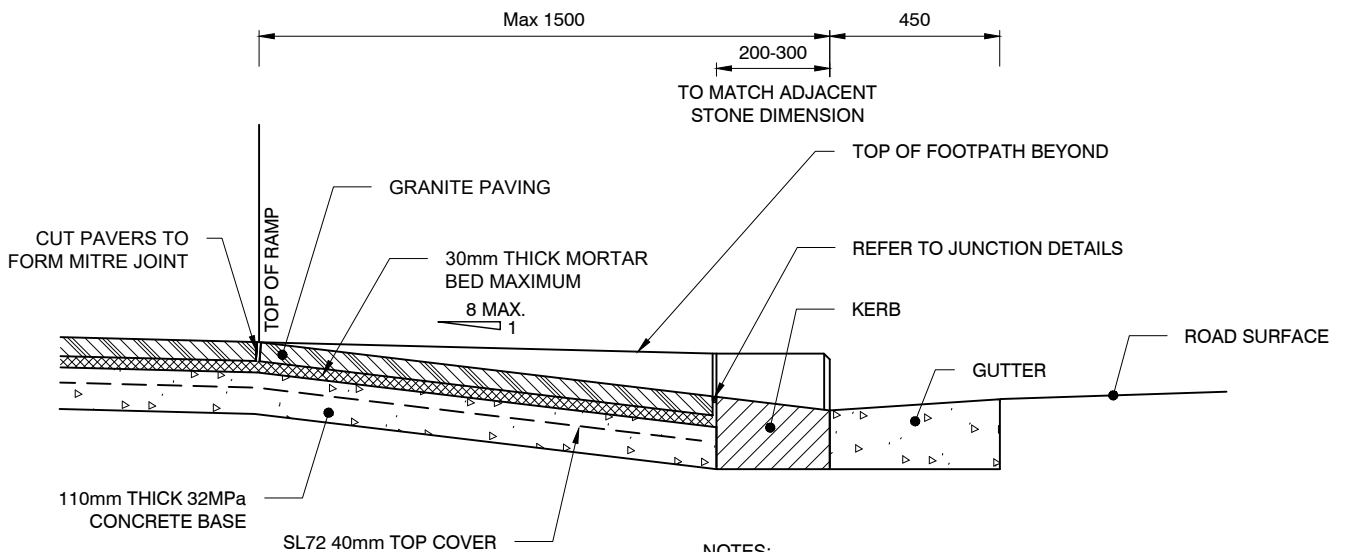
NOTES:
1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



KERB TYPES:

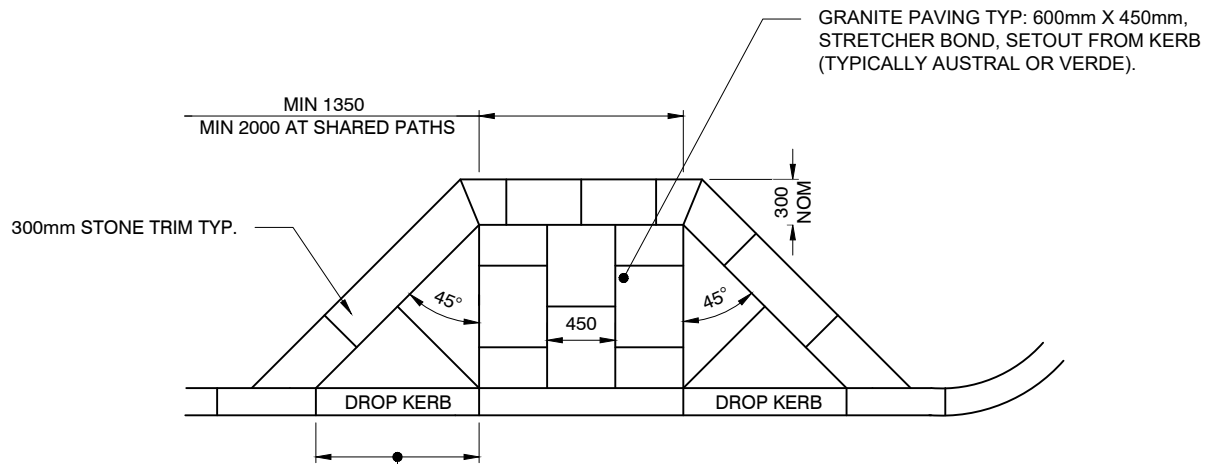
TYPE K(F): FULL HEIGHT
 TYPE K(KRF): PEDESTRIAN CHAMFERED TO FALL
 TYPE K(KR): PEDESTRIAN CROSSOVER

PLAN 1:50



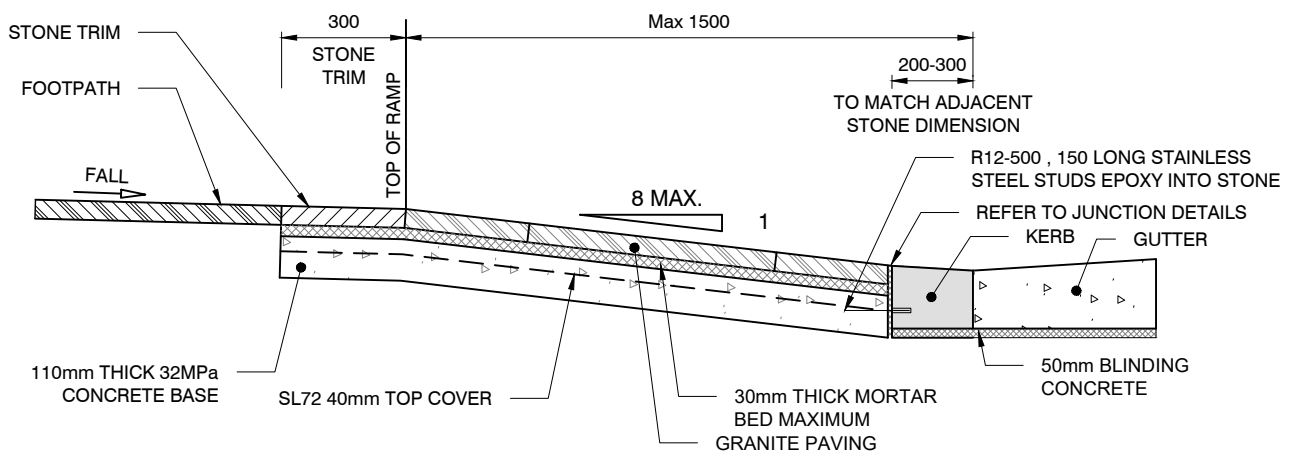
- NOTES:
1. SUB-BASE SHALL BE 100mm THICK DGB20.
 2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

SECTION 1:20



NOTE: 750mm MIN. WHERE RESTRICTED BY SITE CONDITION (TO AUTHORITY APPROVAL)

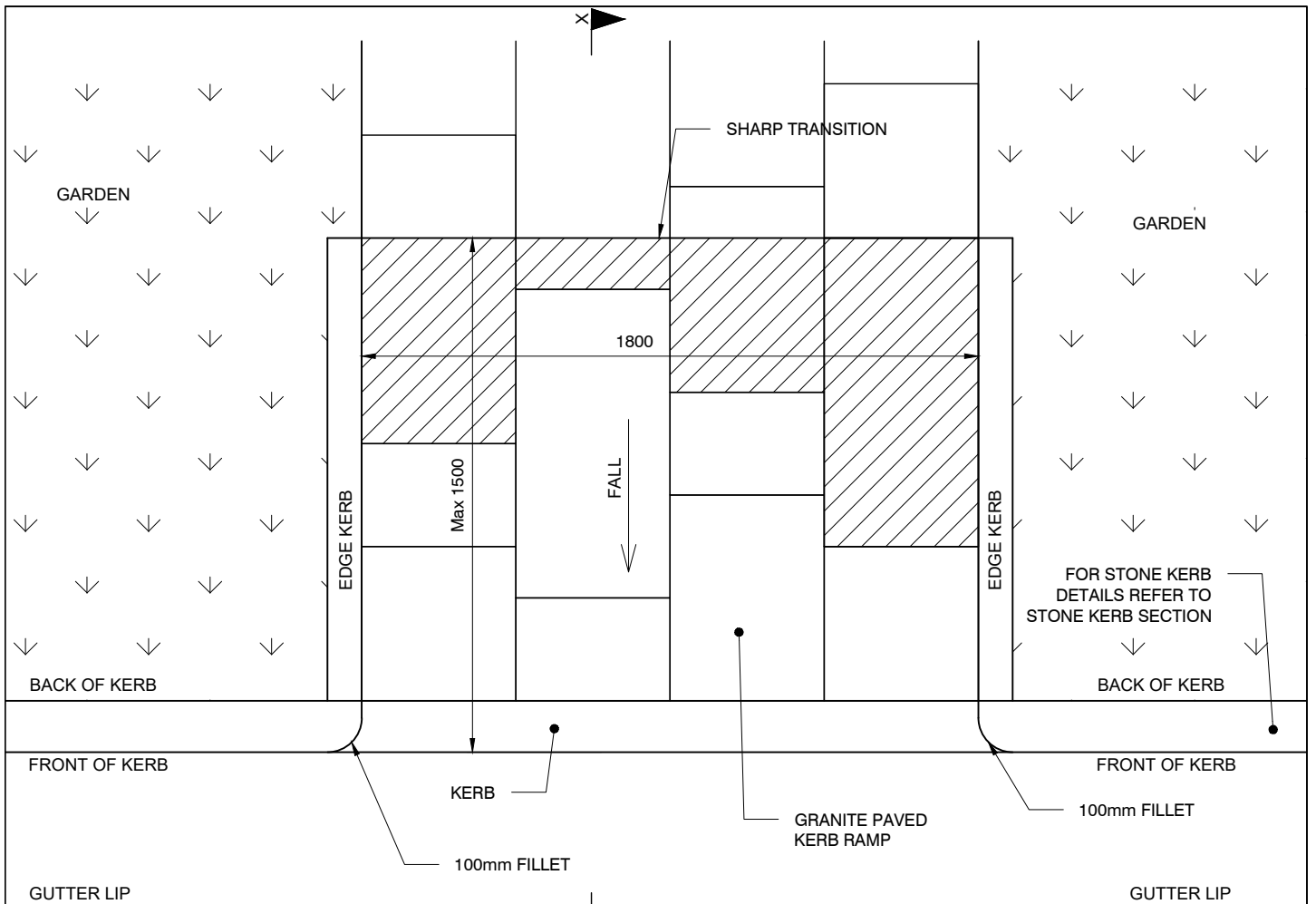
PLAN 1:50



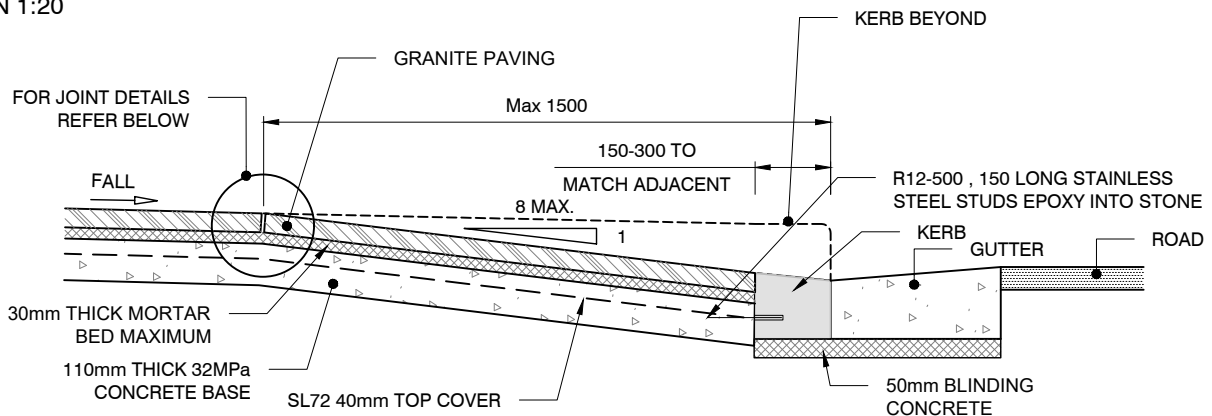
SECTION 1:20

NOTES:

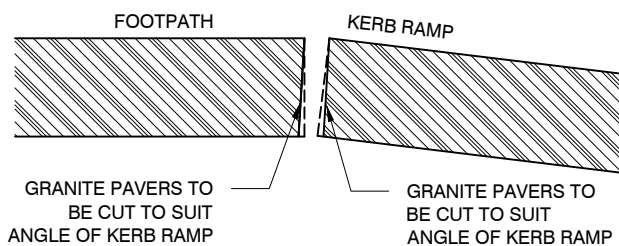
1. ONLY TO BE USED IN COUNCIL APPROVED LOCATIONS.
2. SUB-BASE SHALL BE 100mm THICK DGB20.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN 1:20



SECTION X-X 1:20



DETAIL 1:5

NOTES:

1. ONLY TO BE USED IN COUNCIL APPROVED LOCATIONS.
2. SUB-BASE SHALL BE 100mm THICK DGB20.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

GRANITE FOOTPATH PAVING

VARIES

VARIES TO SUIT
PAVING PATTERN

KERB

GUTTER

SAWCUT PAVING TO
MATCH EDGE OF POLE

PLAN 1:20

POLYURETHANE SEALENT
SMOOTH AND RECESSED

STONE PAVING

30mm THICK MORTAR BED

110mm THICK 32MPa
CONCRETE SLAB

APPROVED SELF EXPANDING
JOINT FILLER

100mm THICK DGB20
SUB-BASE COMPACTED
TO STANDARD 98 % MDD

600 MIN (MIN 1000
FOR TRAFFIC SIGNALS)

VARIES

KERB

GUTTER

WEAK CEMENT
MORTAR 1:5 MIX

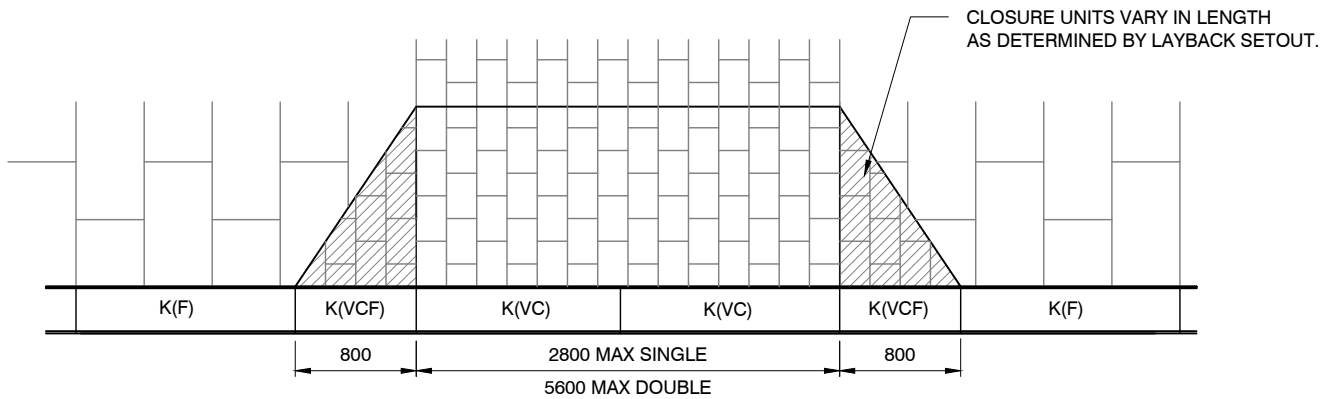
NON SHRINK GROUT

POWER POLE OR
SMART POLE

ELEVATION 1:20

NOTES:

1. SUB-GRADE SHALL BE COMPACTED TO MINIMUM 4% CBR
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN

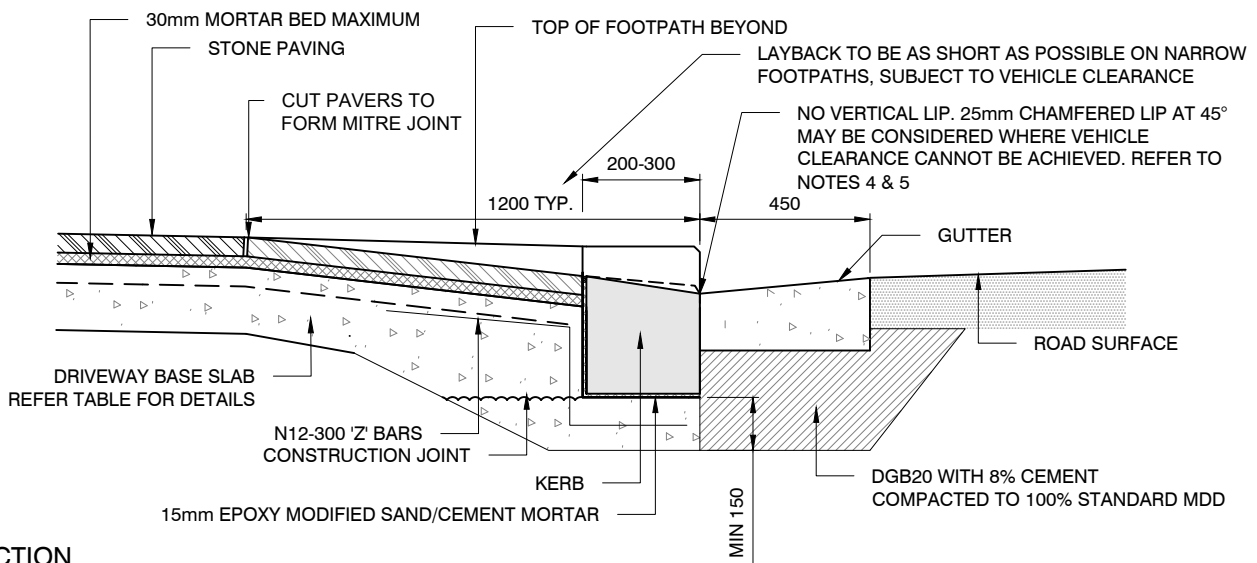
1:50

NOTES:

1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE UNLESS APPROVED OTHERWISE
3. FOR NARROW FOOTPATHS, LENGTH OF RAMP TO BE REDUCED TO 900mm SUBJECT TO VEHICLE CLEARANCE
4. VERTICAL AND HORIZONTAL CLEARANCE SHALL BE CHECKED BY THE DESIGNER IN ACCORDANCE WITH AS2890.1
5. VERTICAL LIP ADJACENT TO CYCLEWAY MUST BE APPROVED BY CITY'S REPRESENTATIVE
6. SUB-BASE SHALL BE 100mm THICK DGB20.
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

KERB PROFILES

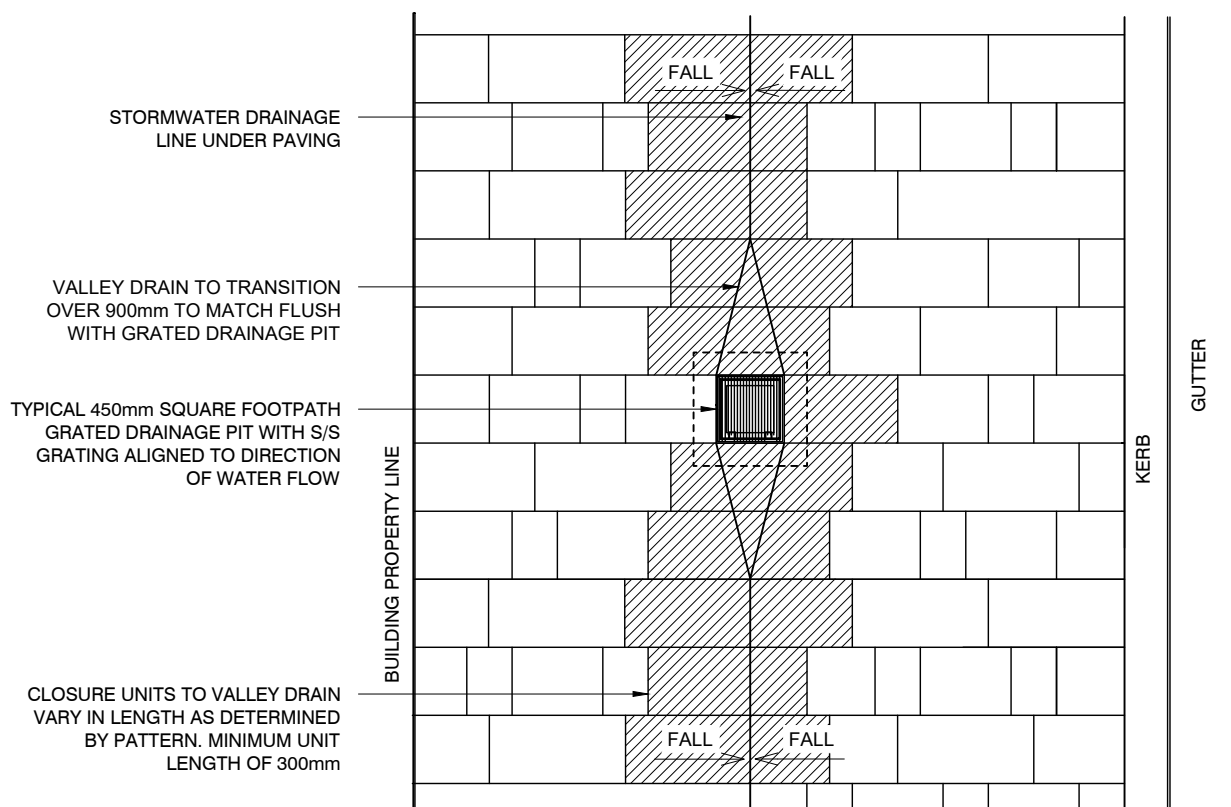
- TYPE K(F): FULL HEIGHT
 TYPE K(VCF): VEHICULAR CHAMFERED TO FALL
 TYPE K(VC): VEHICULAR CROSSOVER



SECTION

1:20

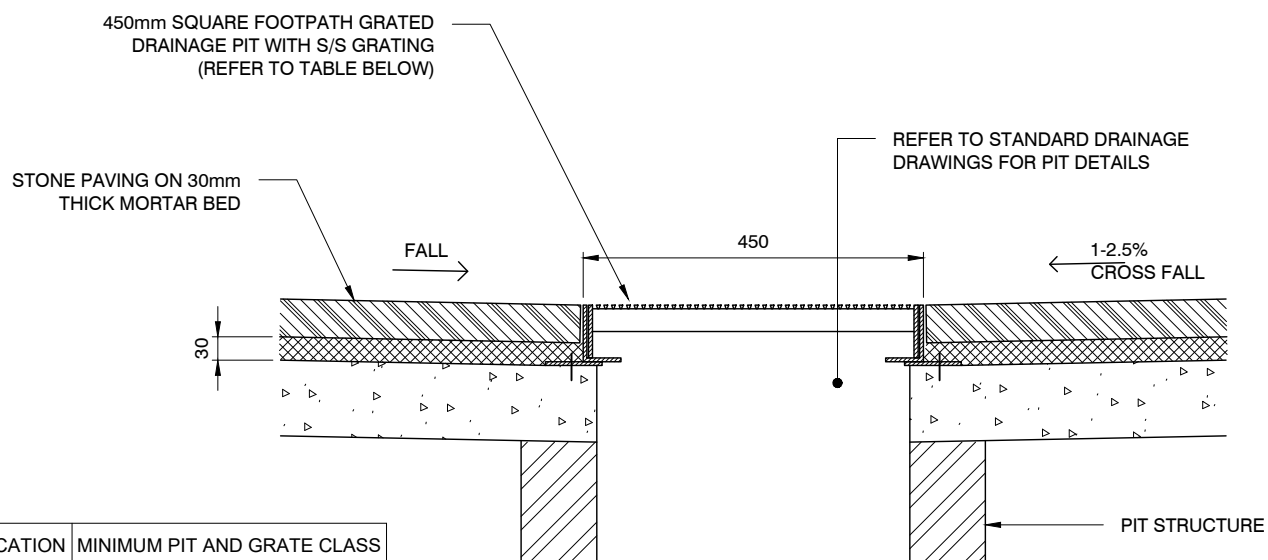
DRIVEWAY SPECIFICATIONS				
DRIVEWAY USE	CONCRETE STRENGTH	THICKNESS	REINFORCEMENT	PAVER SIZE
SINGLE RESIDENTIAL	32MPa	150	SL82, 50 COVER TOP	200 x 300 x 70
MULTI RESIDENTIAL	32MPa	200	SL82, 50 COVER TOP	200 x 300 x 70
COMMERCIAL/ INDUSTRIAL	32MPa	250	TWO LAYERS SL82 50 COVER TOP & BOTTOM	200 x 300 x 80



NOTES:

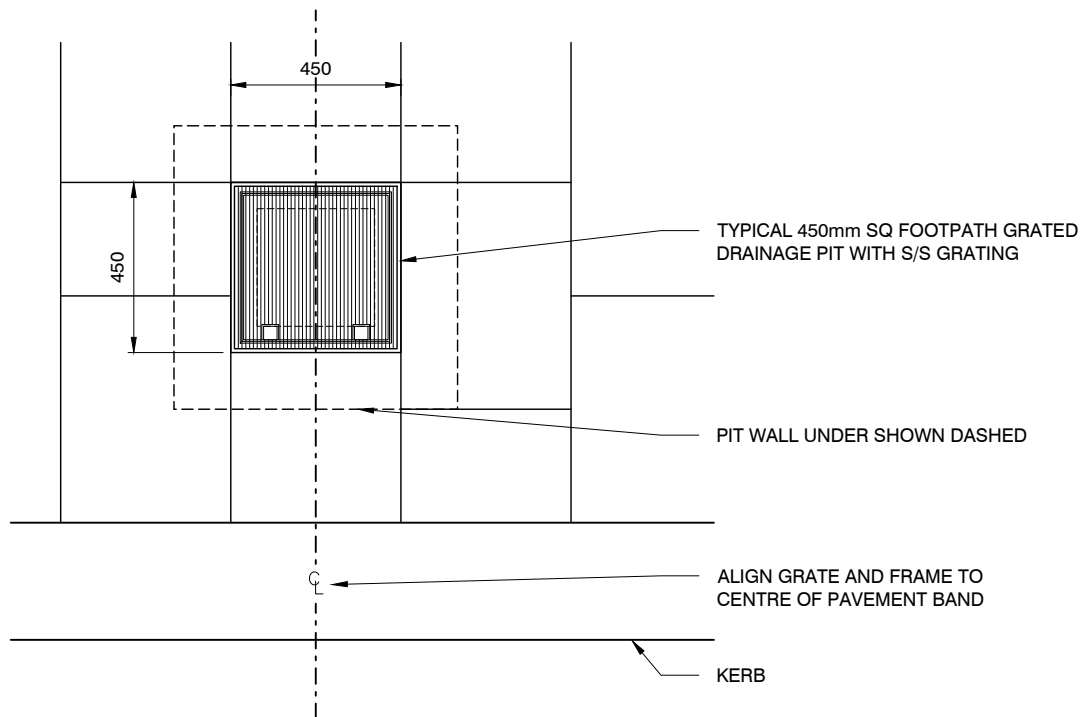
1. ALIGN 450mm SQUARE PIT FRAME TO PAVEMENT BANDING & TO PAVING MODULES AS SHOWN.
2. DETAIL NOT TO BE USED WITHOUT SITE SPECIFIC APPROVAL FROM COUNCIL.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

PLAN 1:50

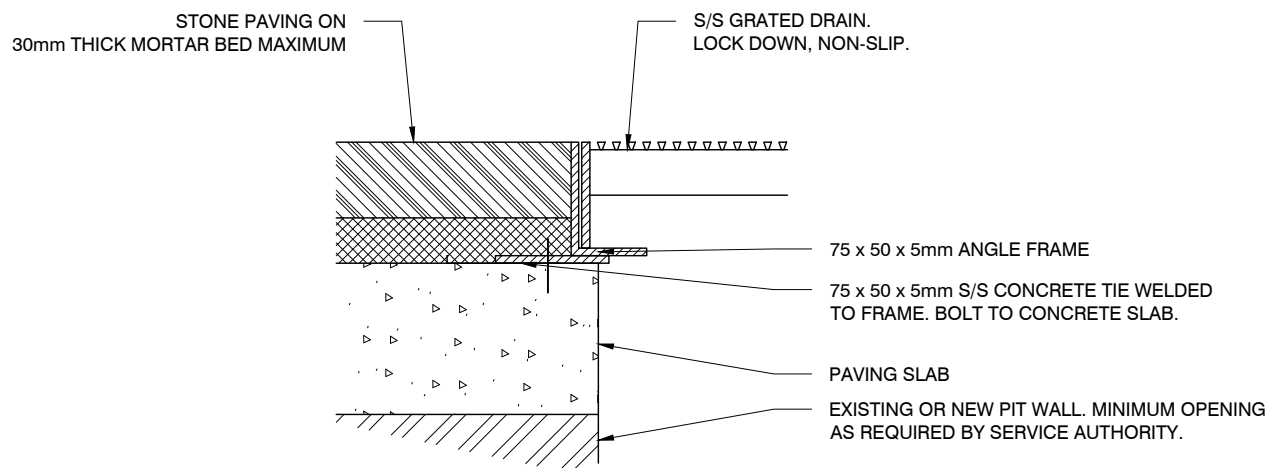


PIT LOCATION	MINIMUM PIT AND GRATE CLASS
FOOTWAY	C (PEDESTRIAN SAFE)
ROADWAY	D (CYCLE SAFE)
DRIVEWAY	D (PEDESTRIAN SAFE)

SECTION 1:10



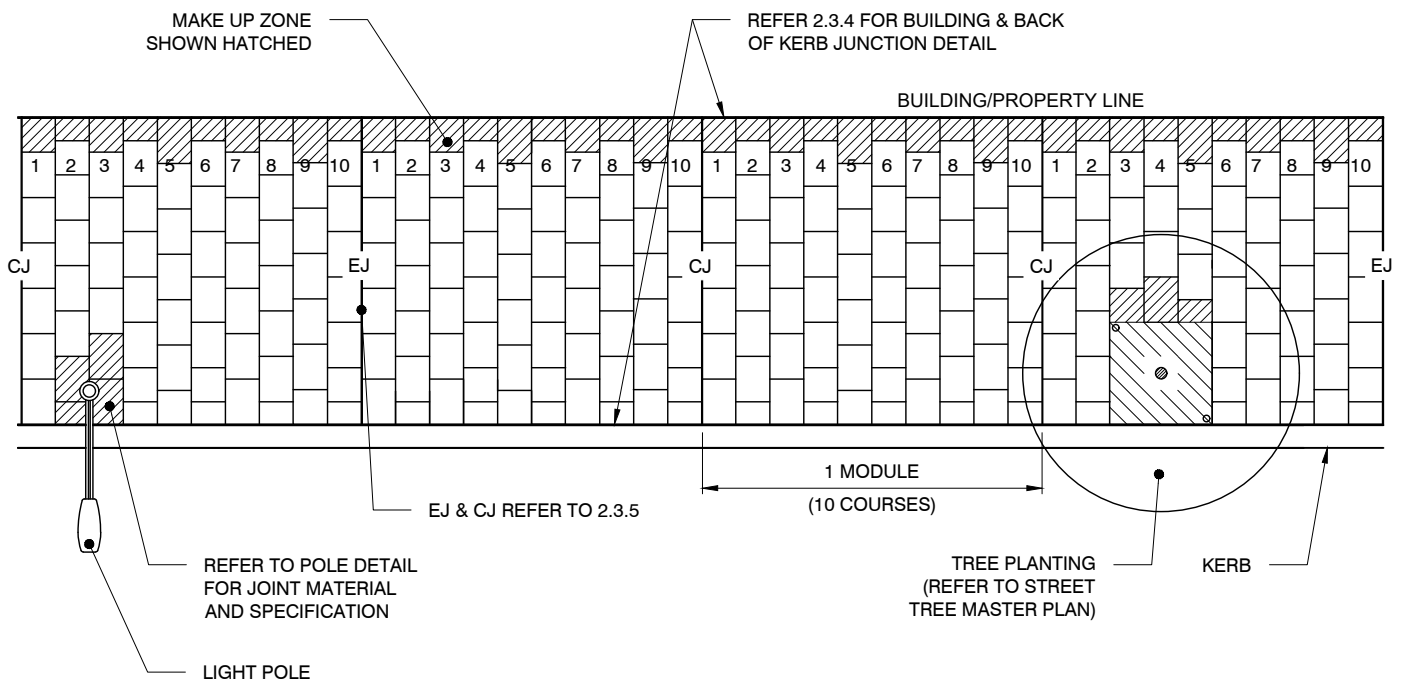
PLAN 1:20



PIT LOCATION	MINIMUM PIT AND GRATE CLASS
FOOTWAY	C (PEDESTRIAN SAFE)
ROADWAY	D (CYCLE SAFE)
DRIVEWAY	D (PEDESTRIAN SAFE)

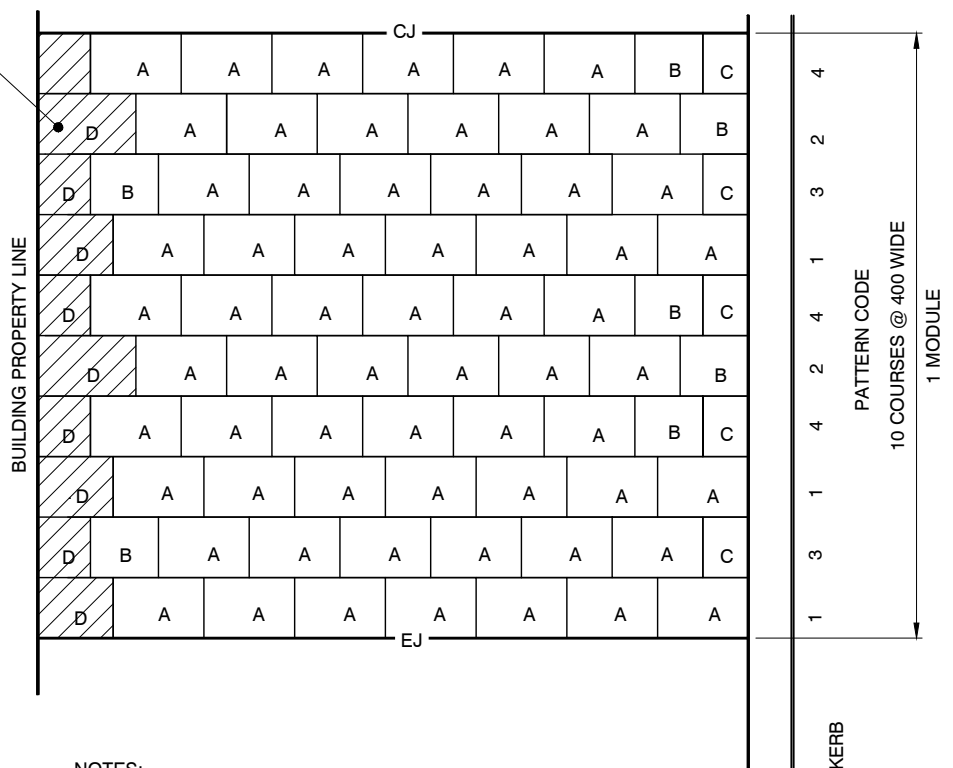
SECTION 1:5

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN 1:100

CLOSURE UNITS AGAINST BUILDING / PROPERTY LINE VARY IN LENGTH AS DETERMINED BY PATTERN. MINIMUM UNIT LENGTH OF 300mm. NON-STANDARD CUT PAVERS



PAVER SIZES

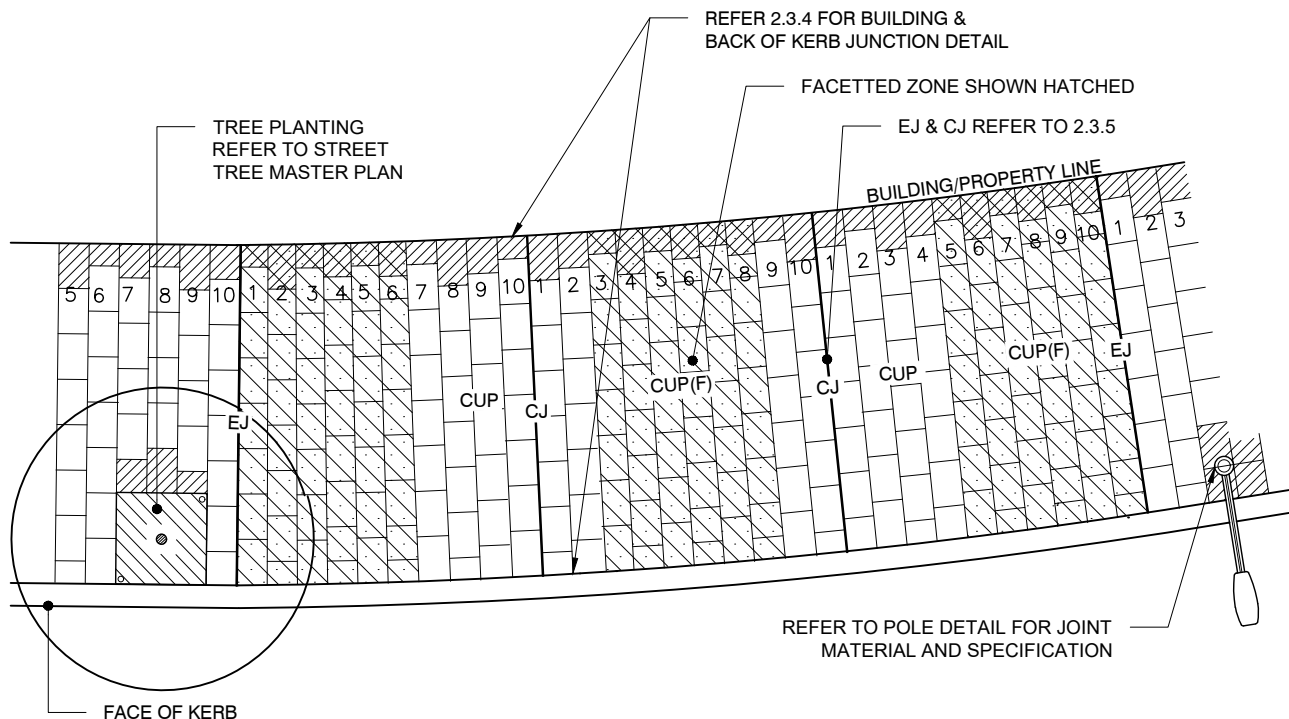
TYPE A: 600 x 400 x 50mm
 TYPE B: 450 x 400 x 50mm
 TYPE C: 300 x 400 x 50mm
 TYPE D: SPECIAL CUT

EJ: EXPANSION JOINT
 CJ: CONTRACTION JOINT

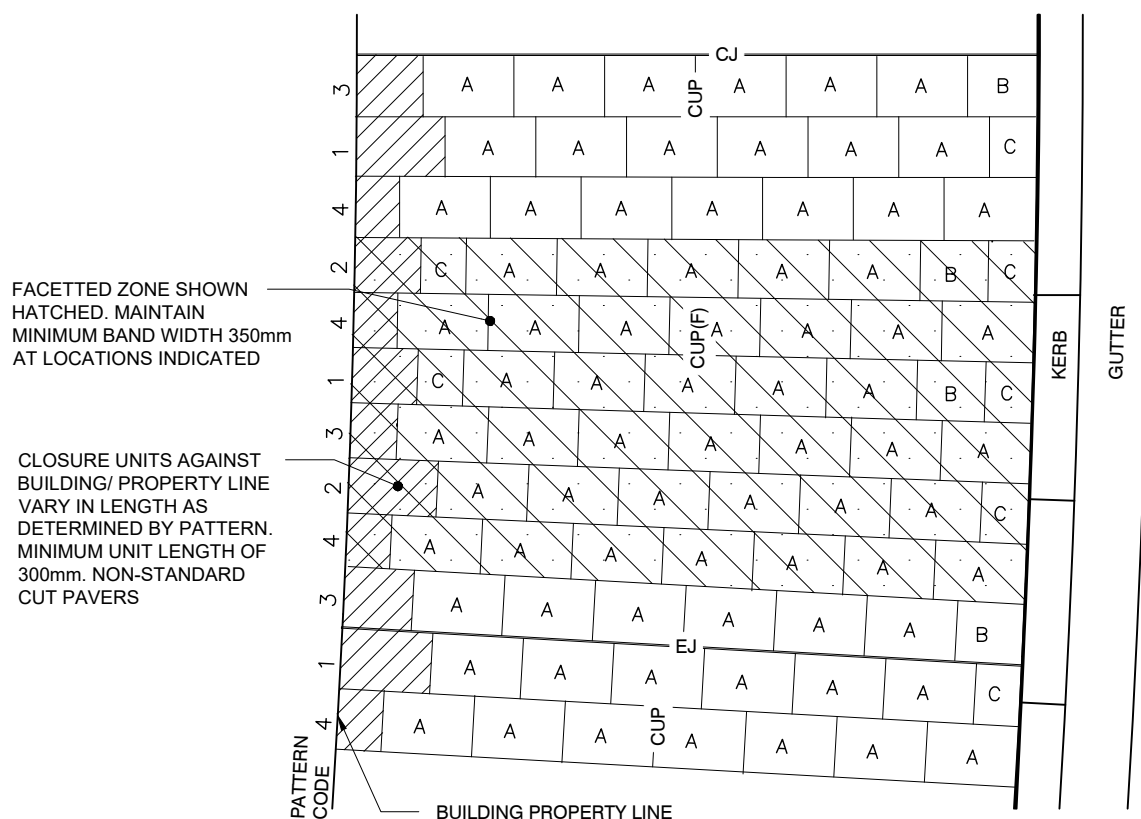
TYPICAL PAVING MODULE 1:50

NOTES:

1. MAINTAIN PAVER BAND WIDTH OF 400mm EXCEPT WHERE REQUIRED TO ADJUST PAVERS TO SUIT SITE CONDITIONS, THEN MIN 350mm BAND WIDTH OVER MIN 5 COURSES IS ACCEPTABLE.
2. 1-3mm GAP REQUIRED BETWEEN PAVERS.
3. PAVERS TO BE SET OUT 90° TO BACK OF KERB.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



PLAN 1:100



PAVER SIZES

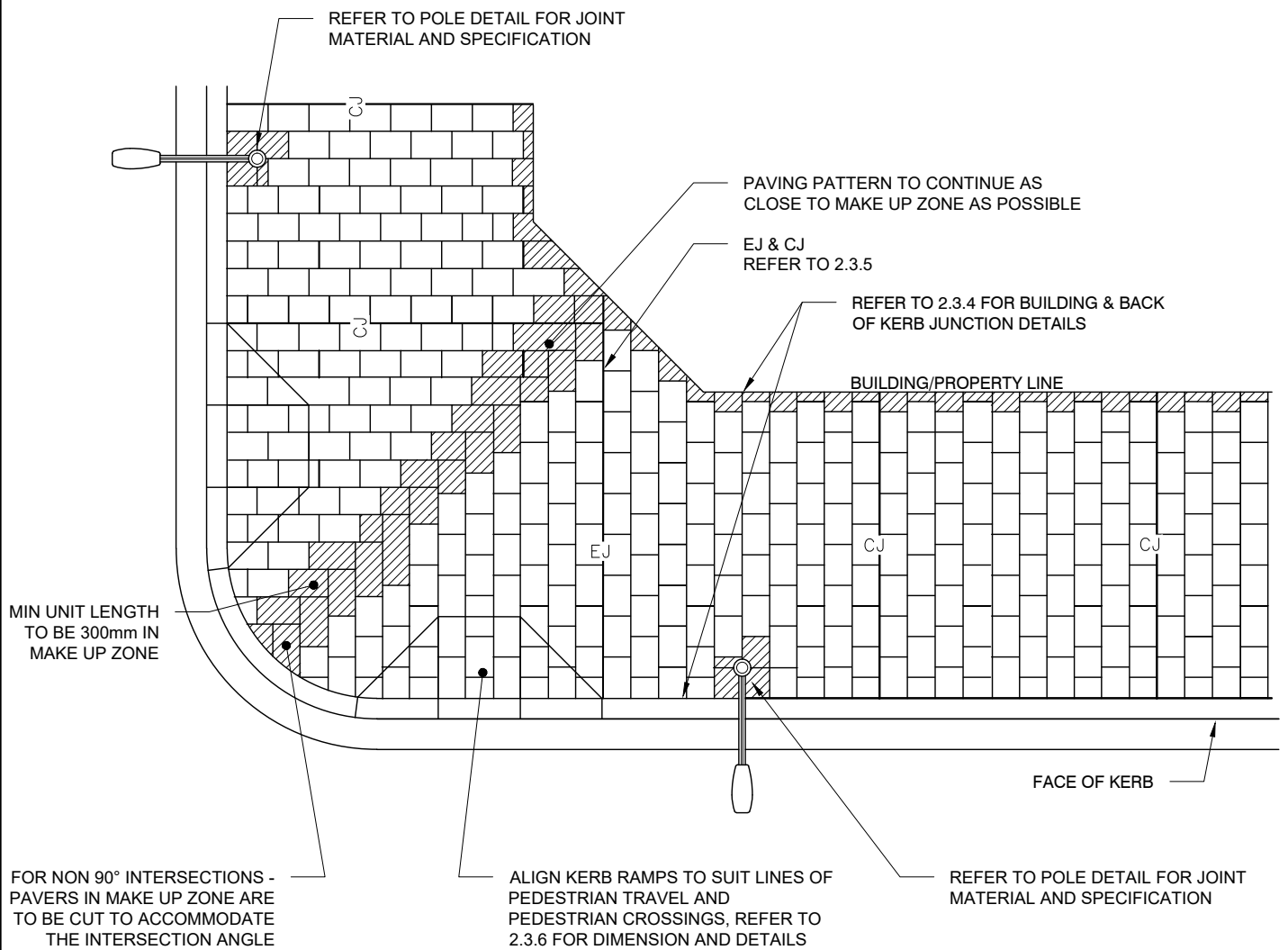
TYPE A: 600 x 400 x 50mm
 TYPE B: 450 x 400 x 50mm
 TYPE C: 300 x 400 x 50mm
 TYPE D: SPECIAL CUT

EJ: EXPANSION JOINT
 CJ: CONTRACTION JOINT
 CUP: CONCRETE UNIT PAVING
 CUP(F): CONCRETE UNIT PAVING (FACETTED)

NOTES:

- 1 - 3 mm GAP REQUIRED BETWEEN PAVERS.
- PAVERS TO BE SET OUT 90° TO BACK OF KERB.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

TYPICAL PAVING MODULE 1:50



PLAN 1:100

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

JUNCTION WITH BUILDING

BUILDING / PROPERTY LINE

POLYURETHANE SEALANT
SMOOTH AND RECESSED

30mm THICK MORTAR BED

APPROVED SELF
EXPANDING JOINT FILLER

SL72 40mm TOP COVER

110mm THICK 32MPa
CONCRETE SLAB

100mm THICK DGB20
SUB-BASE COMPACTED
TO STANDARD 98 % MDD

CONCRETE UNIT PAVING

JUNCTION WITH BACK OF KERB

CONCRETE UNIT PAVING

30mm THICK MORTAR BED

POLYURETHANE SEALANT
SMOOTH AND RECESSED

KERB

GUTTER

SL72 40mm TOP COVER

APPROVED SELF
EXPANDING JOINT FILLER

110mm THICK 32MPa CONCRETE SLAB

100mm THICK DGB20
SUB-BASE COMPACTED
TO STANDARD 98 % MDD

JUNCTION WITH EXISTING CONCRETE SLAB NOT AT SAME LEVEL

CONCRETE UNIT PAVING

POLYURETHANE SEALANT
SMOOTH AND RECESSED

IF NOT FLUSH, RESTORE EXISTING
PAVEMENT TO MATCH NEW LEVELS

EXISTING PAVEMENT
SURFACE

Max. 3%

SAWCUT (UNLESS PAVING)
WHERE RESTORATION IS
REQUIRED
AS SPECIFIED IN CONSTRUCTION

DOCUMENT (600 TYP)

APPROVED SELF EXPANDING
JOINT FILLER

SL72 40mm TOP COVER

110mm THICK 32MPa
CONCRETE SLAB

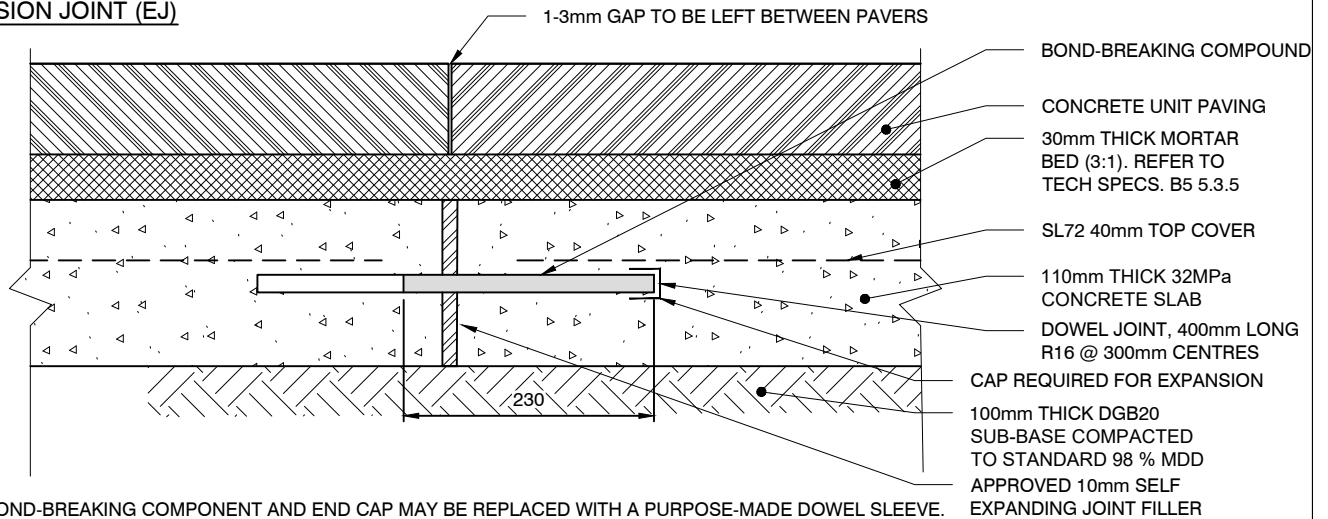
100mm THICK DGB20
SUB-BASE COMPACTED
TO STANDARD 98 % MDD

NOTES:

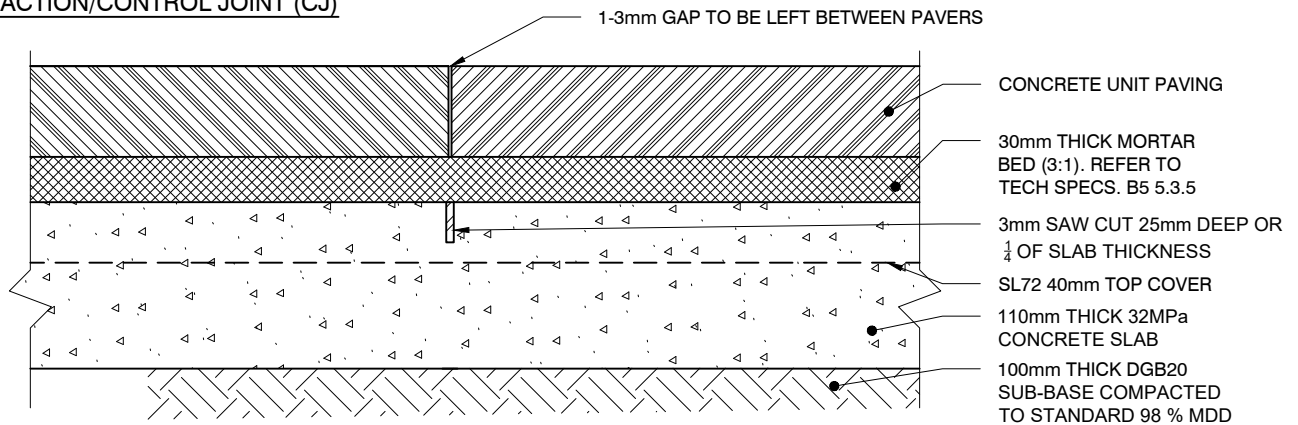
1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

SECTION 1:5

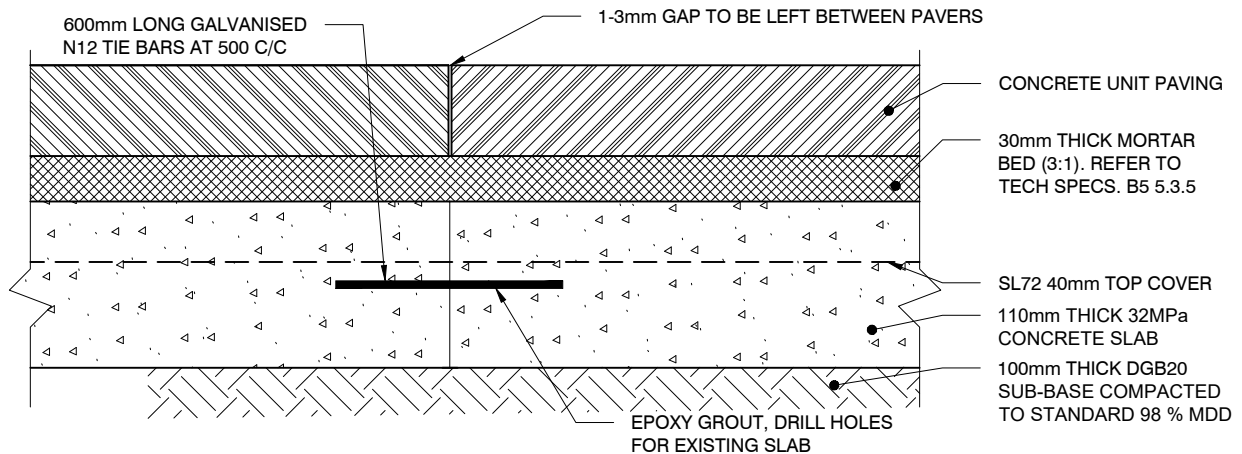
EXPANSION JOINT (EJ)



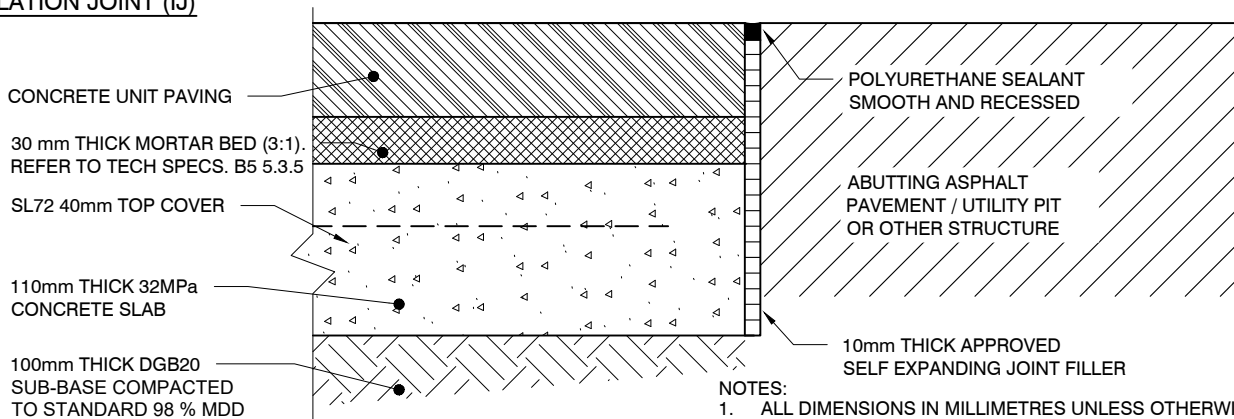
CONTRACTION/CONTROL JOINT (CJ)

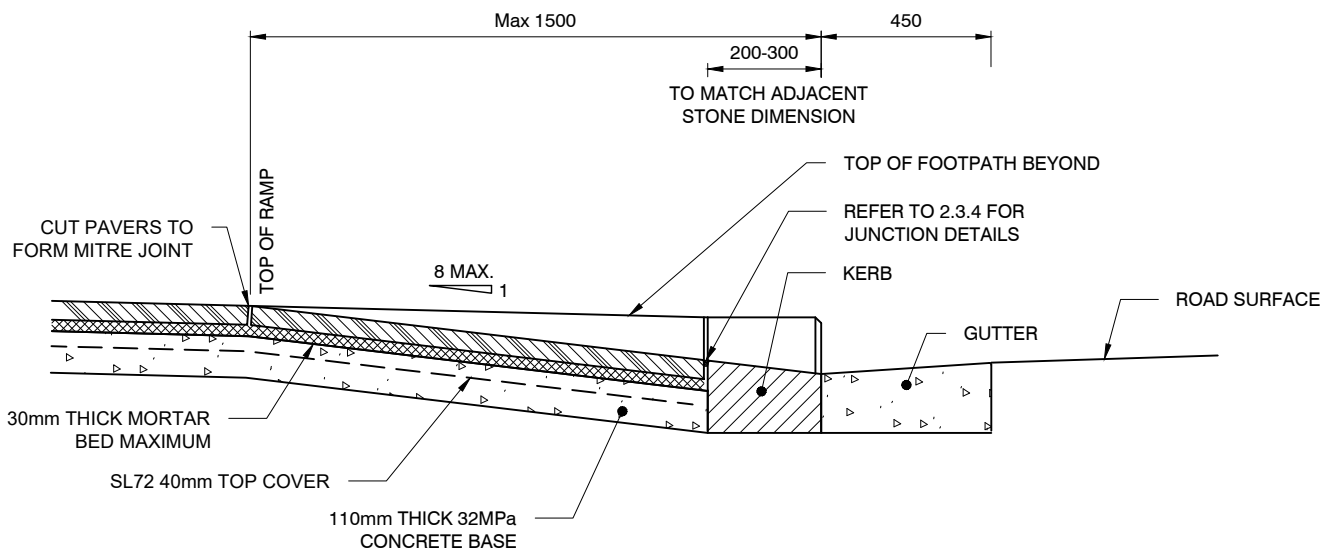
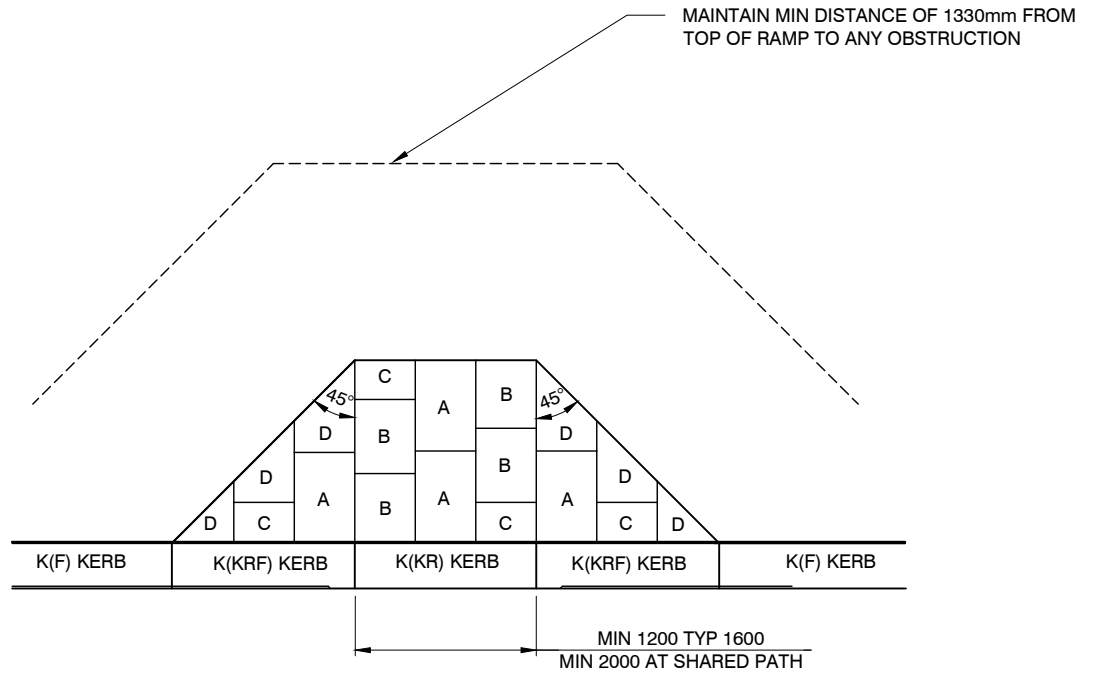


CONSTRUCTION JOINT / JUNCTION WITH EXISTING CONCRETE SLAB

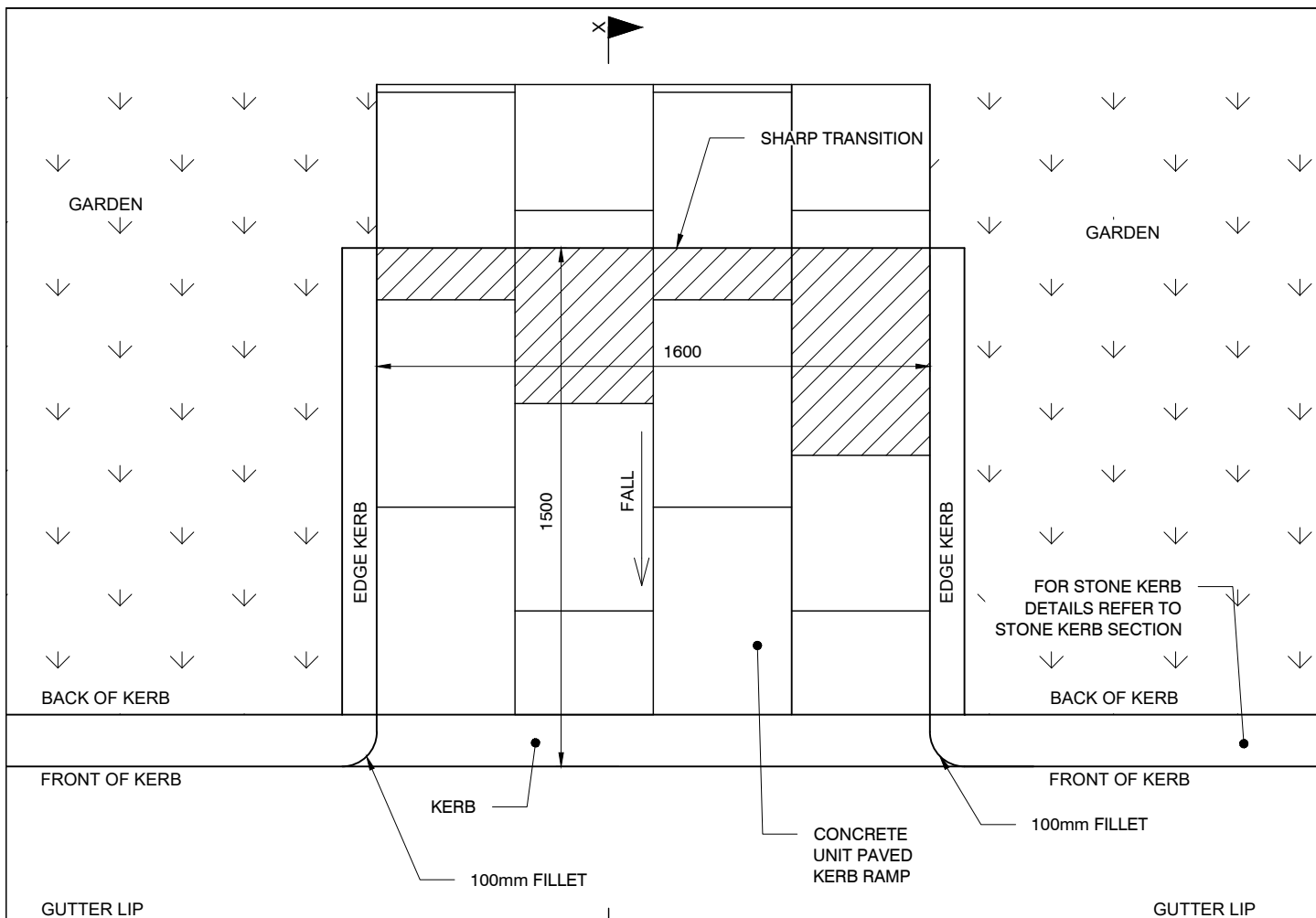


ISOLATION JOINT (IJ)





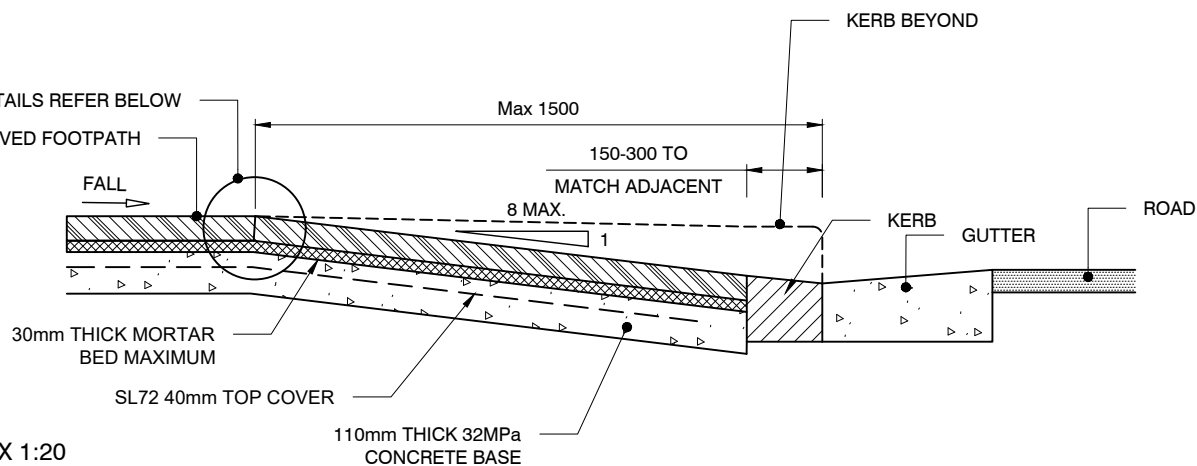
- NOTES:
- SUB-BASE SHALL BE 100mm THICK DGB20.
 - ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



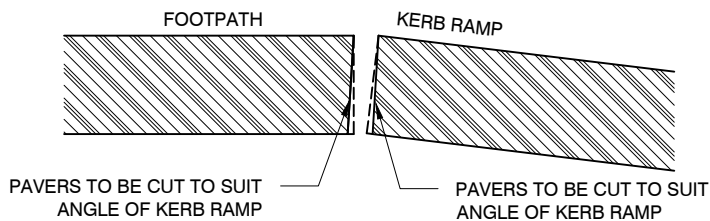
PLAN 1:20

FOR JOINT DETAILS REFER BELOW

CONCRETE PAVED FOOTPATH



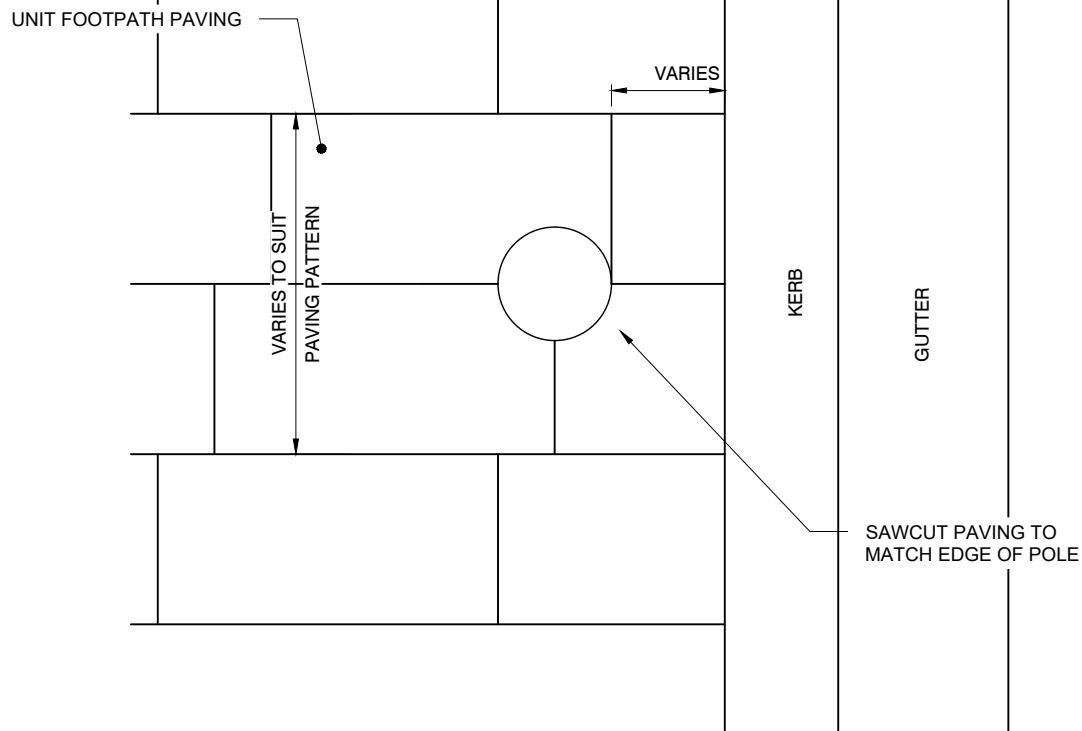
SECTION X-X 1:20



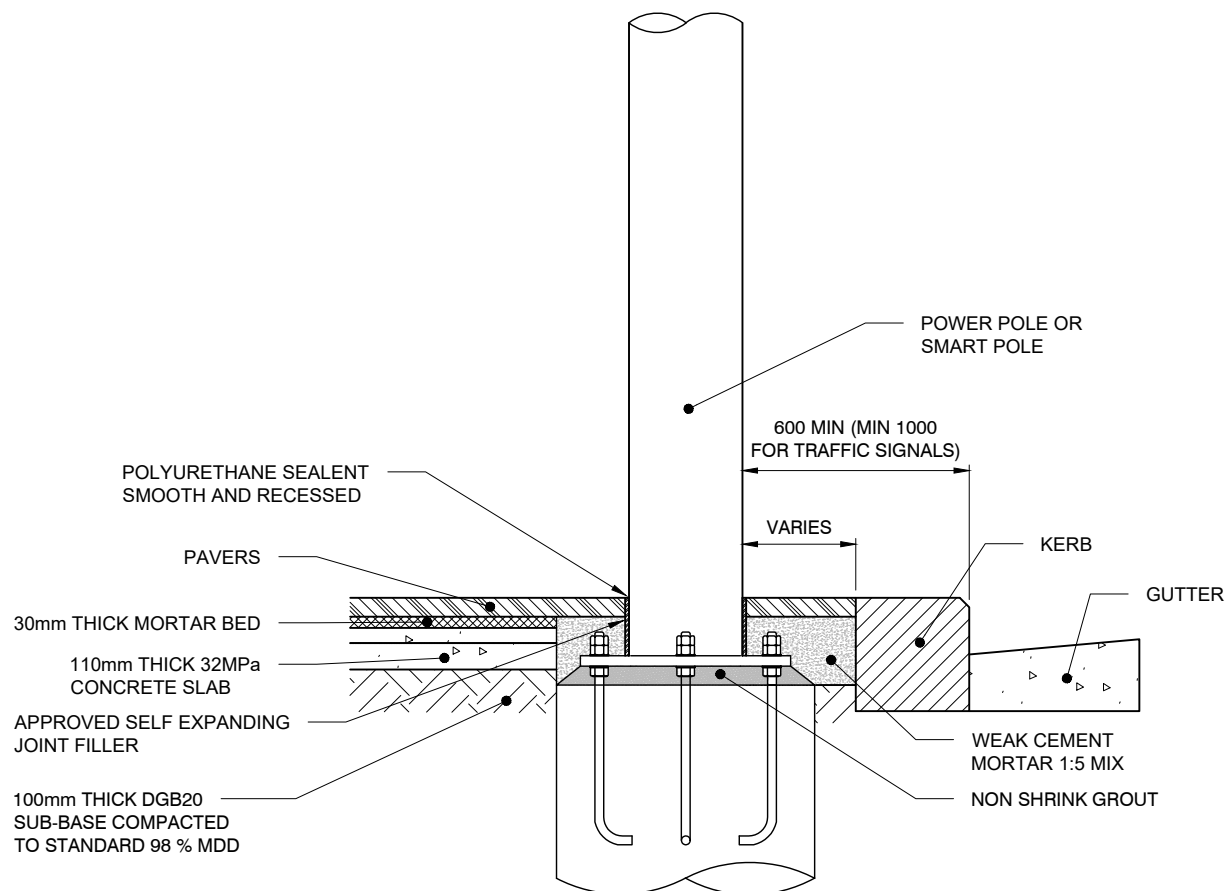
DETAIL 1:5

NOTES:

1. ONLY TO BE USED IN COUNCIL APPROVED LOCATIONS.
2. SUB-BASE SHALL BE 100mm THICK DGB20.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

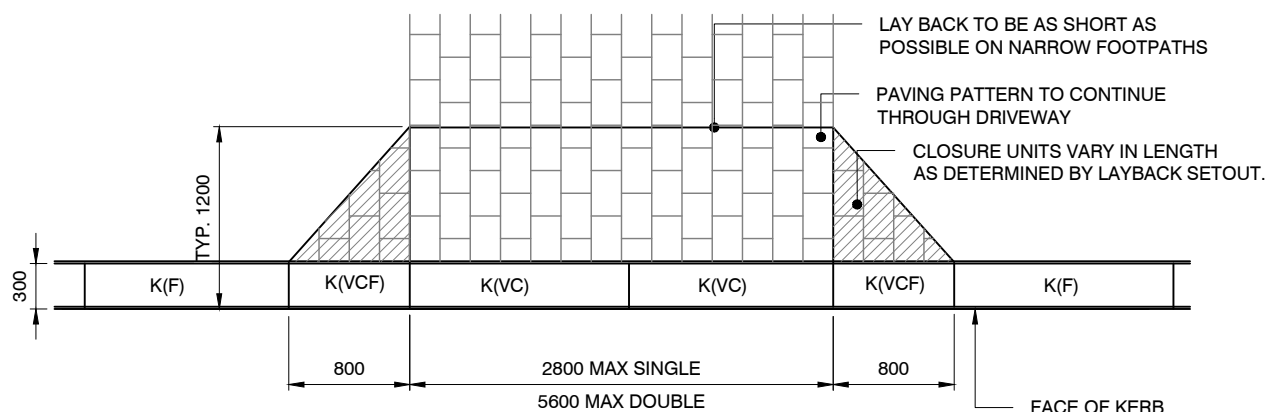


PLAN 1:20



SECTION 1:20

- NOTES:
1. SUB-GRADE SHALL BE COMPACTED TO MINIMUM 4% CBR
 2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



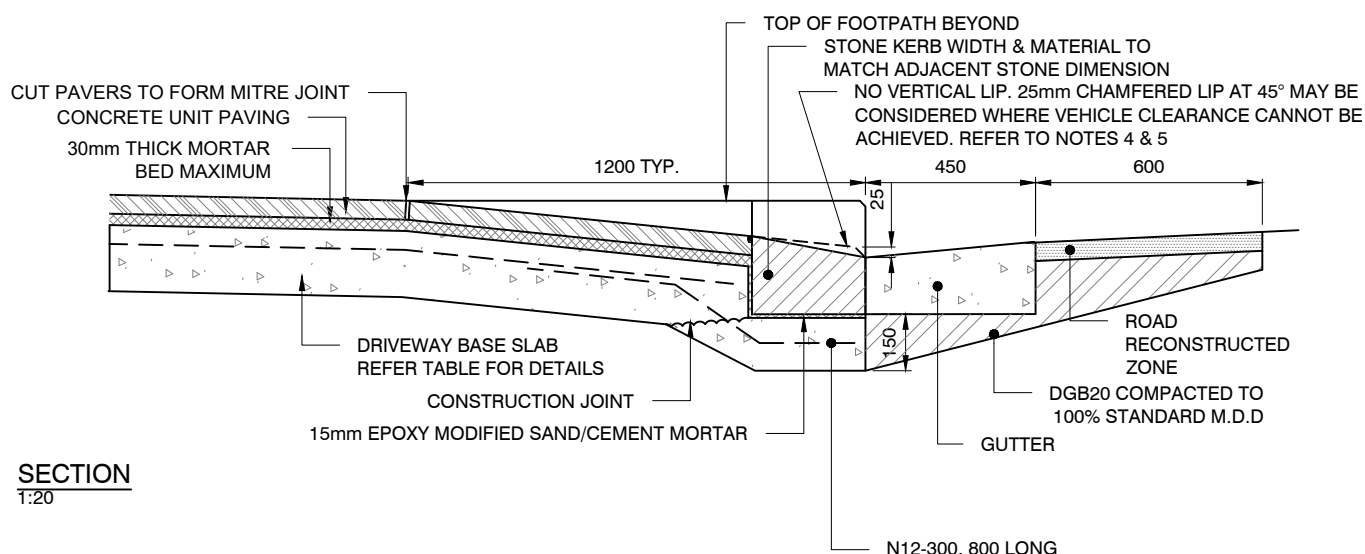
KERB TYPES:

TYPE K(F): FULL HEIGHT
 TYPE K(VC): VEHICULAR CHAMFERED TO FALL
 TYPE K(V): VEHICULAR CROSSOVER

NOTES:

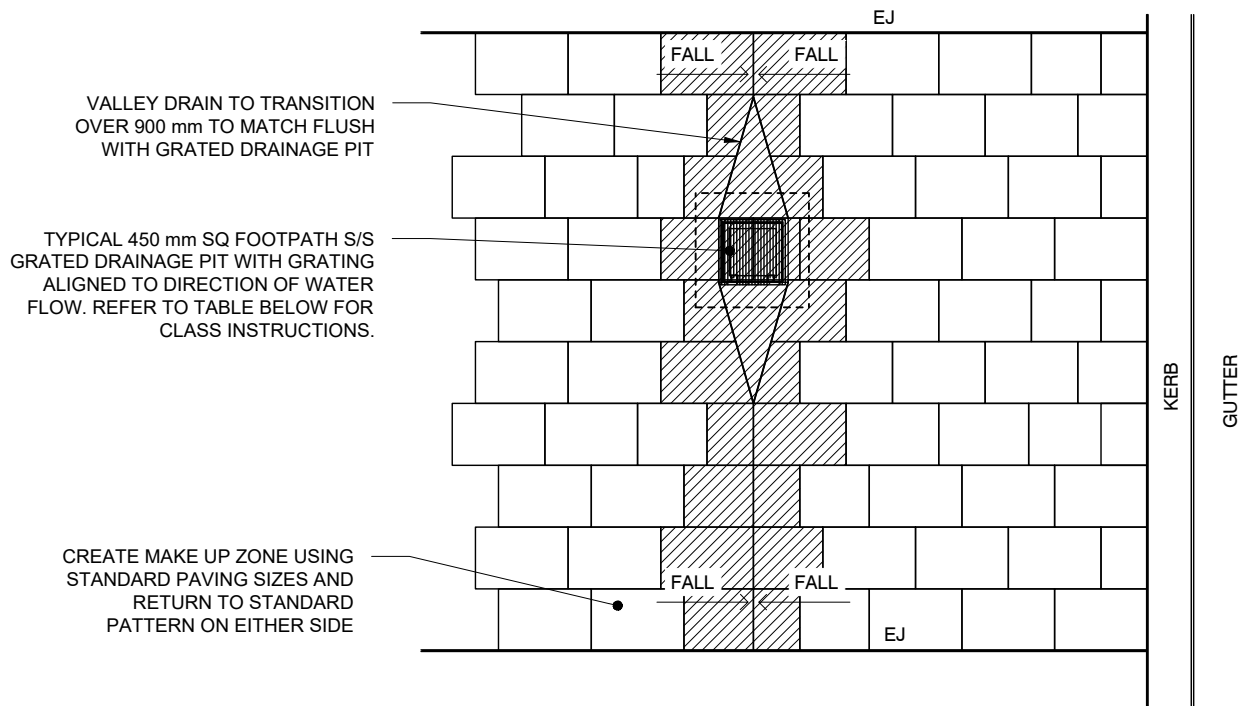
1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE UNLESS APPROVED OTHERWISE
3. FOR NARROW FOOTPATHS, LENGTH OF RAMP TO BE REDUCED TO 900mm SUBJECT TO VEHICLE CLEARANCE
4. VERTICAL AND HORIZONTAL CLEARANCE SHALL BE CHECKED BY THE DESIGNER IN ACCORDANCE WITH AS2890.1
5. VERTICAL LIP ADJACENT TO CYCLEWAY MUST BE APPROVED BY CITY'S REPRESENTATIVE
6. SUB-BASE SHALL BE 100mm THICK DGB20 COMPACTED TO STANDARD 98% MDD
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

PLAN 1:50

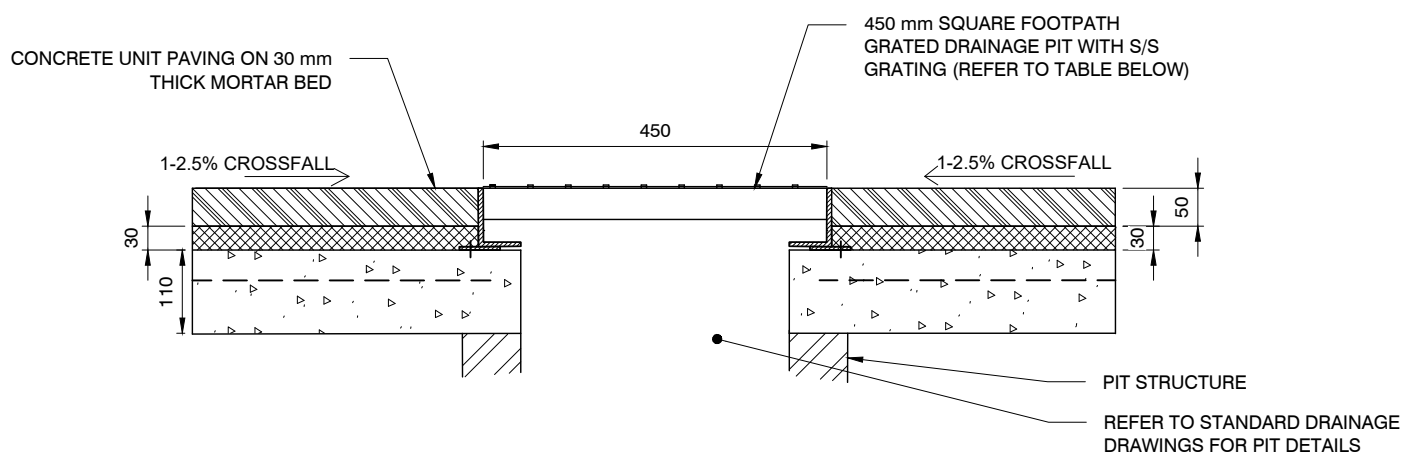


SECTION 1:20

DRIVEWAY SPECIFICATIONS				
DRIVEWAY USE	CONCRETE STRENGTH	THICKNESS	REINFORCEMENT	PAVER SIZE
SINGLE RESIDENTIAL	32MPa	150	SL82, 50 TOP COVER	200 x 300 x 70
MULTI RESIDENTIAL	32MPa	200	SL82, 50 TOP COVER	200 x 300 x 70
COMMERCIAL/ INDUSTRIAL	32MPa	250	TWO LAYERS SL82 50 COVER TOP & BOTTOM	200 x 300 x 70



PLAN 1:50

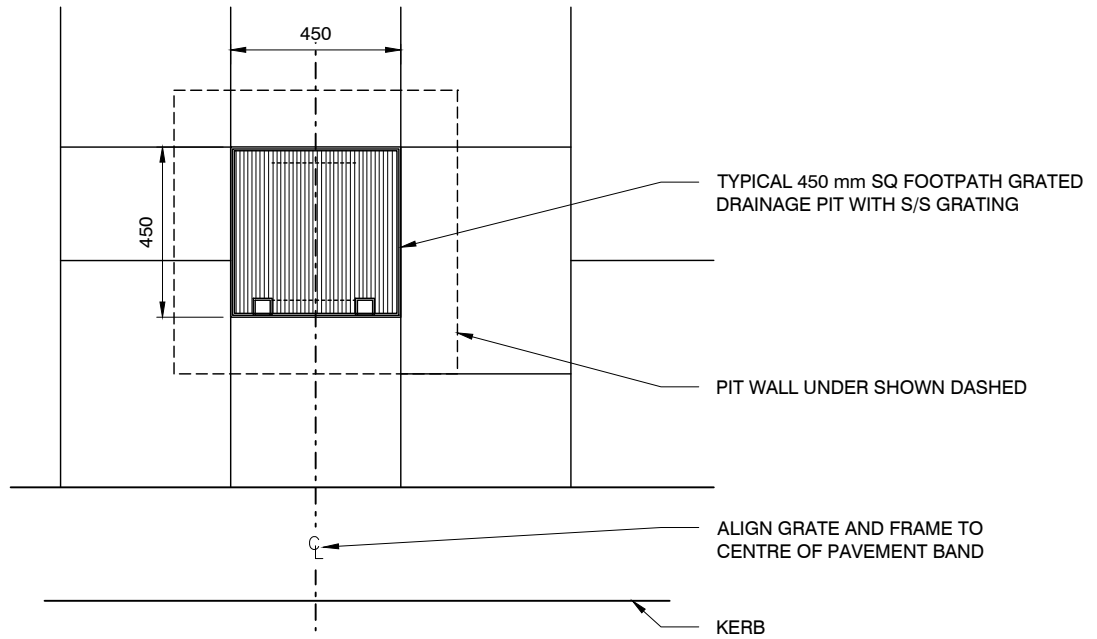


NOTES:

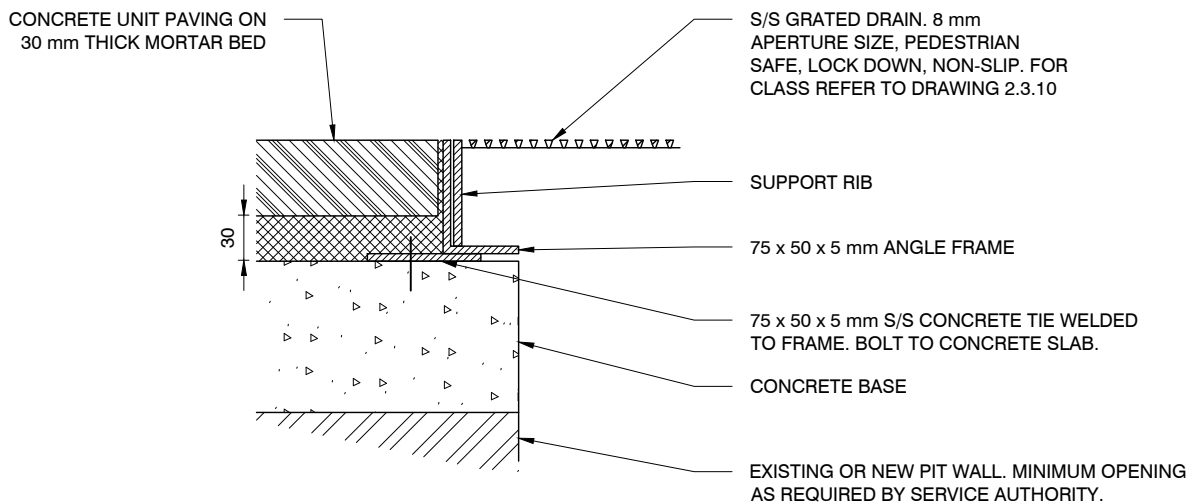
1. ALIGN 450 mm SQUARE PIT FRAME TO PAVEMENT BANDING & TO PAVING MODULES AS SHOWN.
2. DETAIL NOT TO BE USED WITHOUT SITE SPECIFIC APPROVAL FROM COUNCIL.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

PIT LOCATION	MINIMUM PIT AND GRATE CLASS
FOOTWAY	C (PEDESTRIAN SAFE)
ROADWAY	D (CYCLE SAFE)
DRIVEWAY	D (CYCLE SAFE)

SECTION 1:10

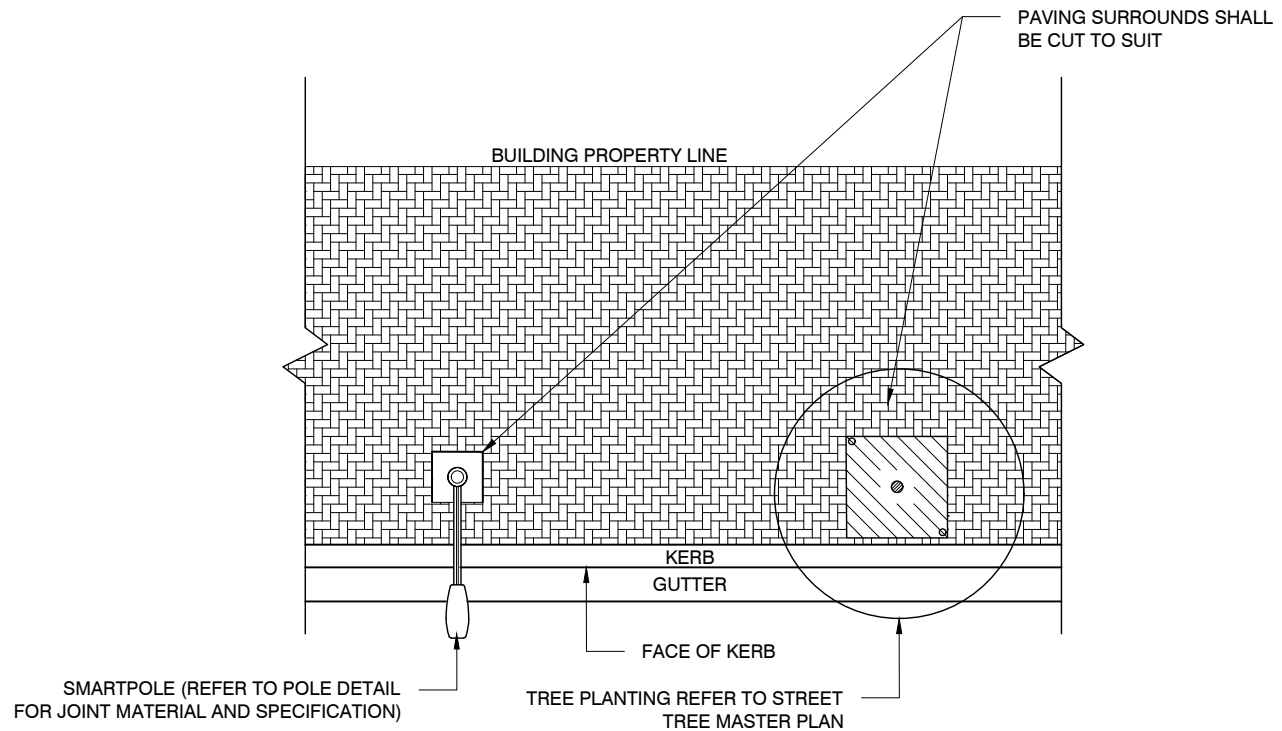


PLAN 1:20

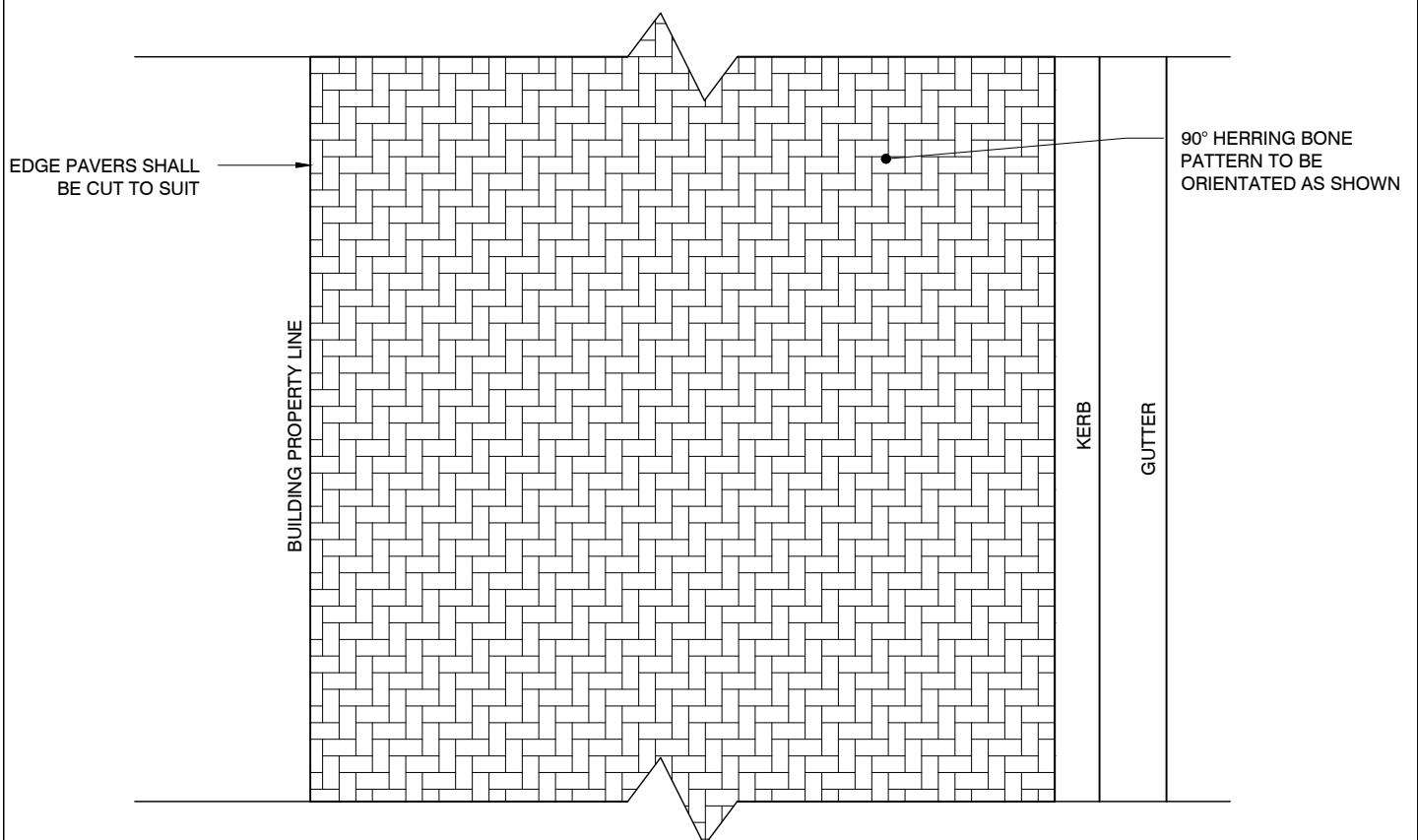


SECTION 1:5

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



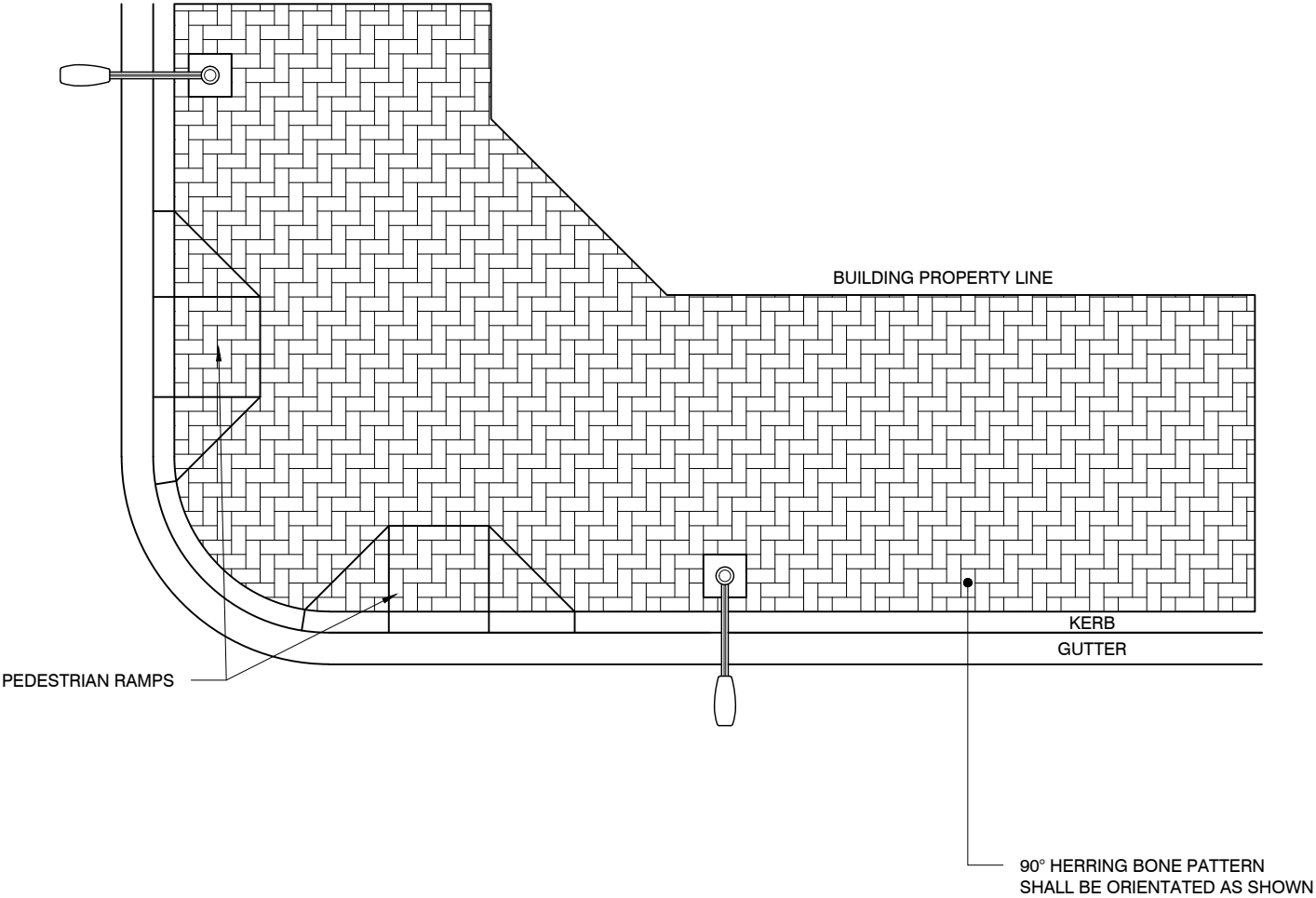
PLAN 1:100



TYPICAL PAVING MODULE 1:50

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

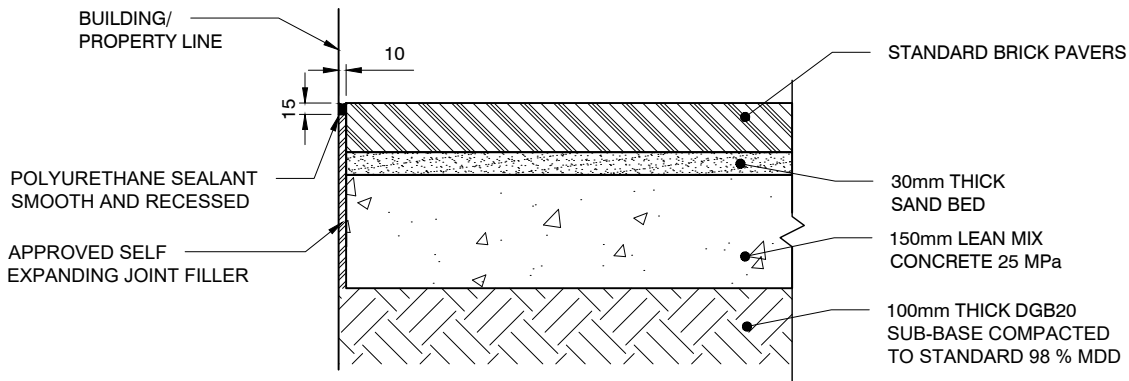
BRICK PAVING CORNER ARRANGEMENT PLAN



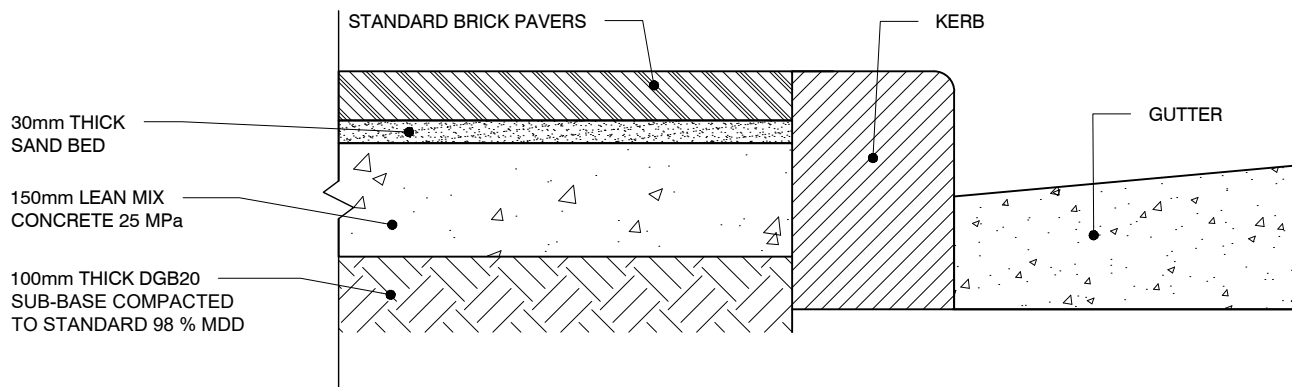
PLAN 1:100

- NOTES:
1. FOR NON 90° INTERSECTIONS, PAVERS IN MAKE UP ZONE ARE TO BE CUT TO ACCOMMODATE THE INTERSECTION ANGLE.
 2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

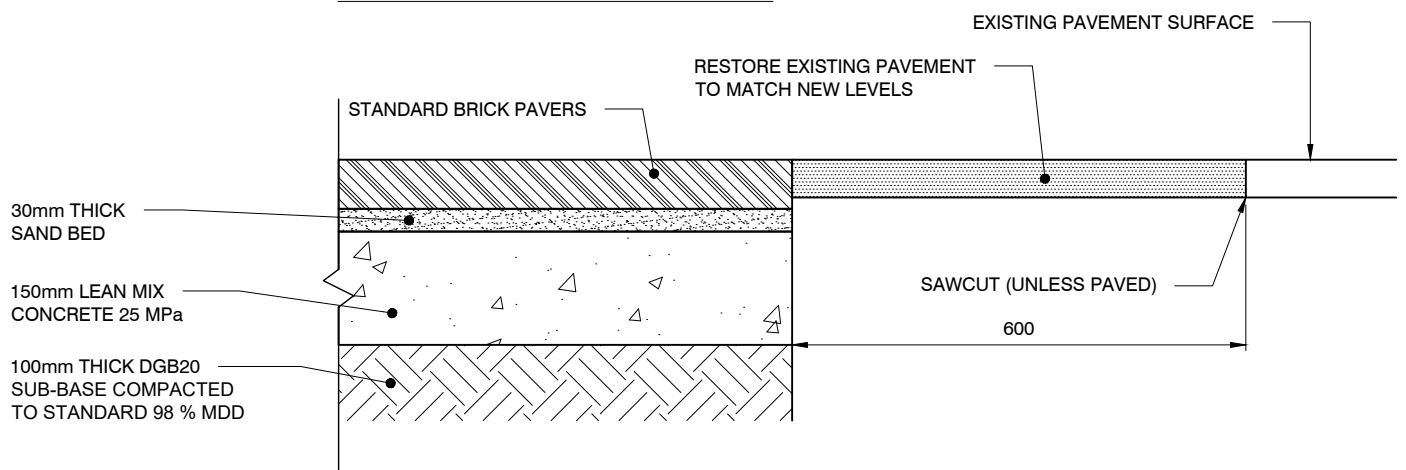
JUNCTION WITH BUILDING



JUNCTION WITH BACK OF KERB

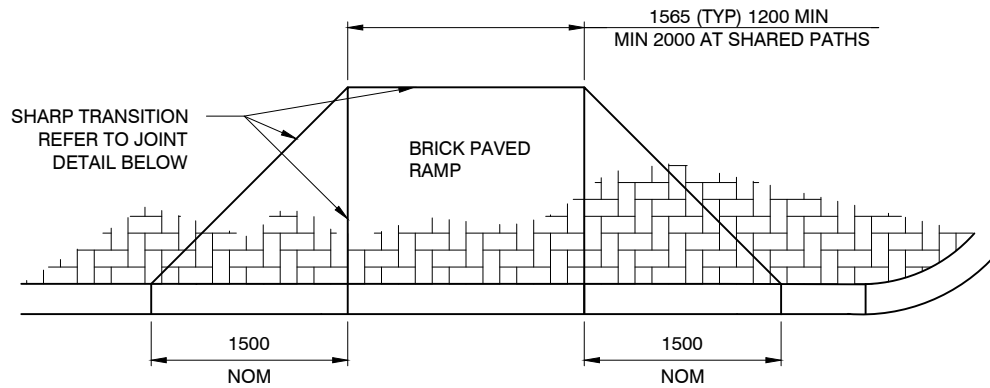


JUNCTION WITH EXISTING PAVEMENT

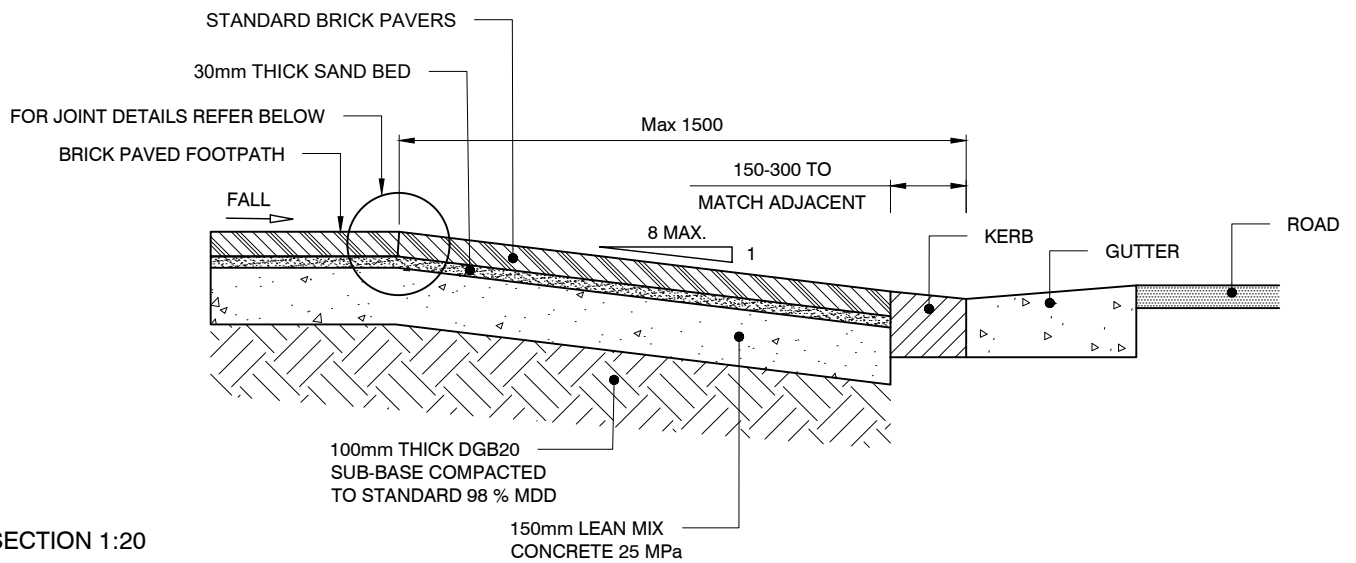


SECTION 1:10

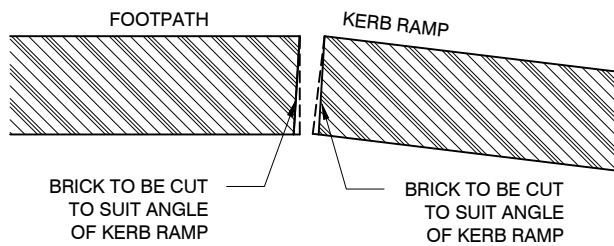
NOTES:
1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN 1:50

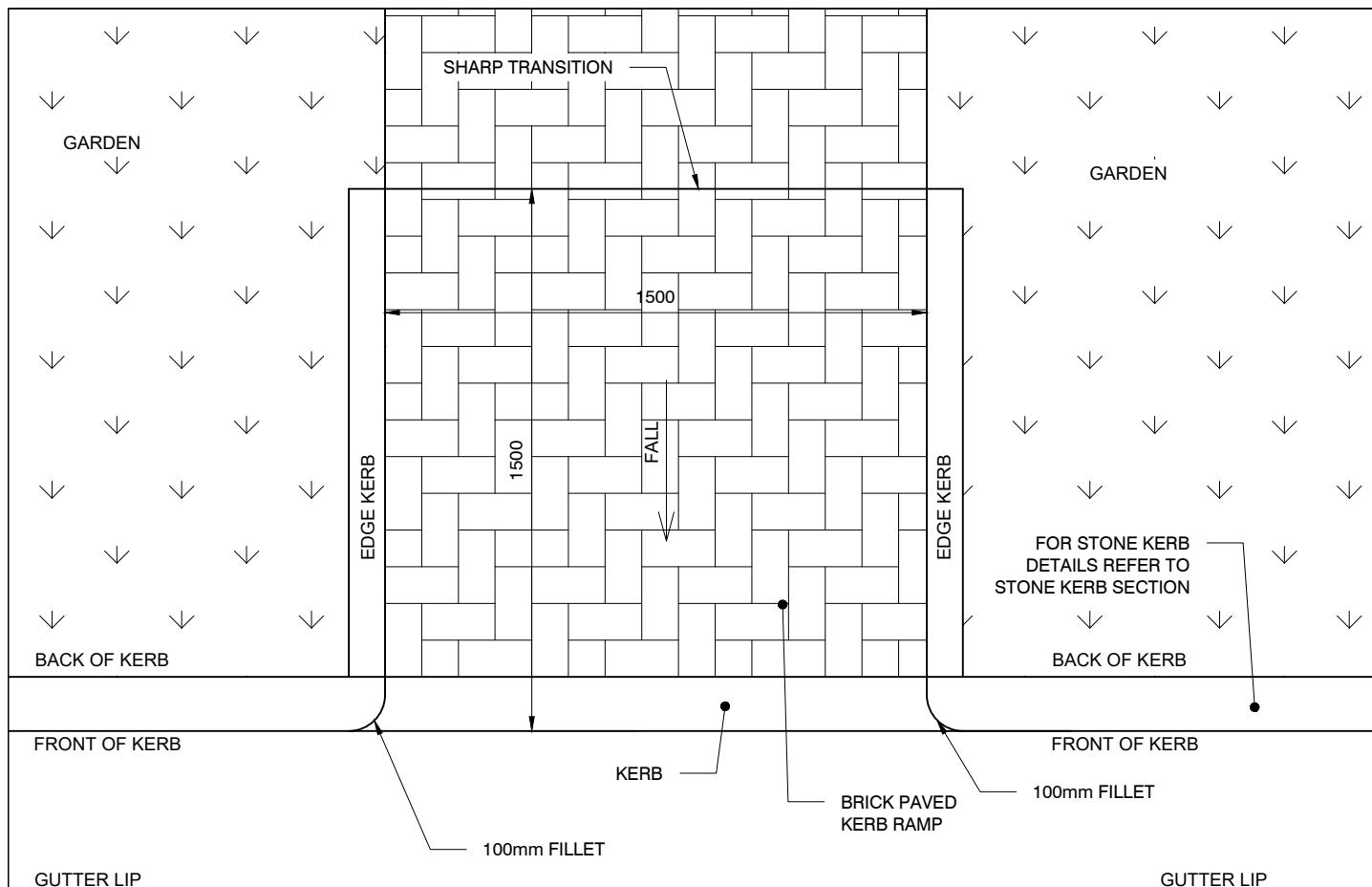


SECTION 1:20

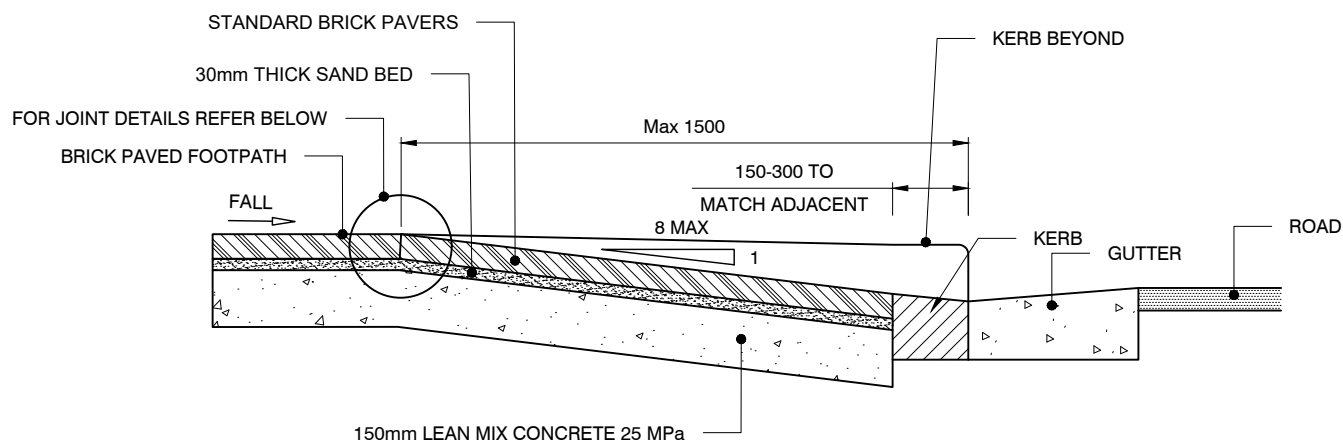


DETAIL 1:5

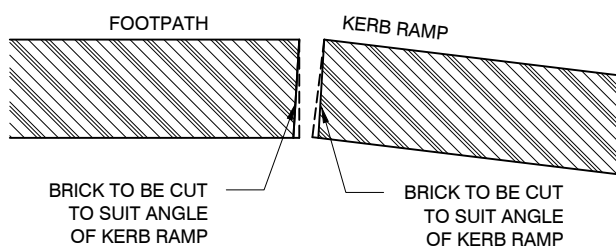
NOTES:
1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN 1:20



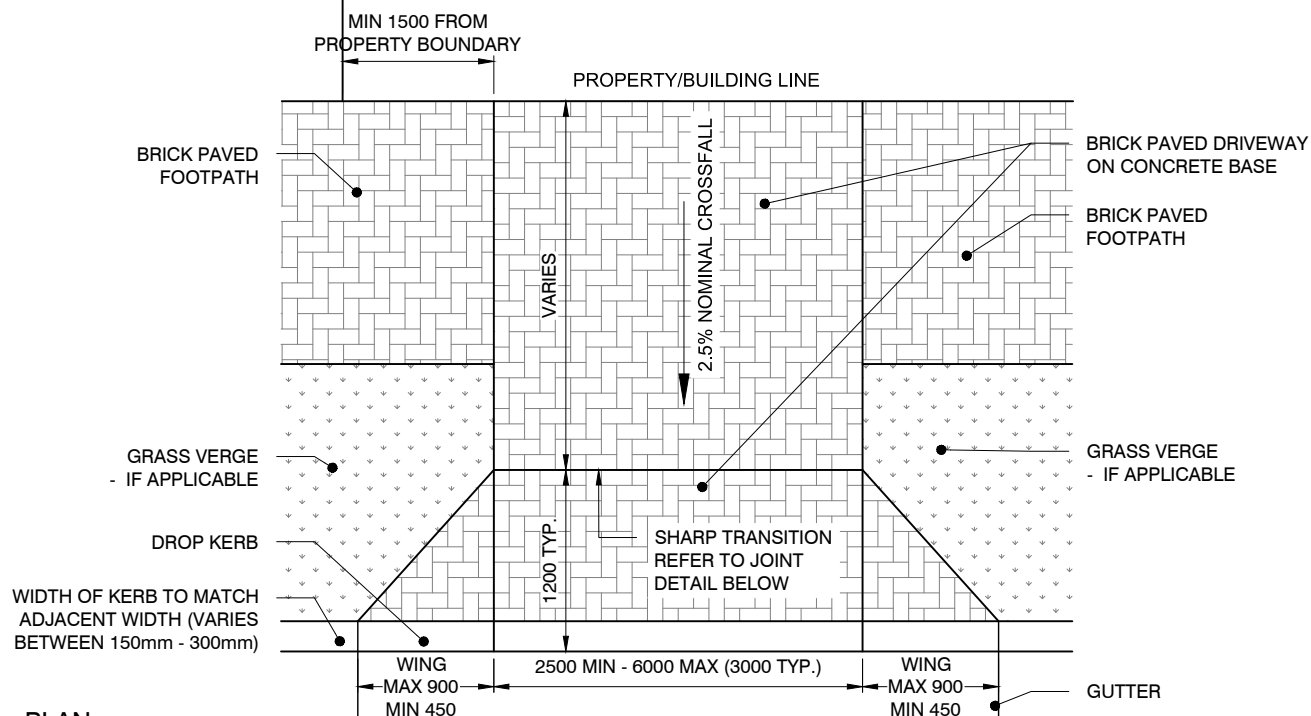
SECTION 1:20



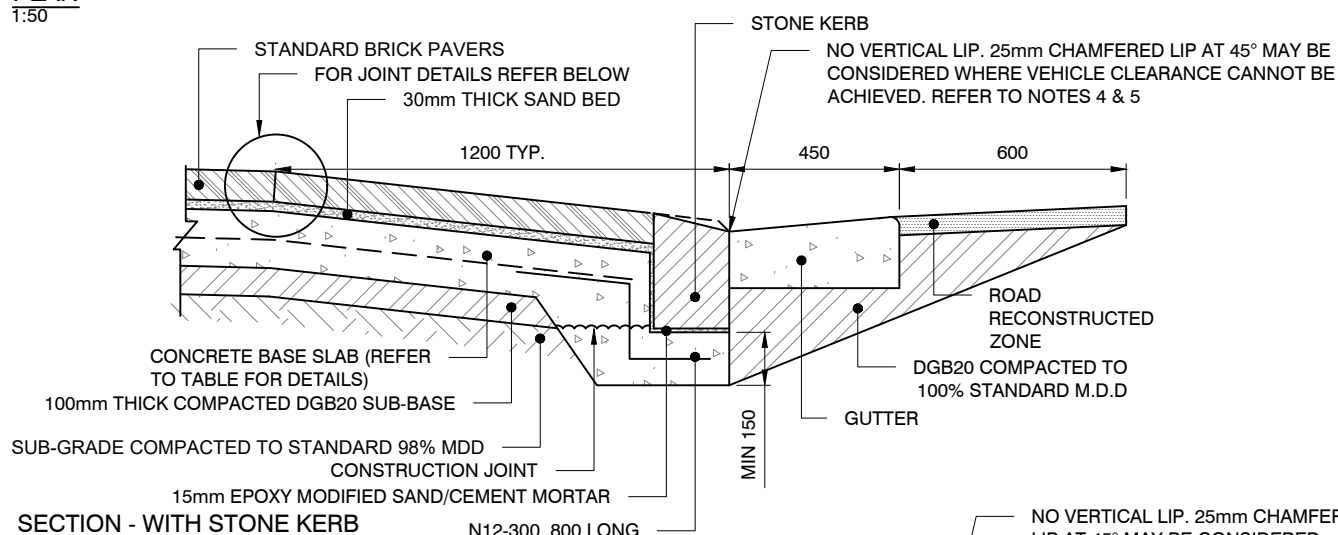
DETAIL 1:5

NOTES:

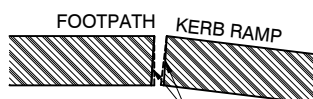
1. ONLY TO BE USED IN COUNCIL APPROVED LOCATIONS.
2. SUB-BASE SHALL BE 100mm THICK DGB20.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN
1:50

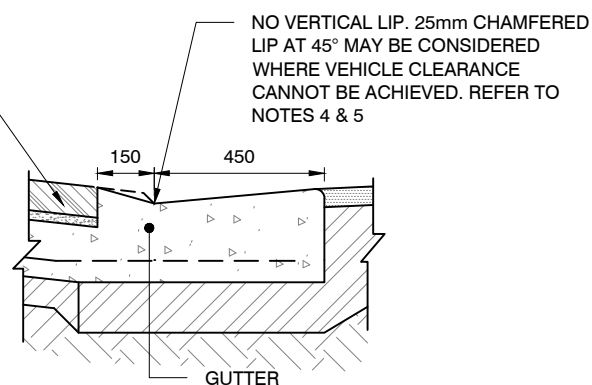


SECTION - WITH STONE KERB
1:20



DETAIL 1:10

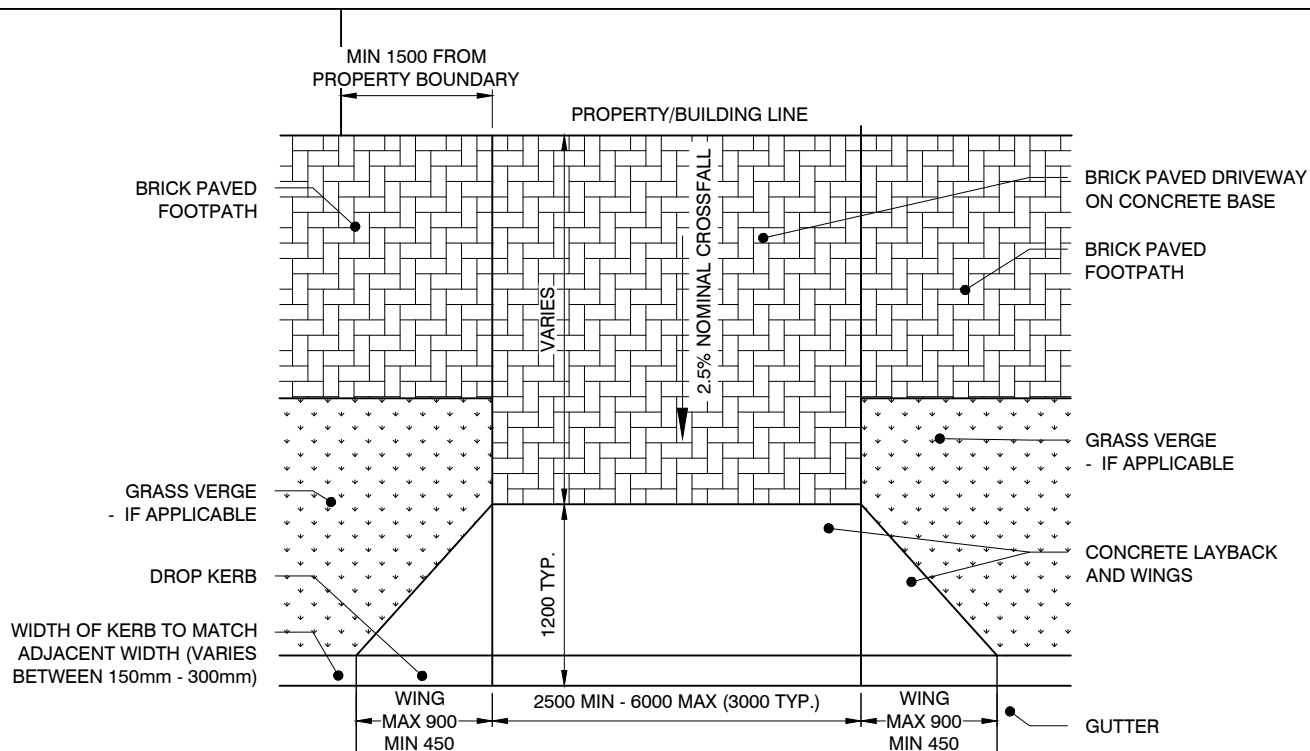
DRIVEWAY SPECIFICATIONS			
DRIVEWAY USE	CONCRETE STRENGTH	THICKNESS	REINFORCEMENT
SINGLE RESIDENTIAL	32MPa	150	SL82, 50 TOP COVER
MULTI RESIDENTIAL	32MPa	200	SL82, 50 TOP COVER



SECTION - WITH CONCRETE K&G
1:20

NOTES:

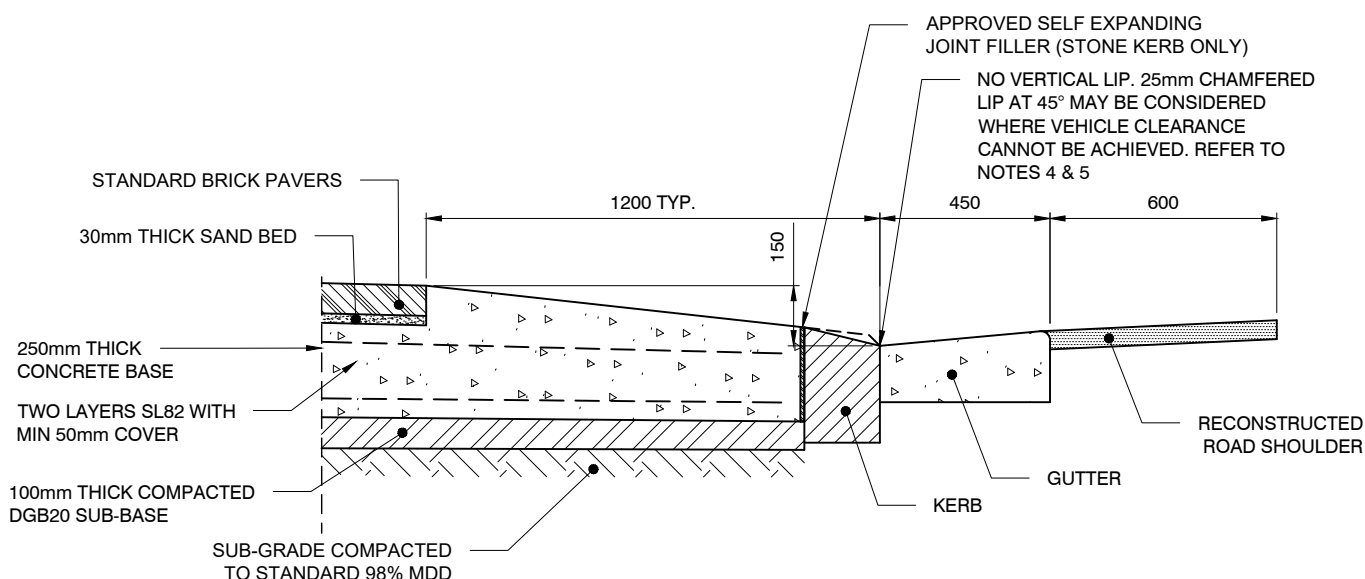
1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE, UNLESS APPROVED OTHERWISE.
3. ALL DRIVEWAY CROSSINGS TO INCLUDE REINFORCED CONCRETE SLAB.
4. VERTICAL AND HORIZONTAL CLEARANCE SHALL BE CHECKED BY THE DESIGNER IN ACCORDANCE WITH AS2890.1
5. VERTICAL LIP ADJACENT TO CYCLEWAY MUST BE APPROVED BY CITY'S REPRESENTATIVE
6. FOR NARROW FOOTPATHS, LENGTH OF RAMP TO BE REDUCED TO 900mm SUBJECT TO VEHICLE CLEARANCE, OR LAYBACK ONLY TO BE USED IN APPROVED APPLICATIONS.
7. FOR DRIVEWAYS WIDER THAN 6.0m A TOOL JOINT SHALL BE PROVIDED ALONG THE CENTRE OF THE DRIVEWAY.
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



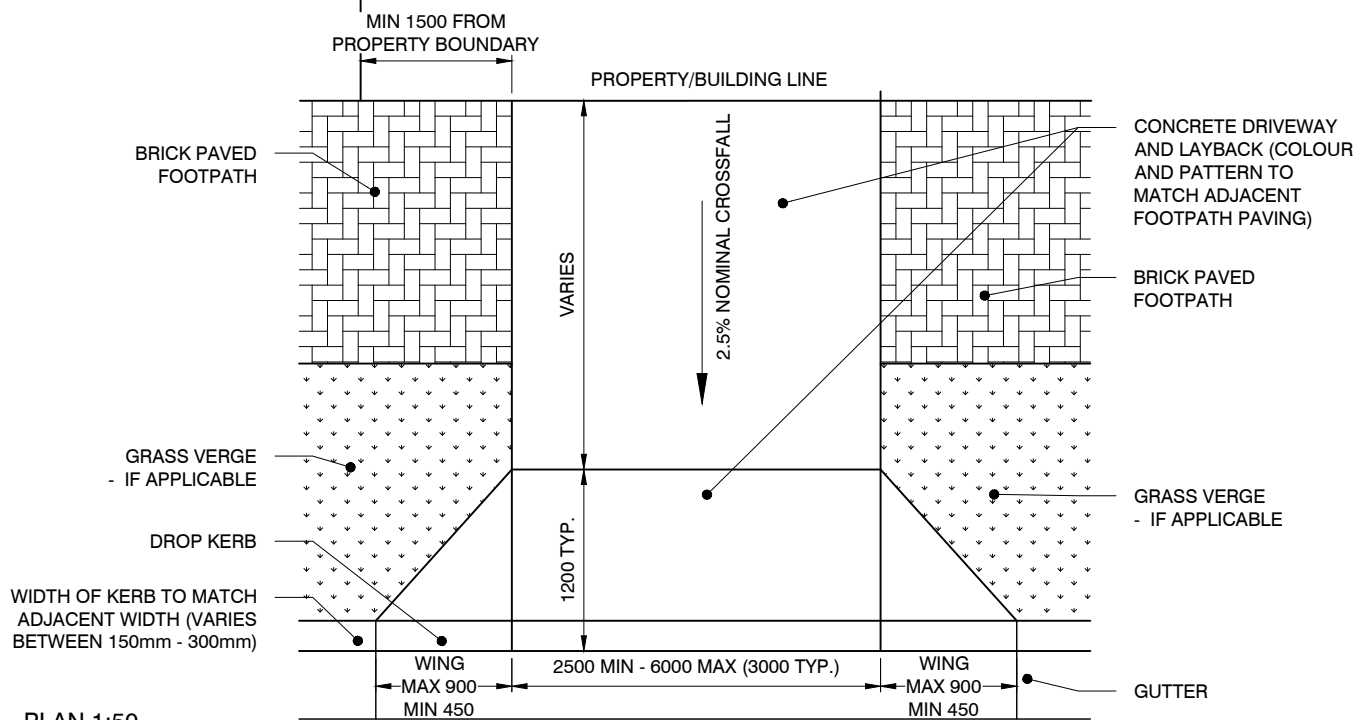
PLAN 1:50

NOTES:

1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE, UNLESS APPROVED OTHERWISE.
3. ALL DRIVEWAY CROSSINGS TO INCLUDE REINFORCED CONCRETE SLAB.
4. VERTICAL AND HORIZONTAL CLEARANCE SHALL BE CHECKED BY THE DESIGNER IN ACCORDANCE WITH AS2890.1
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8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



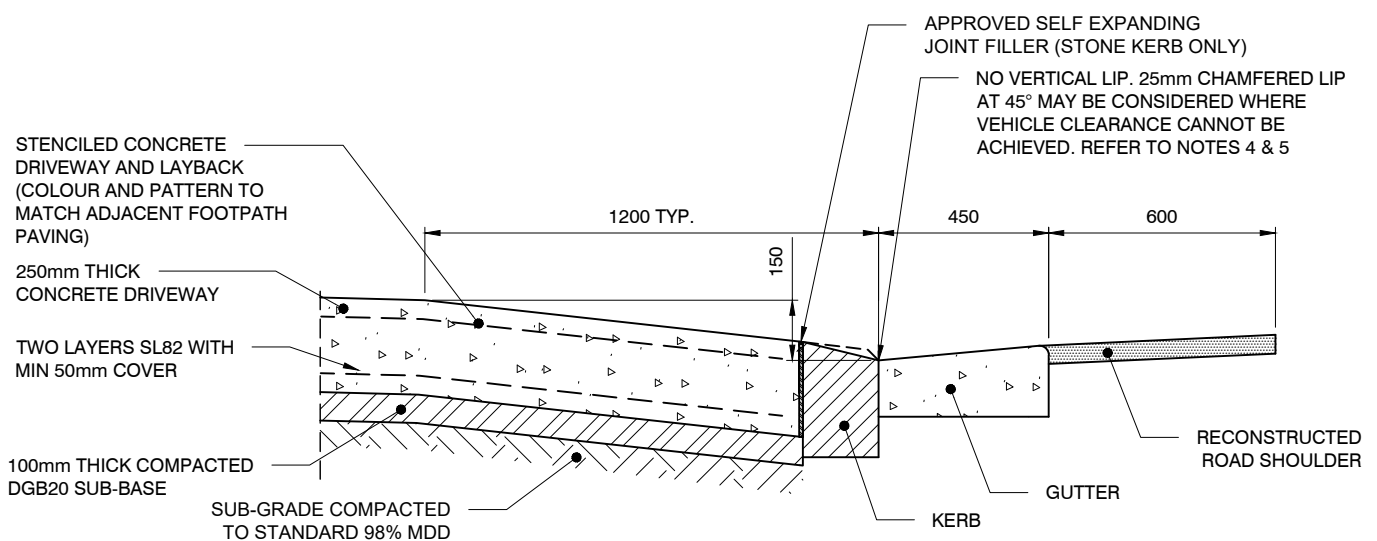
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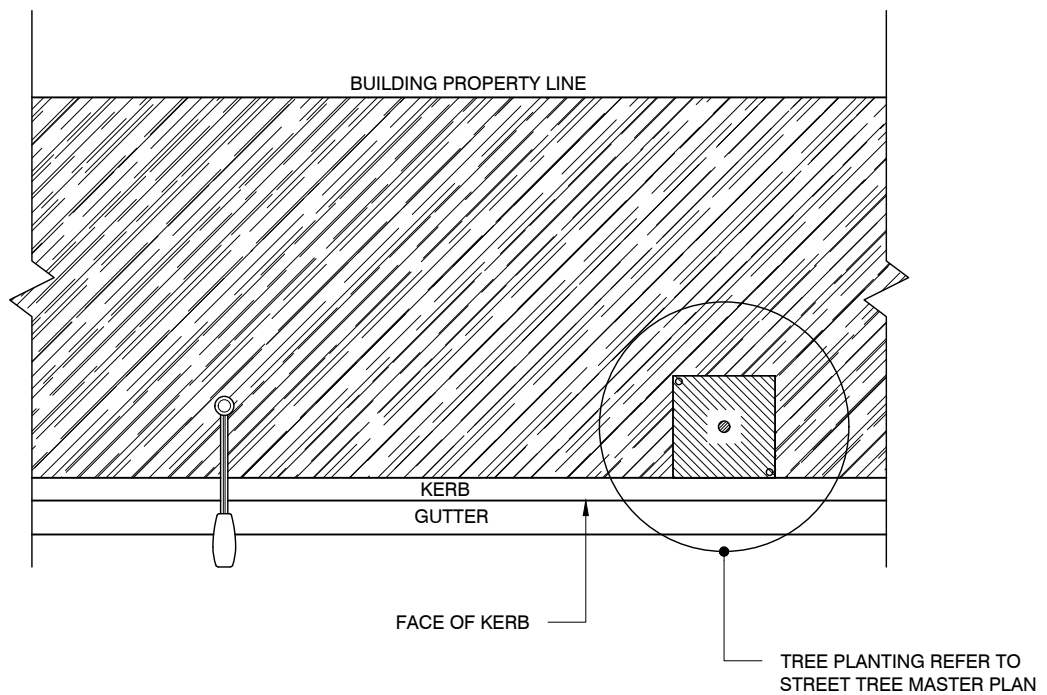
PLAN 1:50

NOTES:

1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE, UNLESS APPROVED OTHERWISE.
3. ALL DRIVEWAY CROSSINGS TO INCLUDE REINFORCED CONCRETE SLAB.
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8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



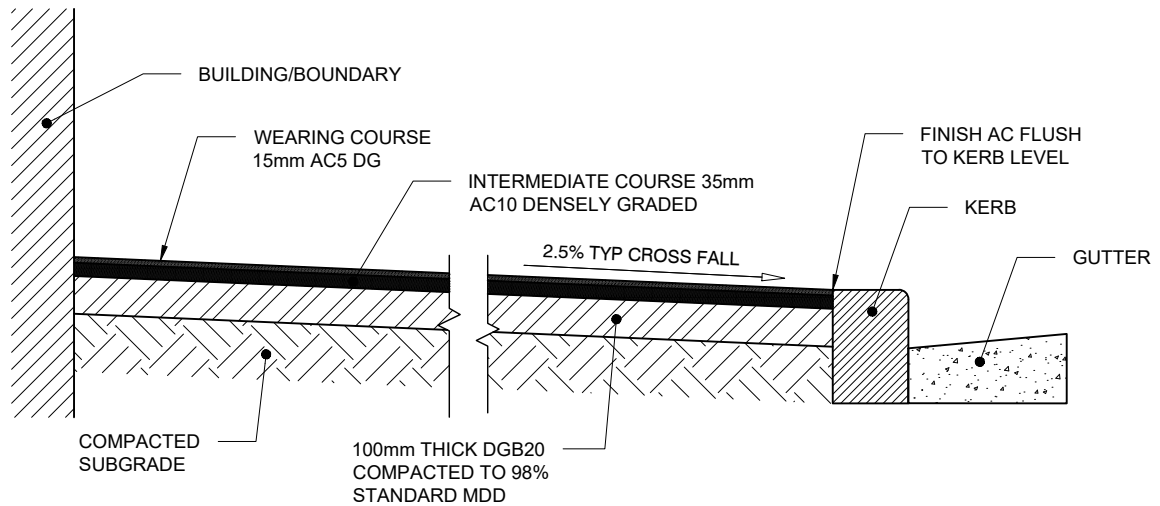
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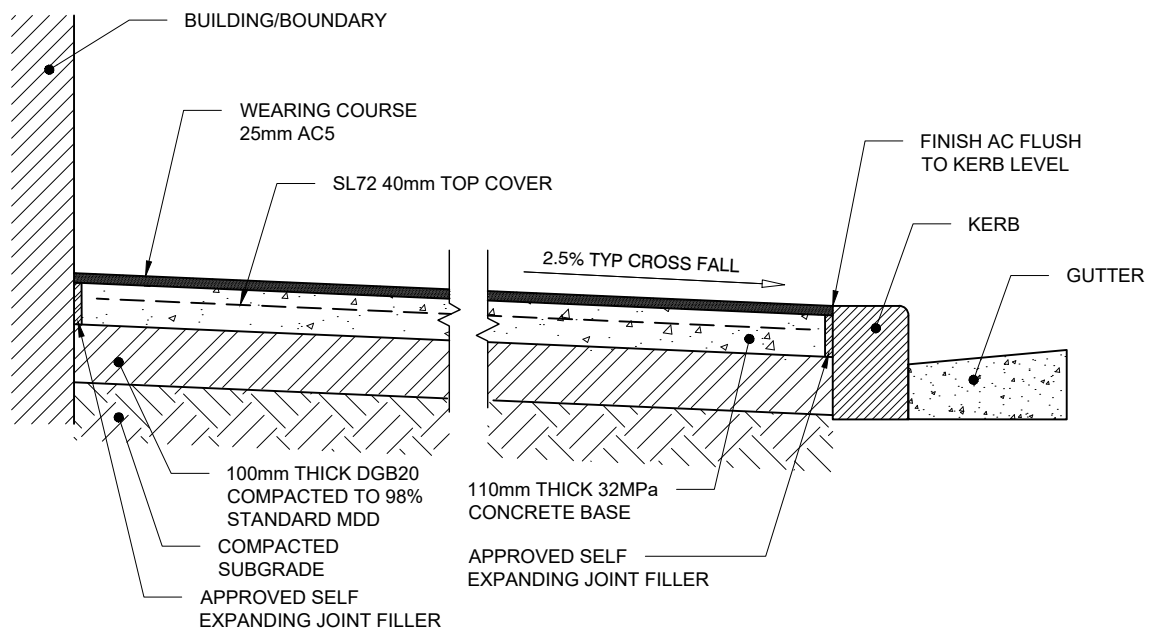
PLAN 1:100

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

TYPICAL FOOTPATH SECTION - FLEXIBLE BASE



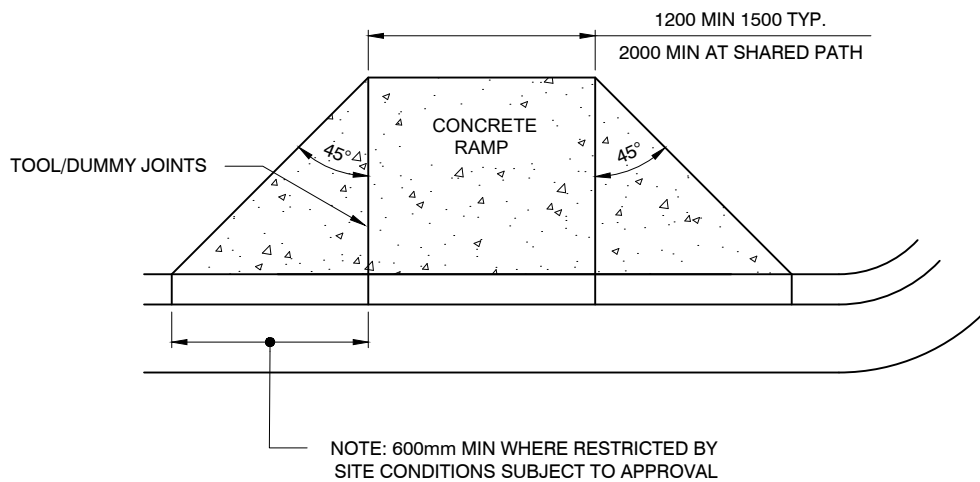
TYPICAL FOOTPATH SECTION - RIGID BASE



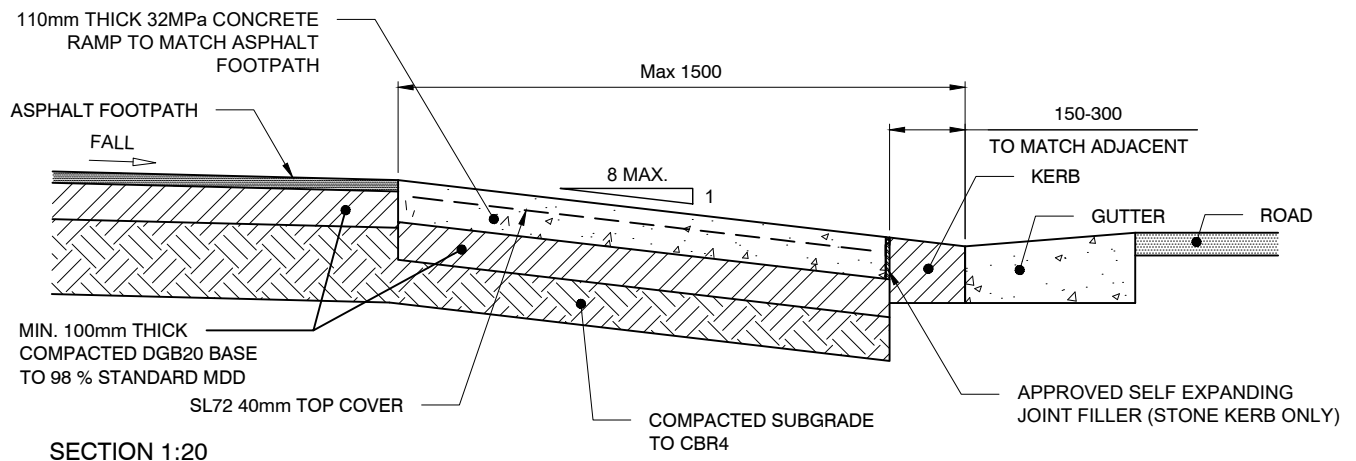
NOTES:

1. MODIFIED ASPHALTIC CONCRETE WITH POLYMER ADDITIVE TO INCREASE SOFTENING POINT TO BE USED IN LOCATIONS WITH EXISTING OR PROPOSED OUTDOOR DINING (FULL FRONTAGE).
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

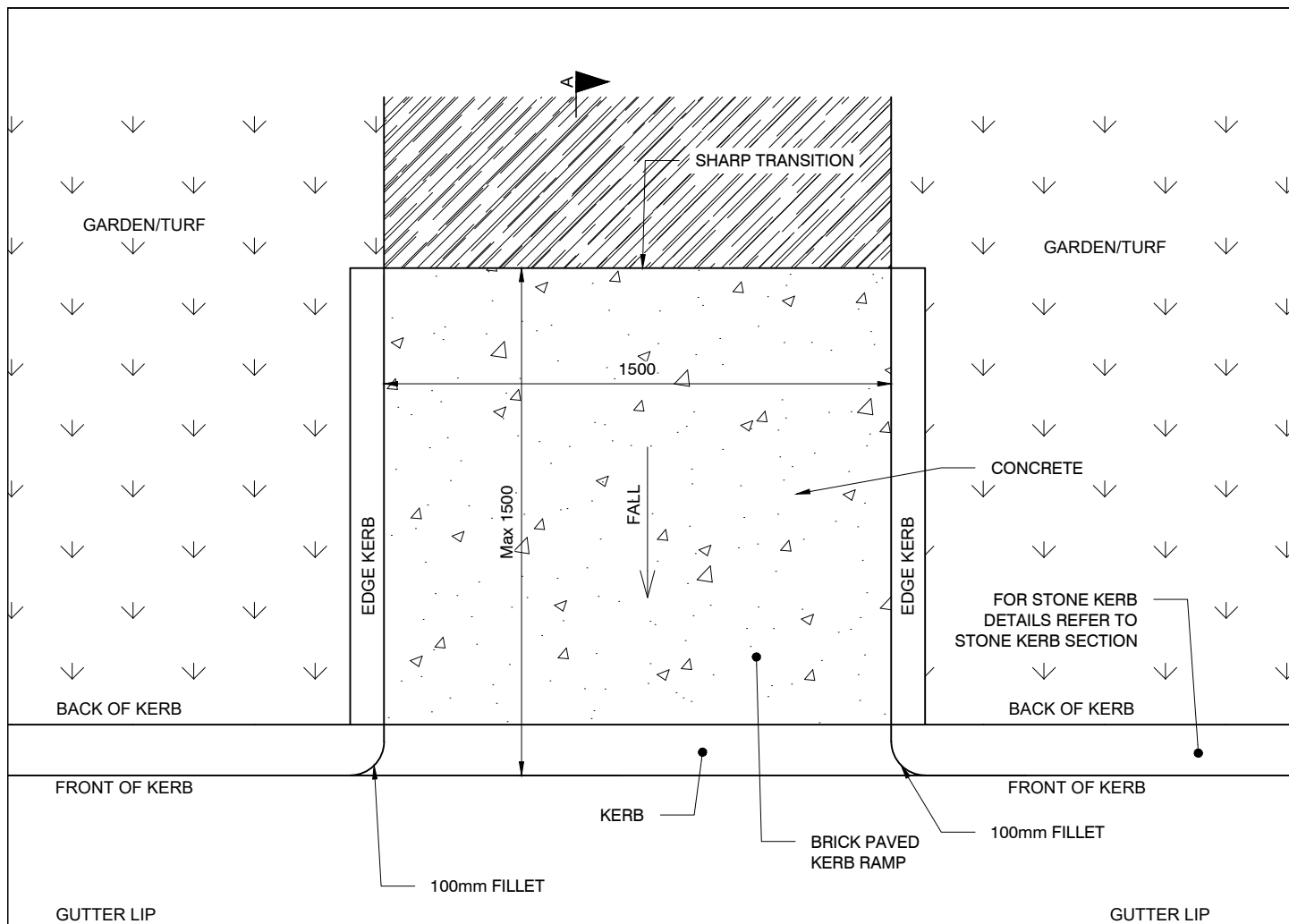
SCALE 1:20



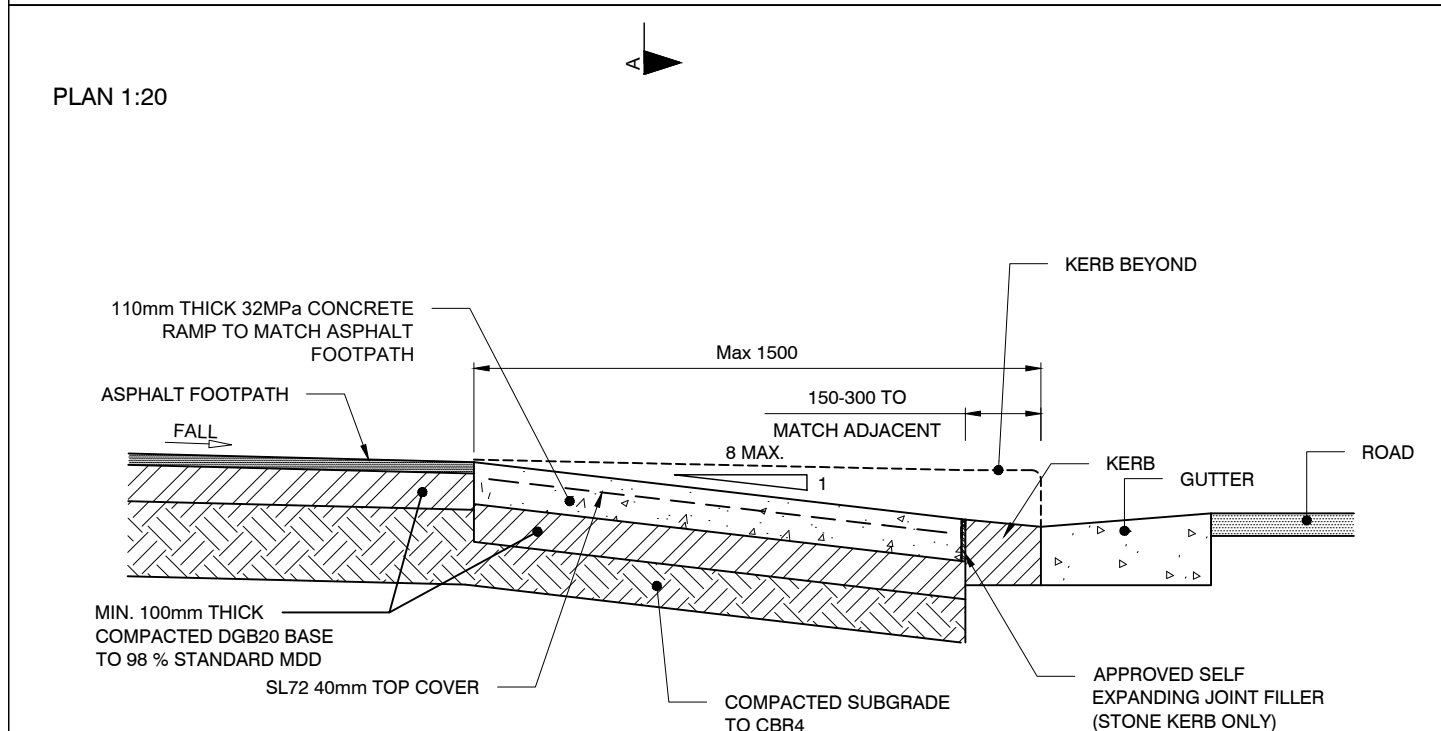
PLAN 1:50



NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

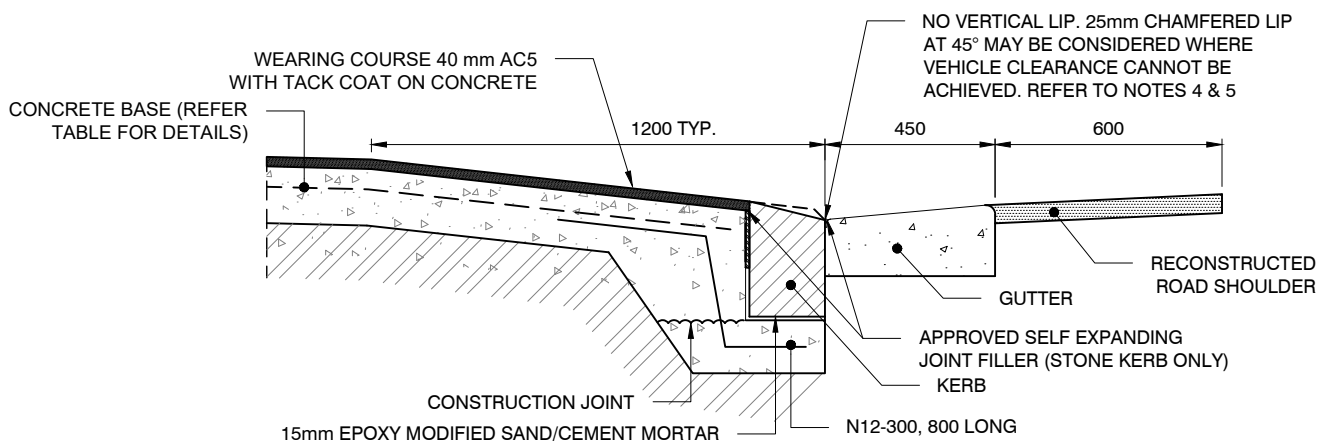
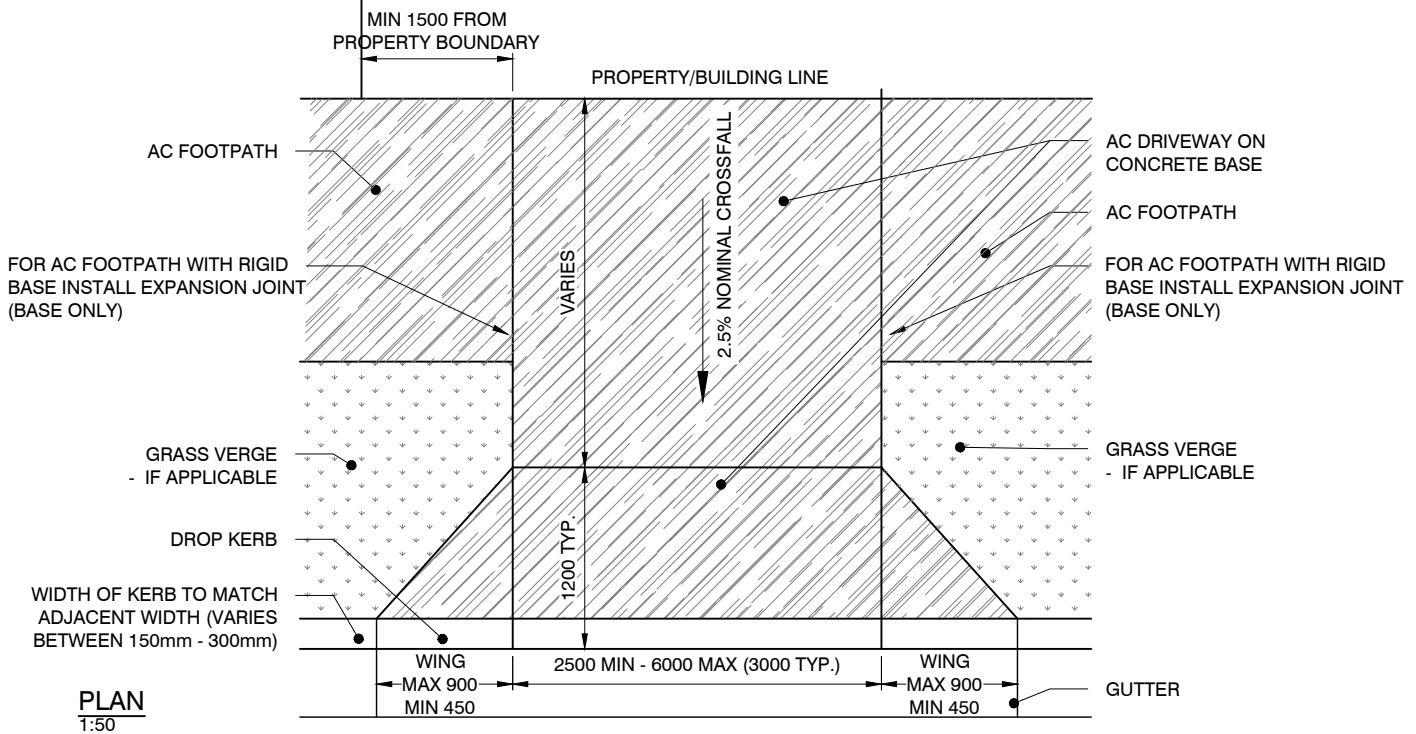


PLAN 1:20



SECTION A-A 1:20

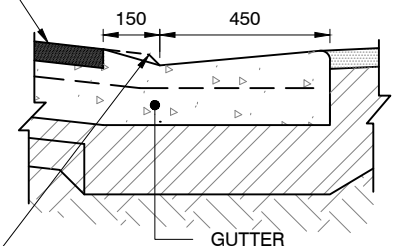
NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



SECTION (WITH STONE KERB)
1:20

DRIVEWAY SPECIFICATIONS			
DRIVEWAY USE	CONCRETE STRENGTH	THICKNESS	REINFORCEMENT
SINGLE RESIDENTIAL	32MPa	150	SL82 50 TOP COVER
MULTI RESIDENTIAL	32MPa	200	SL82 50 TOP COVER

WEARING COURSE 40 mm AC5 WITH TACK COAT ON CONCRETE

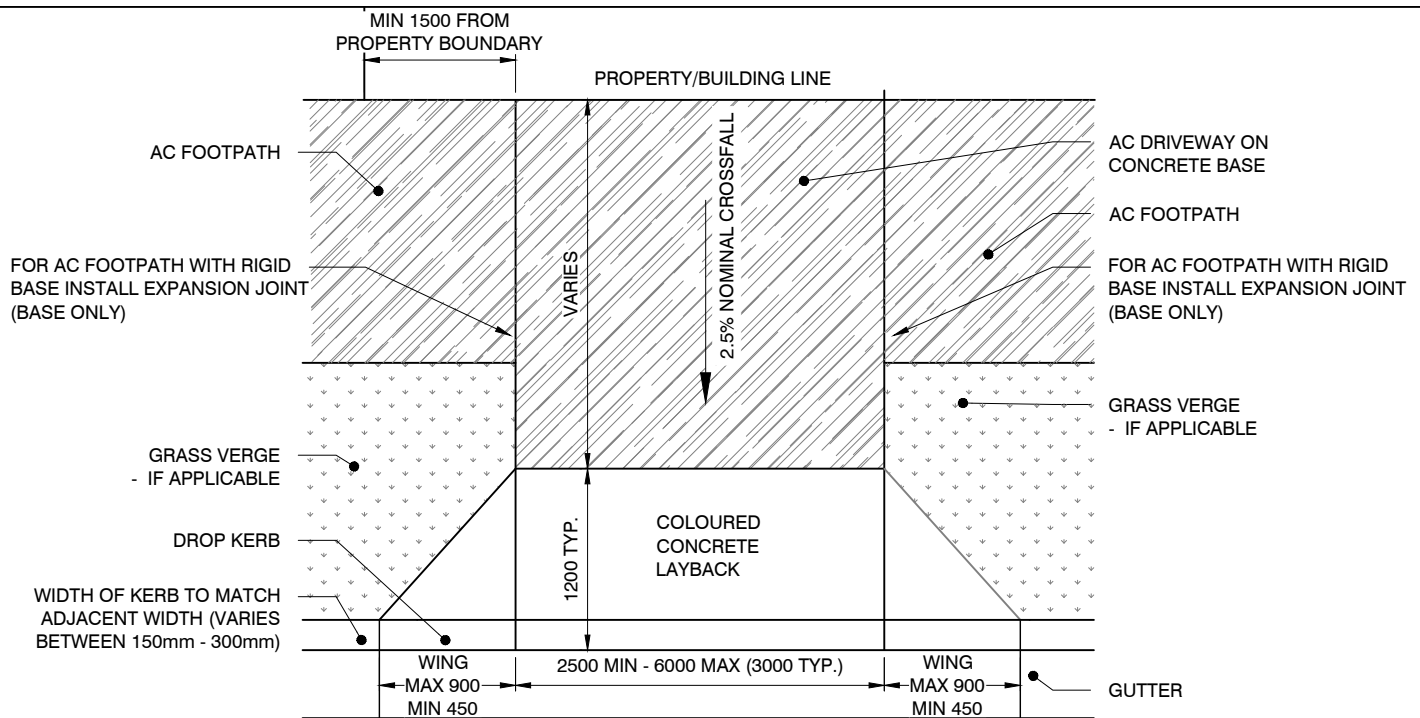


SECTION (WITH CONCRETE KERB)
1:20

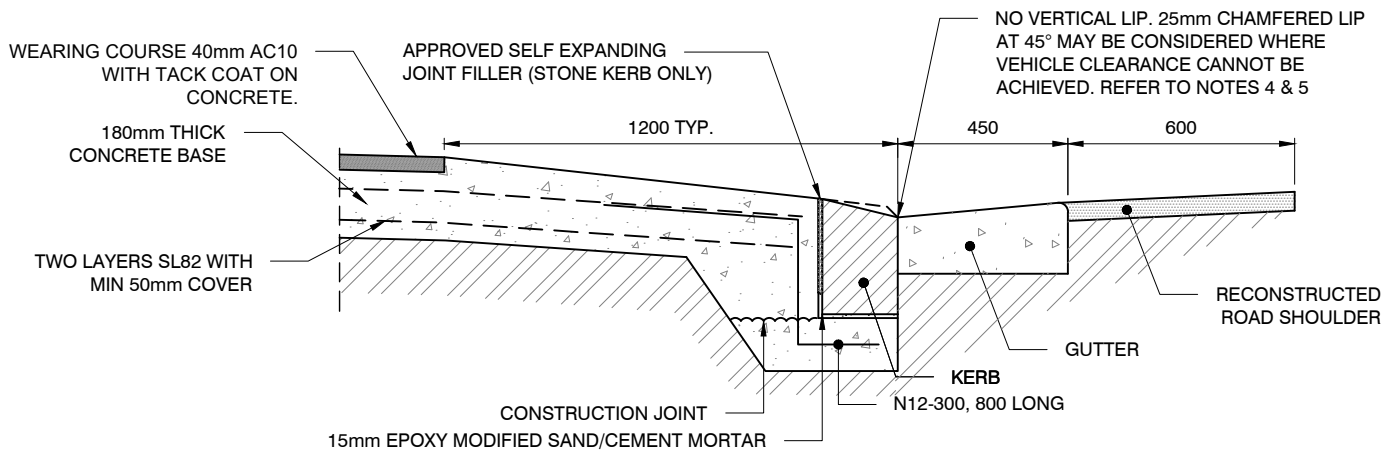
NOTES:

1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE, UNLESS APPROVED OTHERWISE.
3. ALL DRIVEWAY CROSSINGS TO INCLUDE REINFORCED CONCRETE SLAB.
4. VERTICAL AND HORIZONTAL CLEARANCE SHALL BE CHECKED BY THE DESIGNER IN ACCORDANCE WITH AS2890.1
5. VERTICAL LIP ADJACENT TO CYCLEWAY MUST BE APPROVED BY CITY'S REPRESENTATIVE
6. FOR NARROW FOOTPATHS, LENGTH OF RAMP TO BE REDUCED TO 900mm SUBJECT TO VEHICLE CLEARANCE, OR LAYBACK ONLY TO BE USED IN APPROVED APPLICATIONS.
6. SUB-BASE SHALL BE 100mm THICK DGB20 COMPACTED TO STANDARD 98% MDD
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

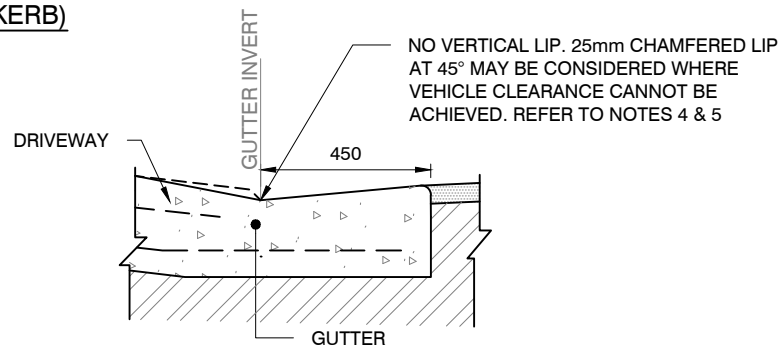
NO VERTICAL LIP. 25mm CHAMFERED LIP AT 45° MAY BE CONSIDERED WHERE VEHICLE CLEARANCE CANNOT BE ACHIEVED. REFER TO NOTES 4 & 5



PLAN
1:50



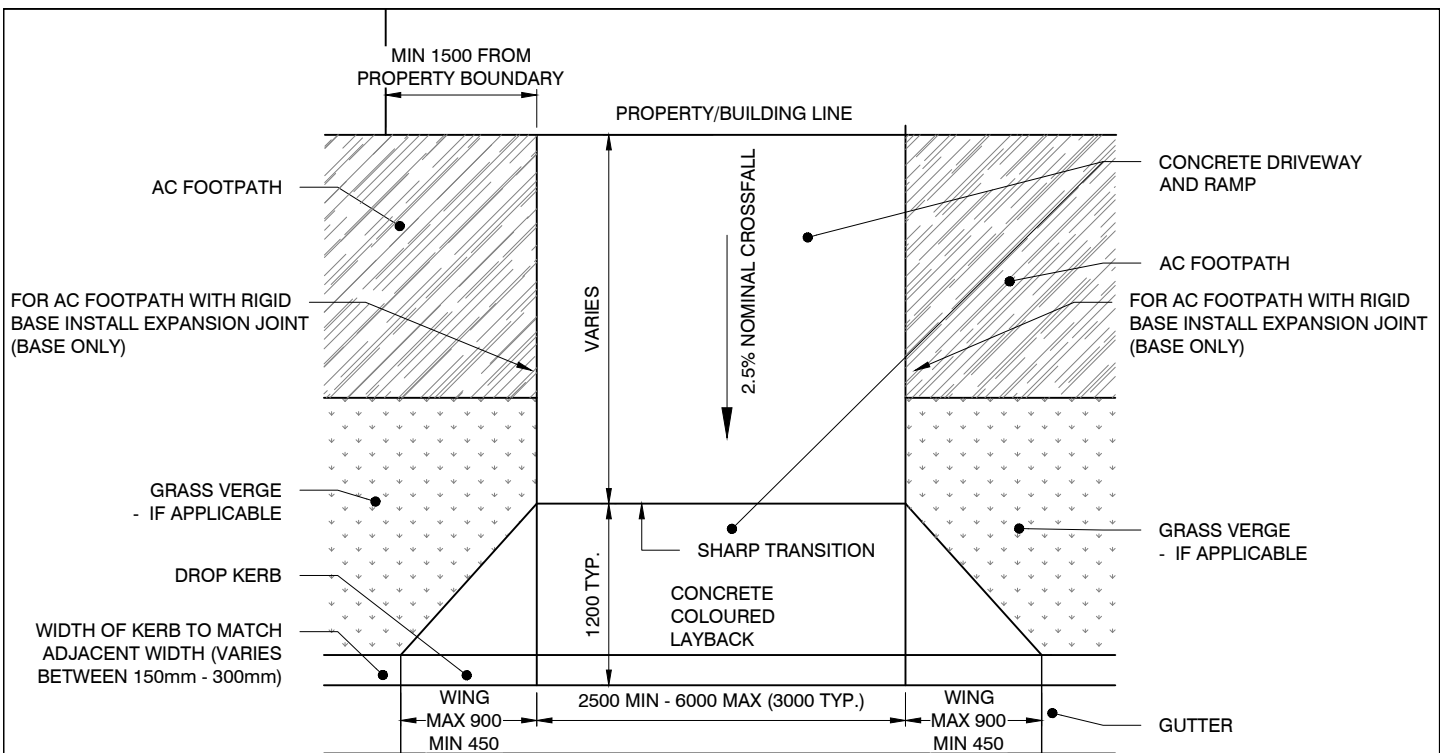
SECTION (WITH STONE KERB)
1:20



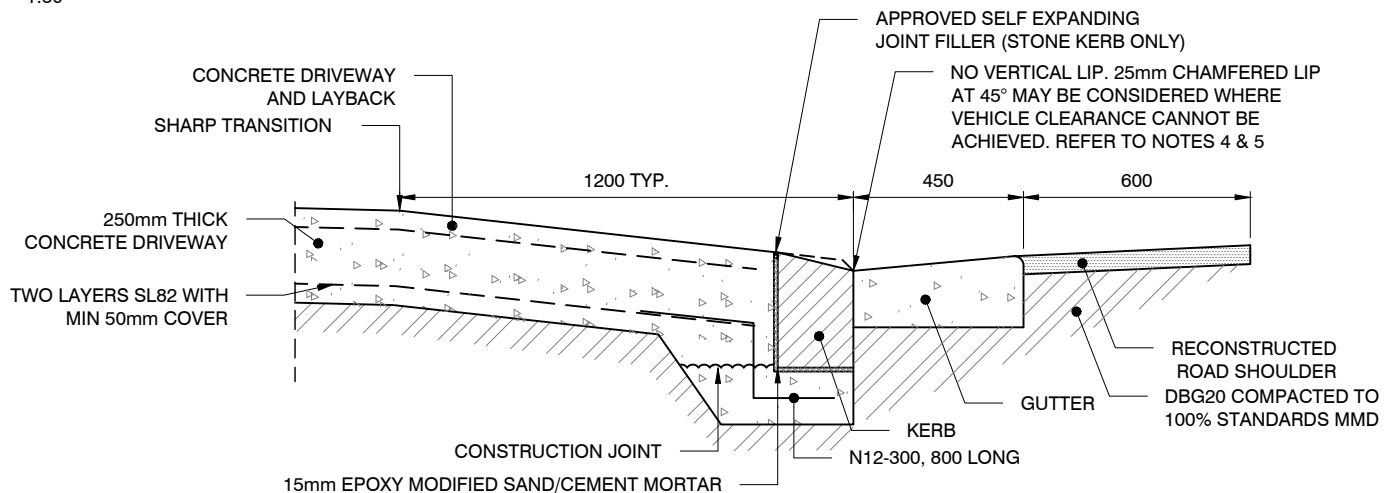
SECTION (WITH CONCRETE KERB)
1:20

NOTES:

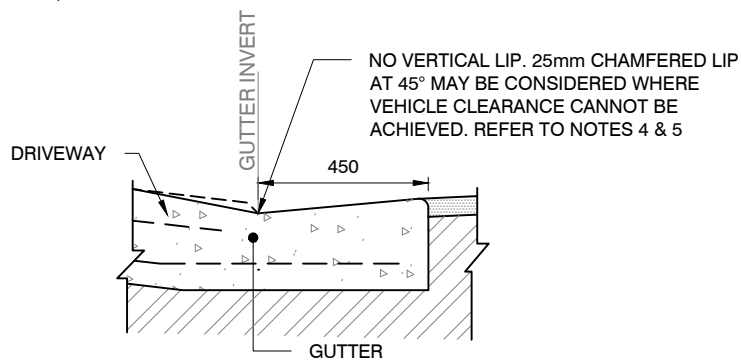
1. THIS DRIVEWAY SUIT COMMERCIAL VEHICULAR CROSSINGS SUBJECT TO AXLE LOADING OF 10 TONNES OR LIGHTER.
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE, UNLESS APPROVED OTHERWISE.
3. ALL DRIVEWAY CROSSINGS TO INCLUDE REINFORCED CONCRETE SLAB.
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7. FOR DRIVEWAYS WIDER THAN 6.0m A TOOL JOINT SHALL BE PROVIDED ALONG THE CENTRE OF THE CONCRETE DRIVEWAY.
8. CONCRETE TO BE MINIMUM 32MPa.
6. SUB-BASE SHALL BE 100mm THICK DGB20 COMPACTED TO STANDARD 98% MDD
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



PLAN
1:50



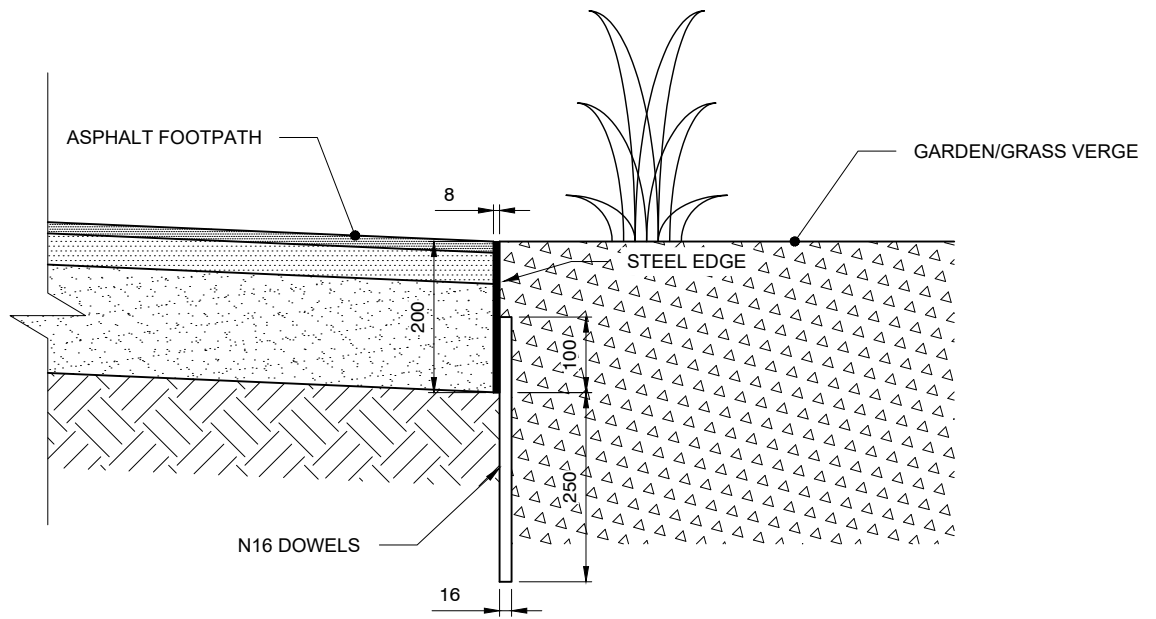
SECTION (WITH STONE KERB)
1:20



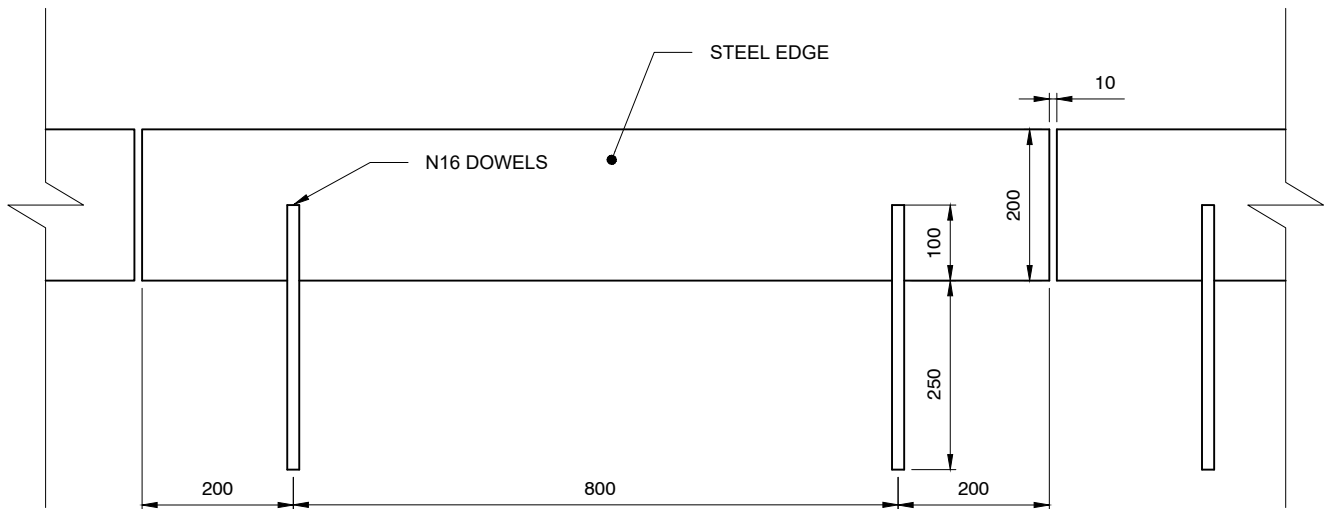
SECTION (WITH CONCRETE KERB)
1:20

NOTES:

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8. CONCRETE TO BE MINIMUM 32MPa.
9. SUB-BASE SHALL BE 100mm THICK DGB20.
10. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



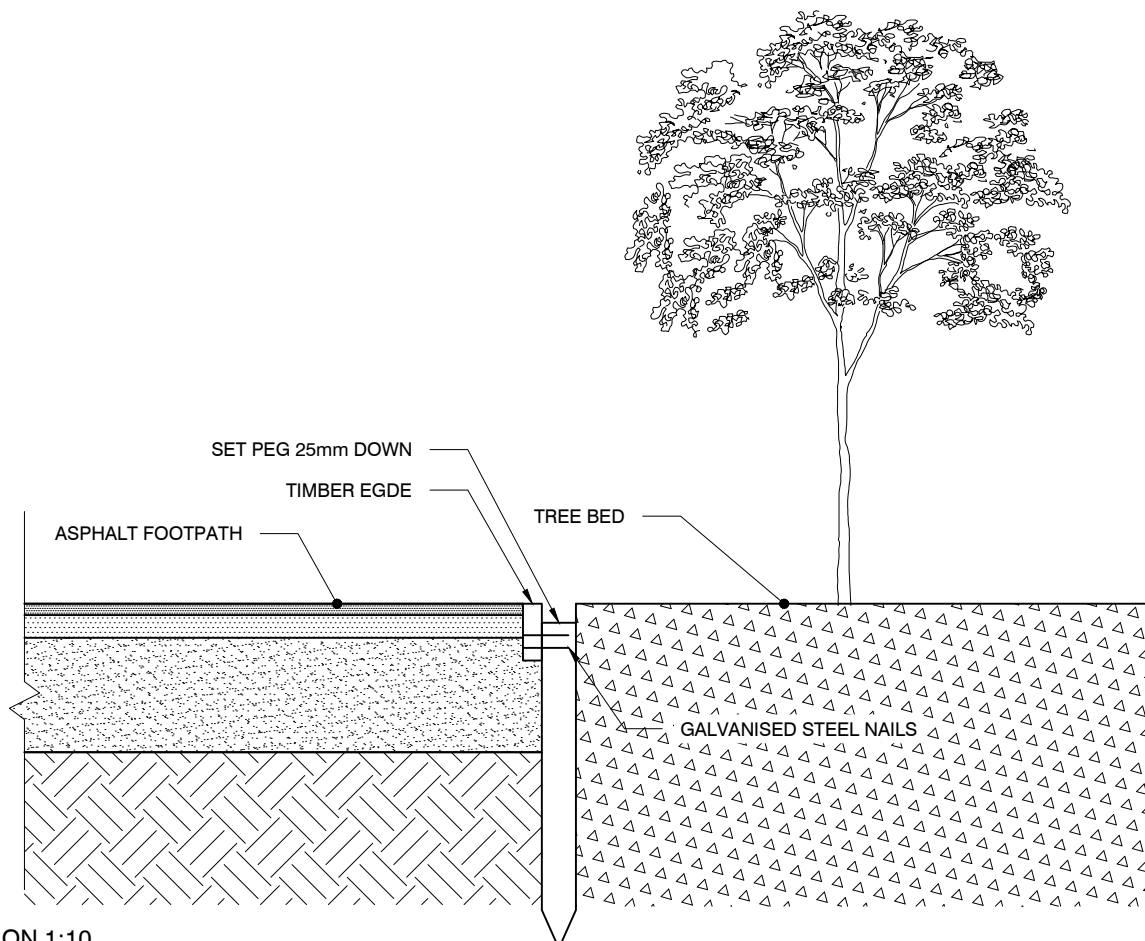
SECTION 1:10



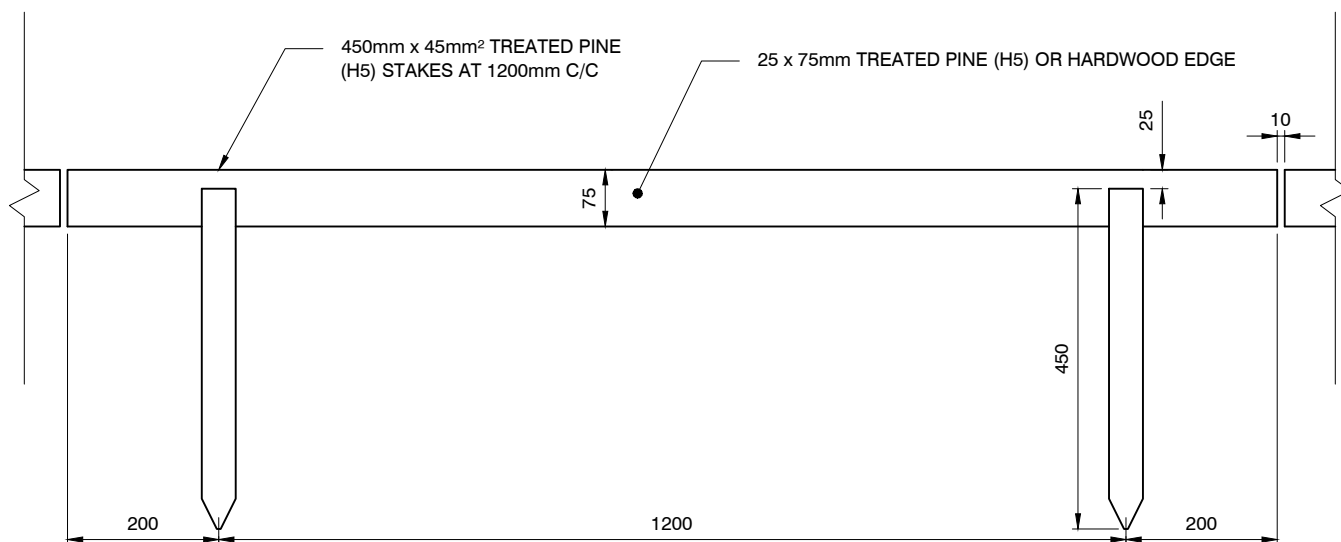
ELEVATION 1:10

NOTES:

1. ALL STEELWORK TO BE HOT DIPPED GALVANISED.
2. ALL STEELWORK BELOW GROUND LEVEL TO BE PAINTED WITH TWO COATS OF BITUMINOUS PAINT.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

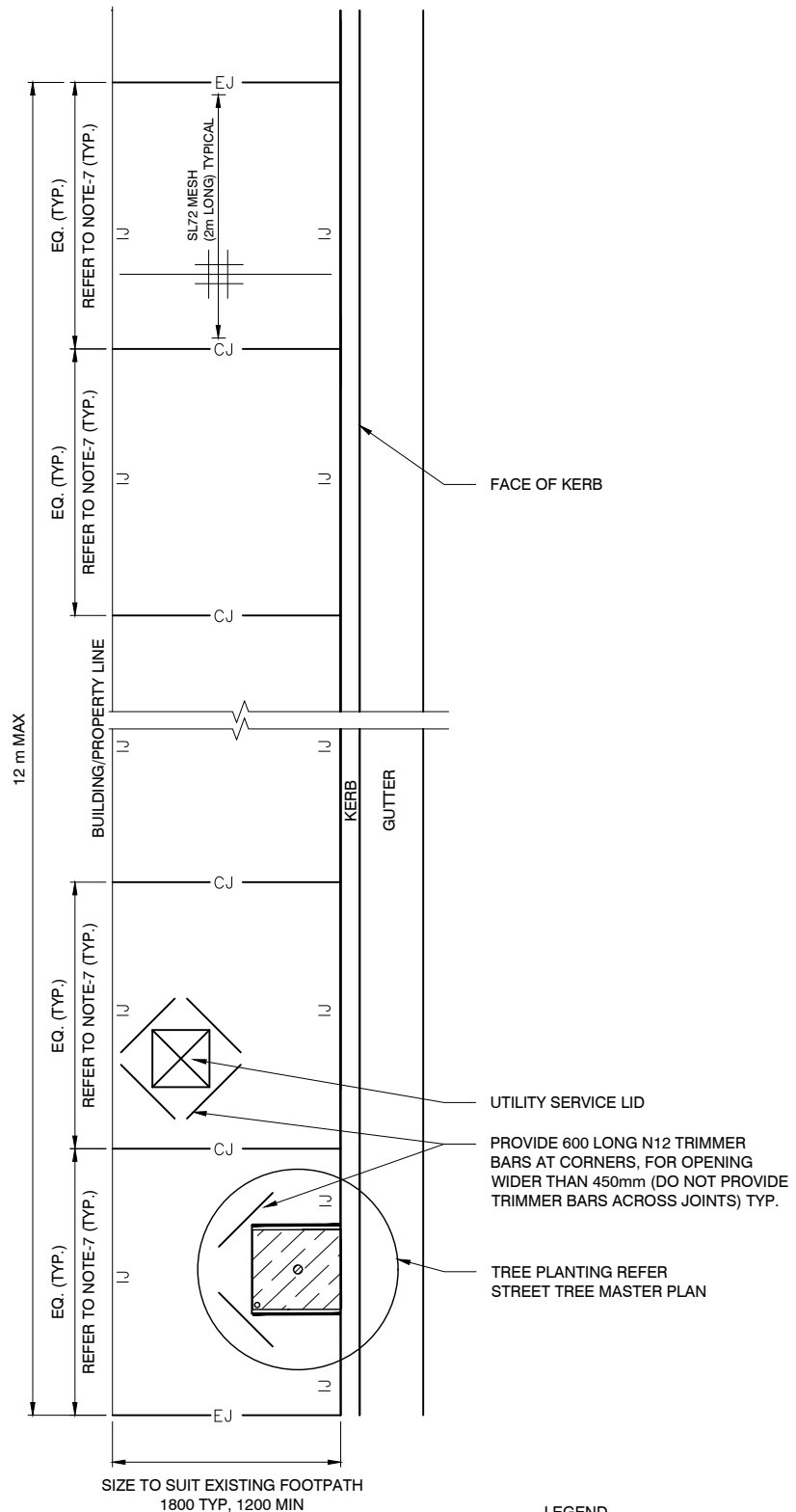


SECTION 1:10



ELEVATION 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

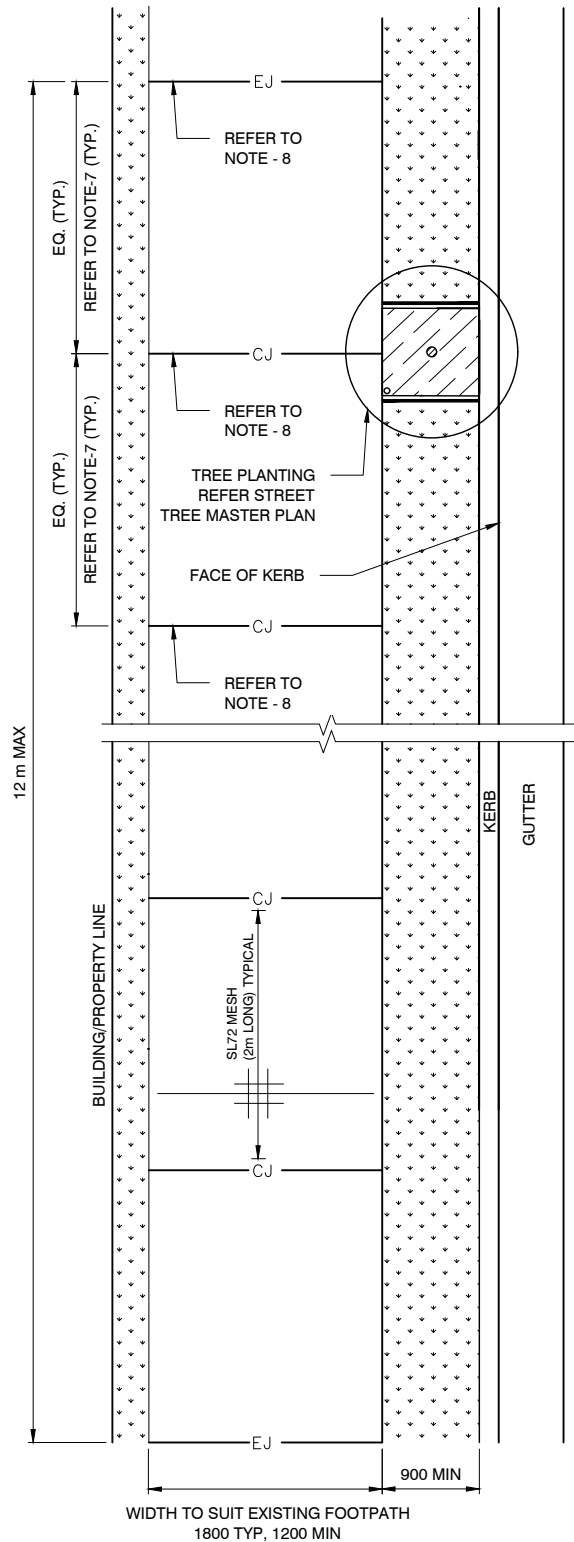
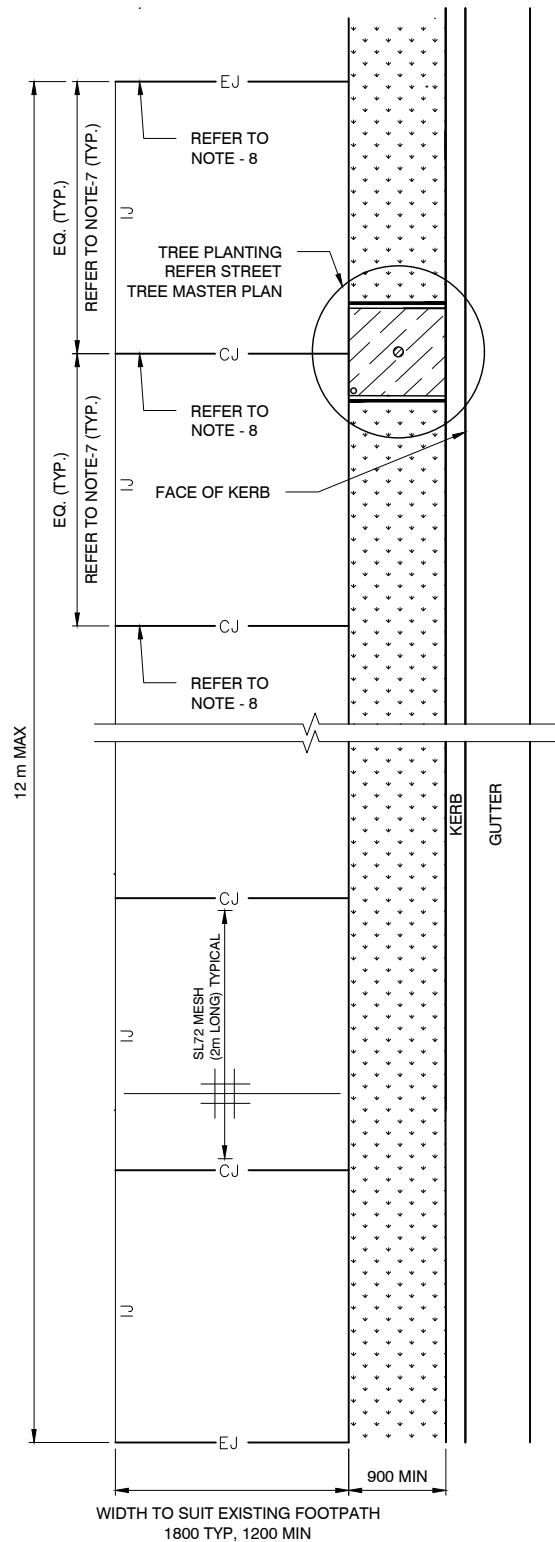


NOTES:

1. DETAIL ONLY TO BE USED IF MATCHING EXISTING, CITY REPRESENTATIVE TO APPROVE.
2. ALL EXPOSED CONCRETE SURFACES MUST BE FINISHED WITH A MEDIUM BROOM FINISH GENERALLY PERPENDICULAR TO THE DIRECTION OF TRAVEL.
3. ALL CONCRETE SLABS MUST HAVE AN ISOLATION JOINT ALONG THE BUILDING LINE, KERB LINE AND ANY PENETRATIONS (EXCEPT WHEN USING BRICK KERBS).
4. ANY FOOTPATH THAT MAY BE SUBJECT TO VEHICULAR LOADS (SUCH AS GARBAGE TRUCKS IN NARROW LANES OR AT INTERSECTIONS WITH A TIGHT TURNING CIRCLE WHERE VEHICLES MAY MOUNT THE FOOTWAY) MUST HAVE AT LEAST 150 mm THICK REINFORCED CONCRETE FOOTPATH WITH SL92 MESH (40 mm TOP COVER).
5. CONCRETE FOOTWAYS TO BE 1.8 m WIDE TYPICAL (MINIMUM OF 1.2 m WIDE, OR AS DIRECTED BY COUNCIL).
6. TYPICALLY ALLOW FOR EXPANSION JOINTS AT 12 m (MAX) SPACING
7. CONTRACTION JOINT SPACINGS OR SLAB PANELS TO A MAXIMUM ASPECT RATIO OF 1:1.5 (NO GREATER THAN 1.5 TIMES THE WIDTH OF THE PATH) AND EQUALLY SPACED BETWEEN EXPANSION JOINTS.
8. TRIPSTOP OR EQUIVALENT TO BE USED ON CONTRACTION/ CONTROL JOINTS ADJACENT TO TREE. TRIPSTOP JOINTS TO EXTEND ONE FULL PANEL PAST DRIP LINE OF MATURE TREE.
9. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

LEGEND

CJ	CONTRACTION JOINTS
EJ	EXPANSION JOINTS
IJ	ISOLATION JOINTS



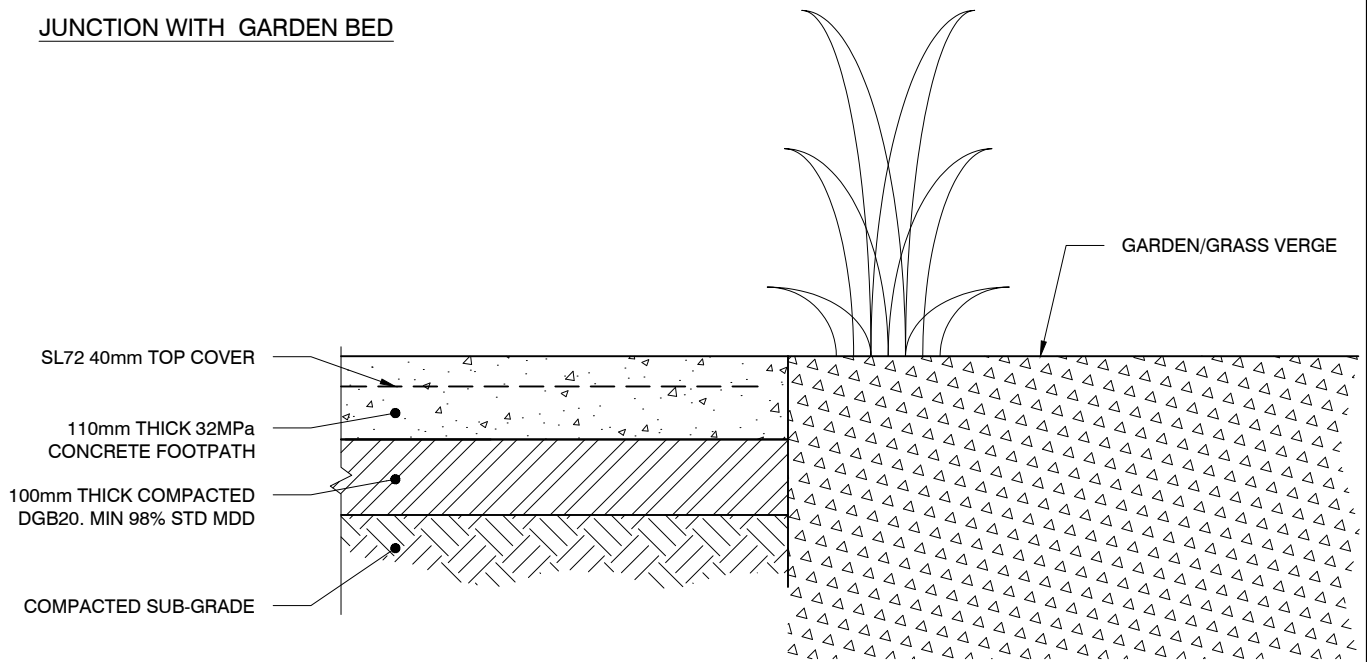
NOTES:

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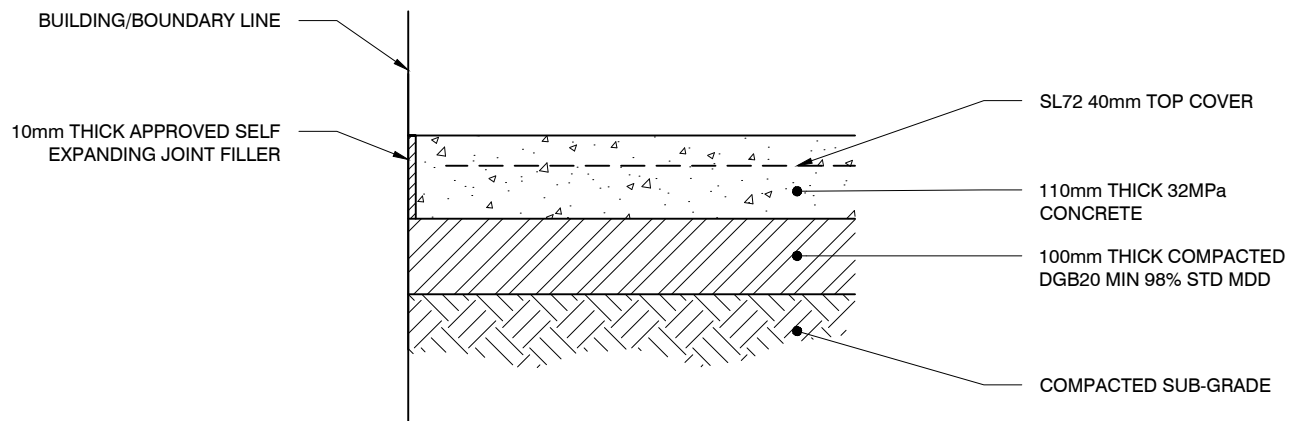
LEGEND

CJ	CONTRACTION JOINTS
EJ	EXPANSION JOINTS
IJ	ISOLATION JOINTS

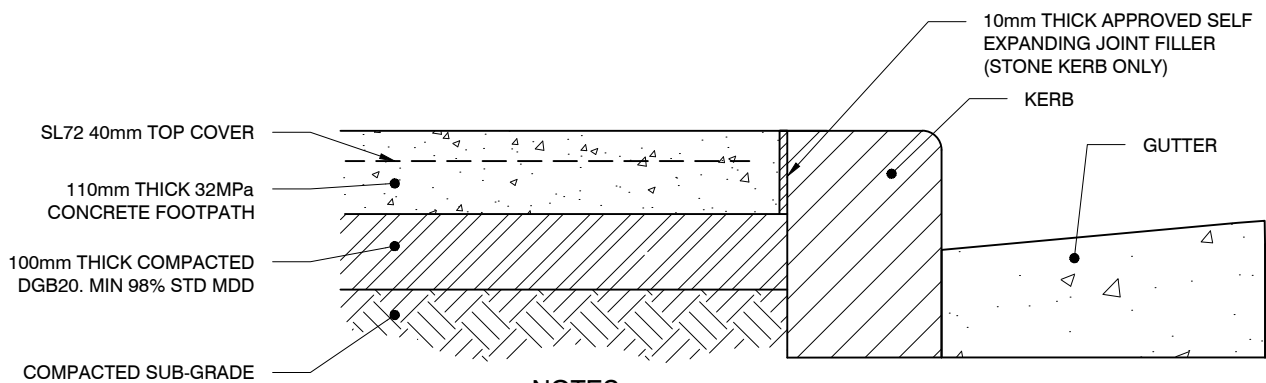
JUNCTION WITH GARDEN BED



JUNCTION WITH BUILDING



JUNCTION WITH BACK OF KERB

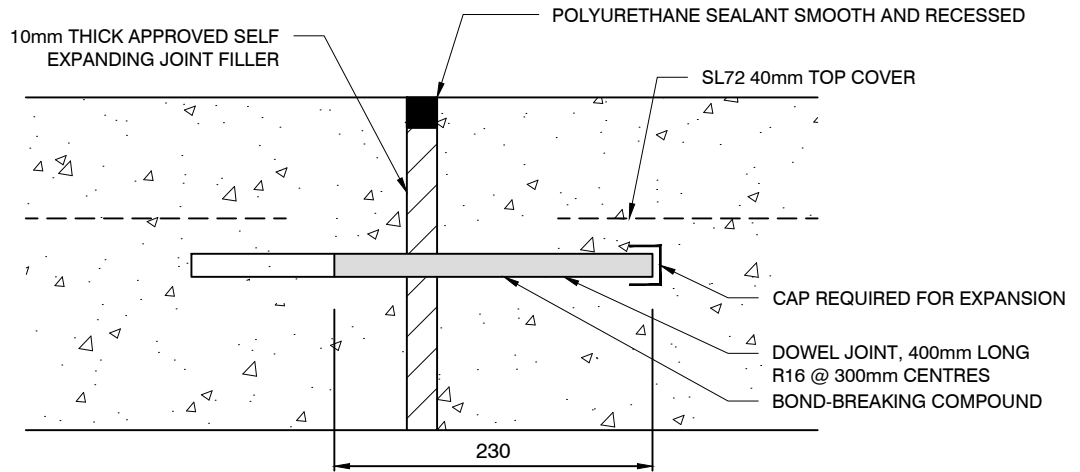


NOTES:

1. FOR KERB AND GUTTER DETAILS REFER TO STANDARD DRAWING # 1.1.1 & 1.1.2
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

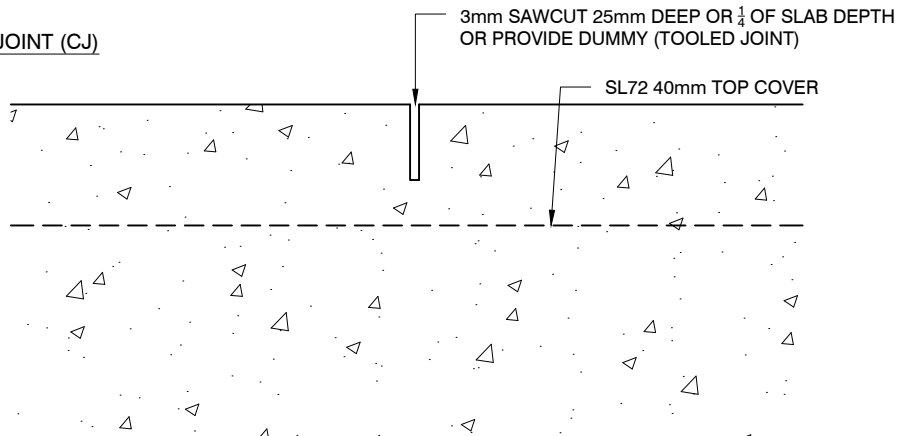
SECTION 1:10

EXPANSION JOINT (EJ)

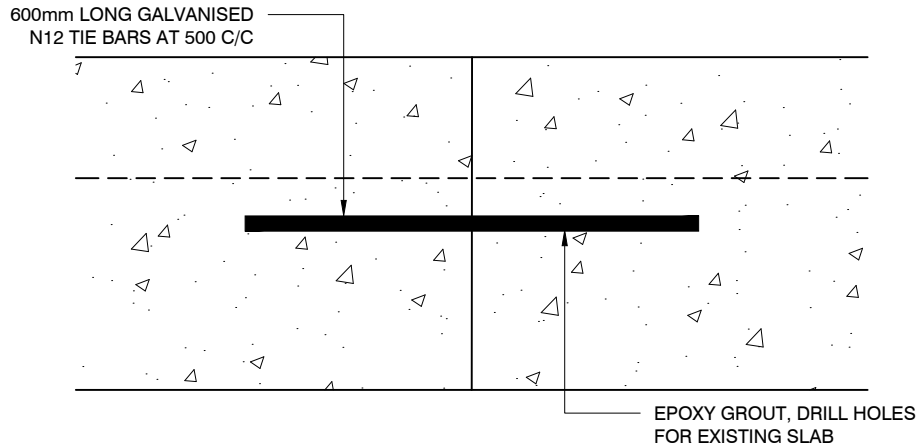


NOTE: BOND-BREAKING COMPONENT AND END CAP MAY BE REPLACED WITH A PURPOSE-MADE DOWEL SLEEVE.

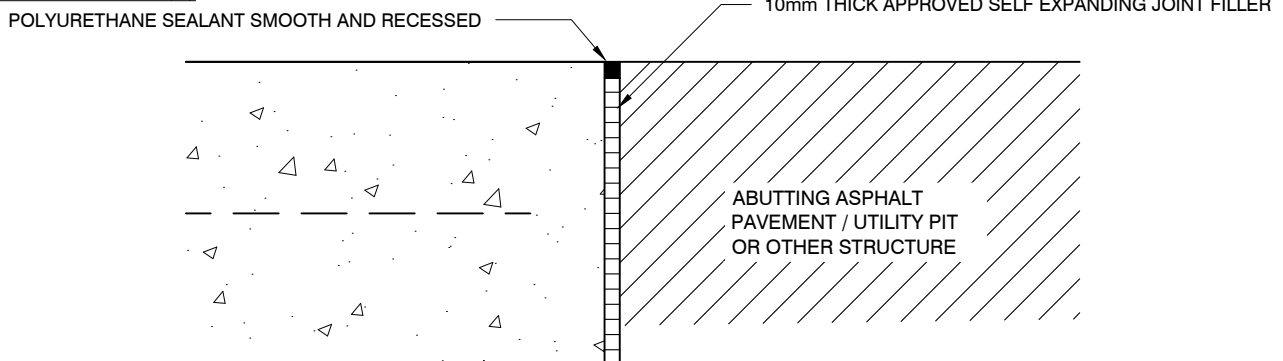
CONTRACTION/CONTROL JOINT (CJ)



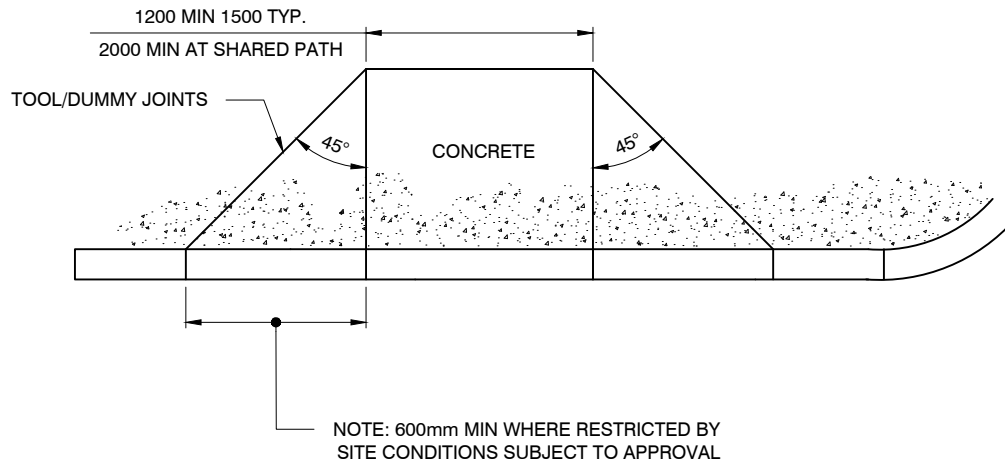
CONSTRUCTION JOINT / JUNCTION WITH EXISTING CONCRETE SLAB



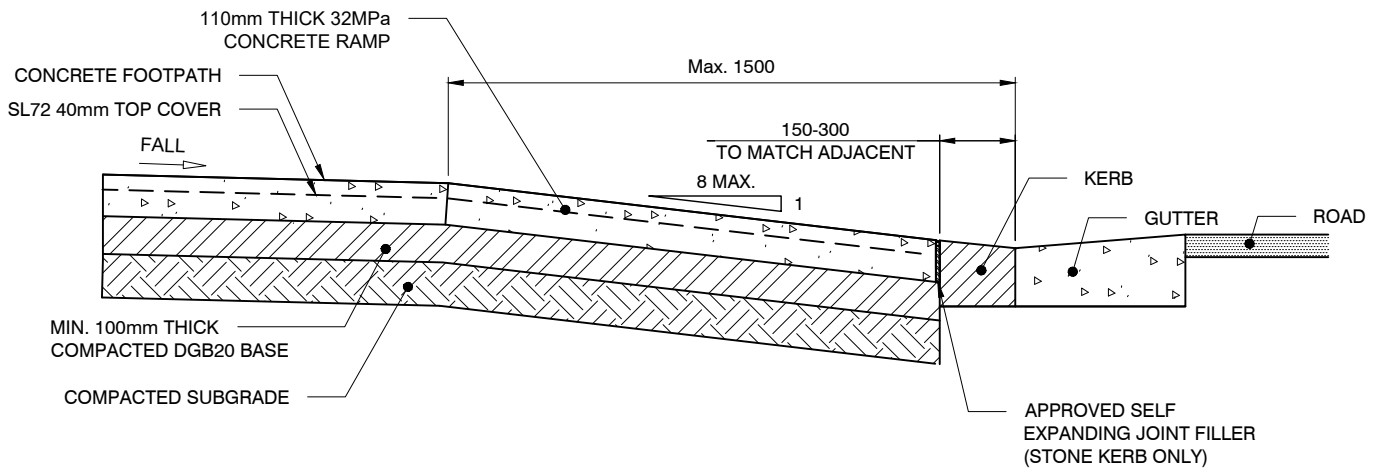
ISOLATION JOINT (IJ)



NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



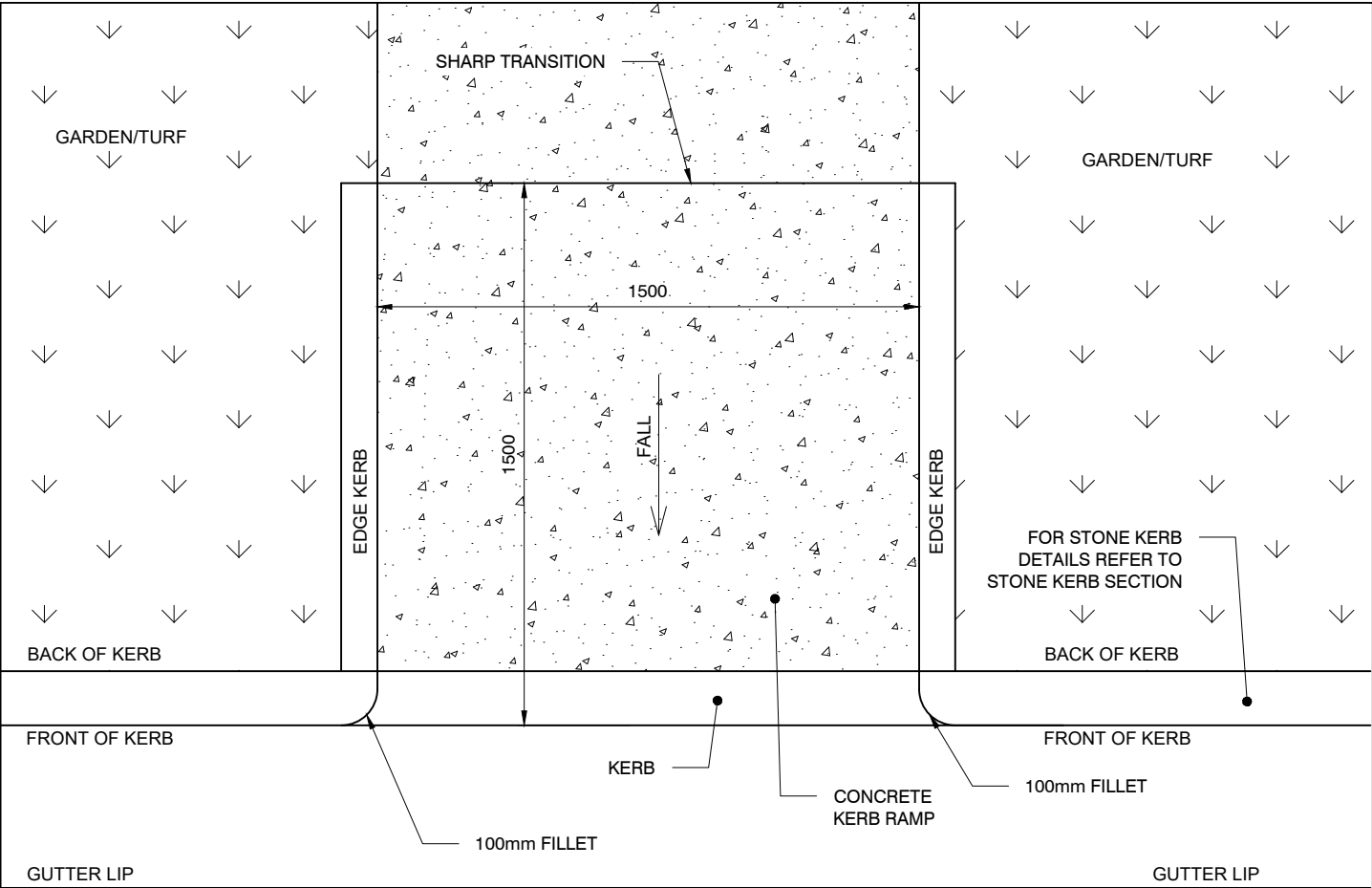
PLAN 1:50



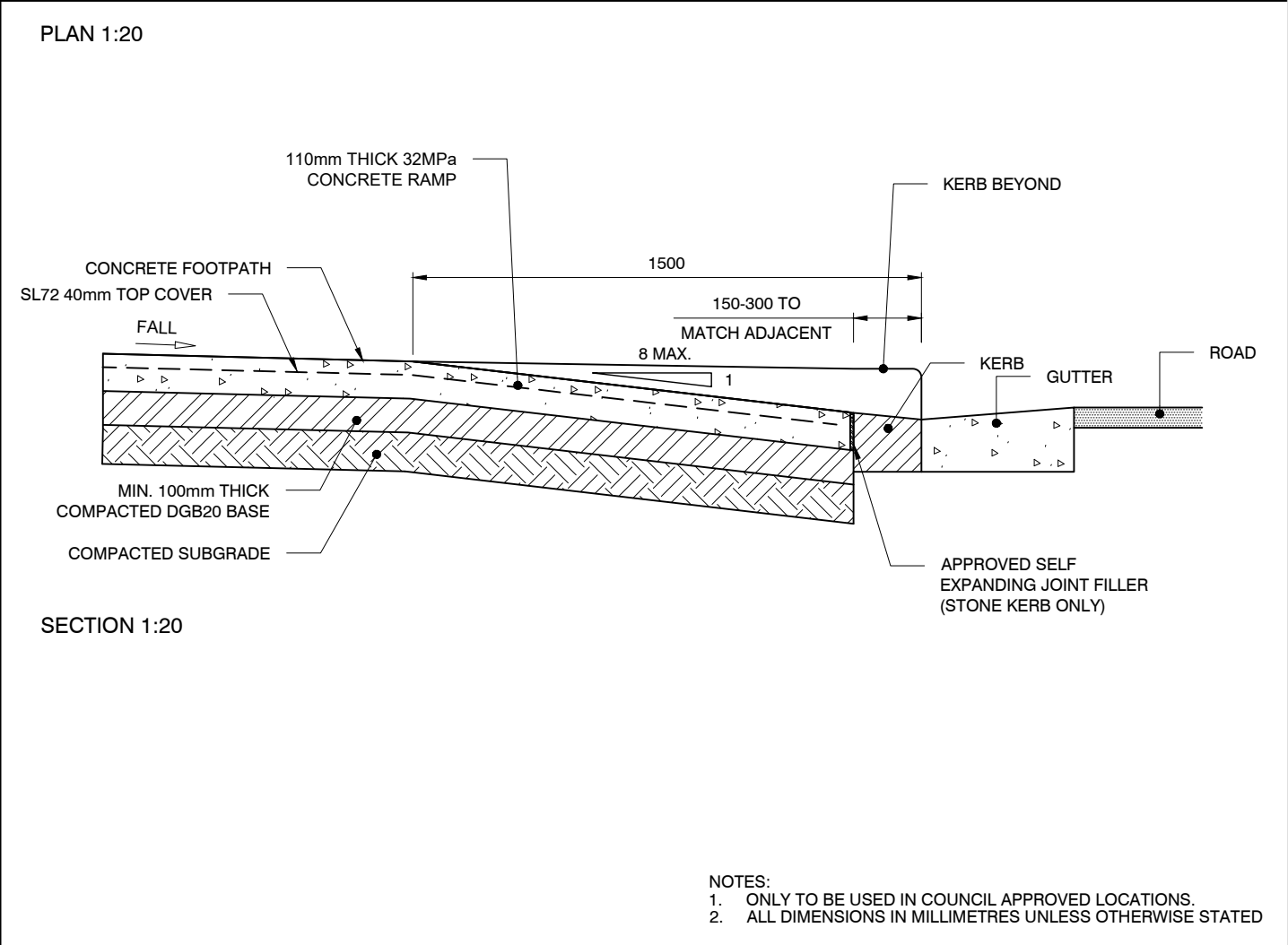
SECTION 1:20

NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. THE 45° WING ANGLE ON KERB RAMPS IS THE GENERAL STANDARD AND TNSW RECOMMENDATION. IF RESTRICTED BY SITE CONDITIONS, THE WING ANGLE CAN BE REDUCED AND DESIGNED TO SUIT SUBJECT TO APPROVAL.

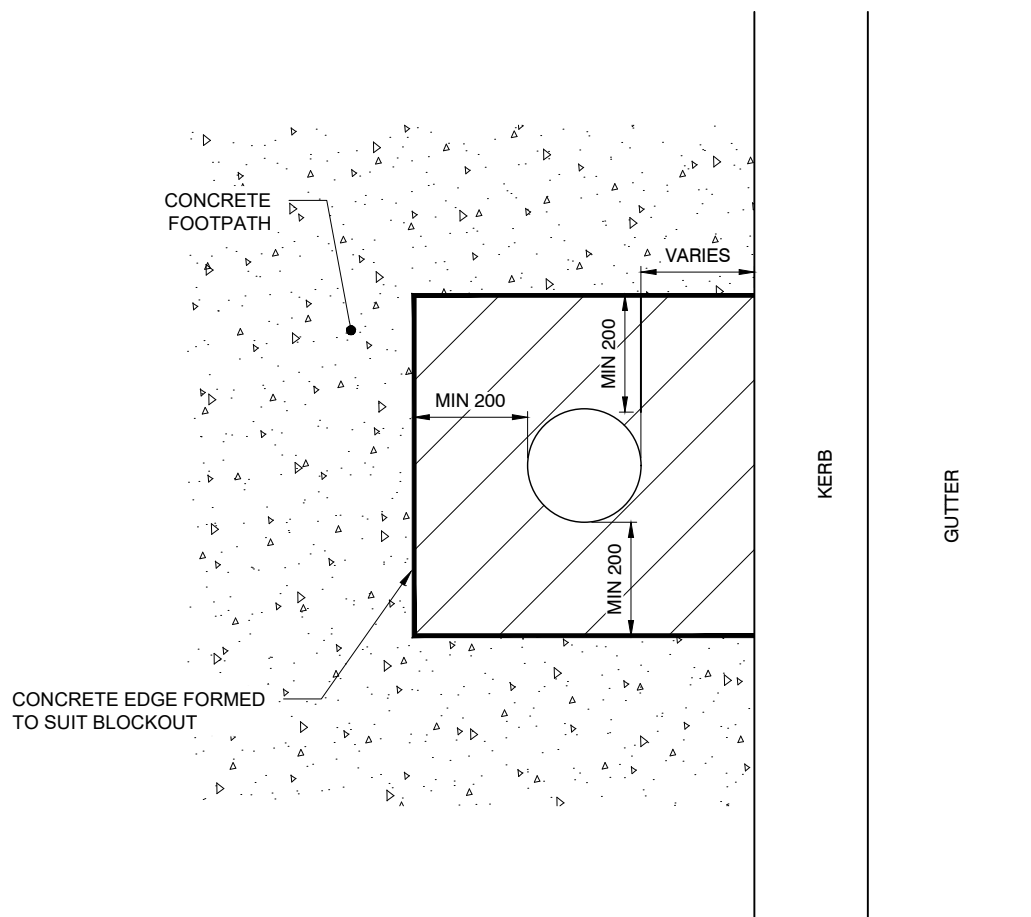


PLAN 1:20

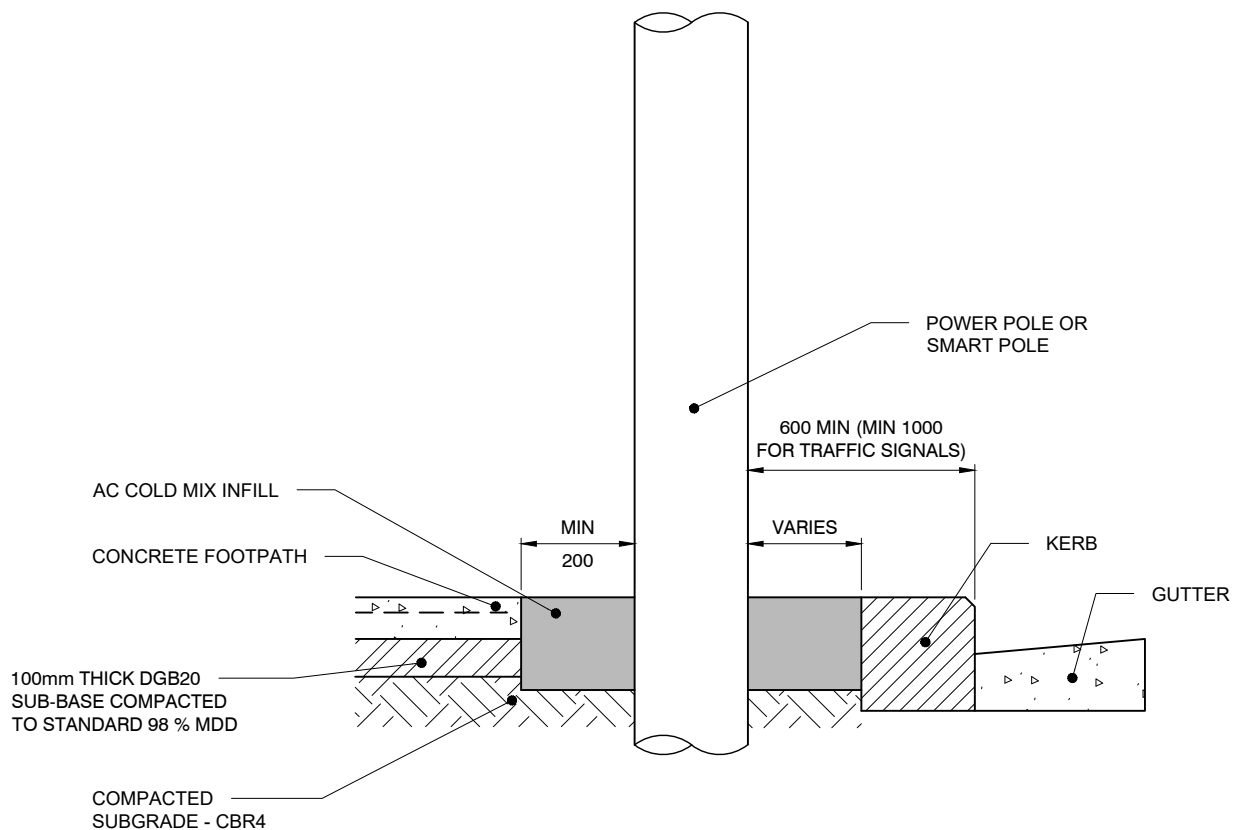


SECTION 1:20

- NOTES:
1. ONLY TO BE USED IN COUNCIL APPROVED LOCATIONS.
 2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

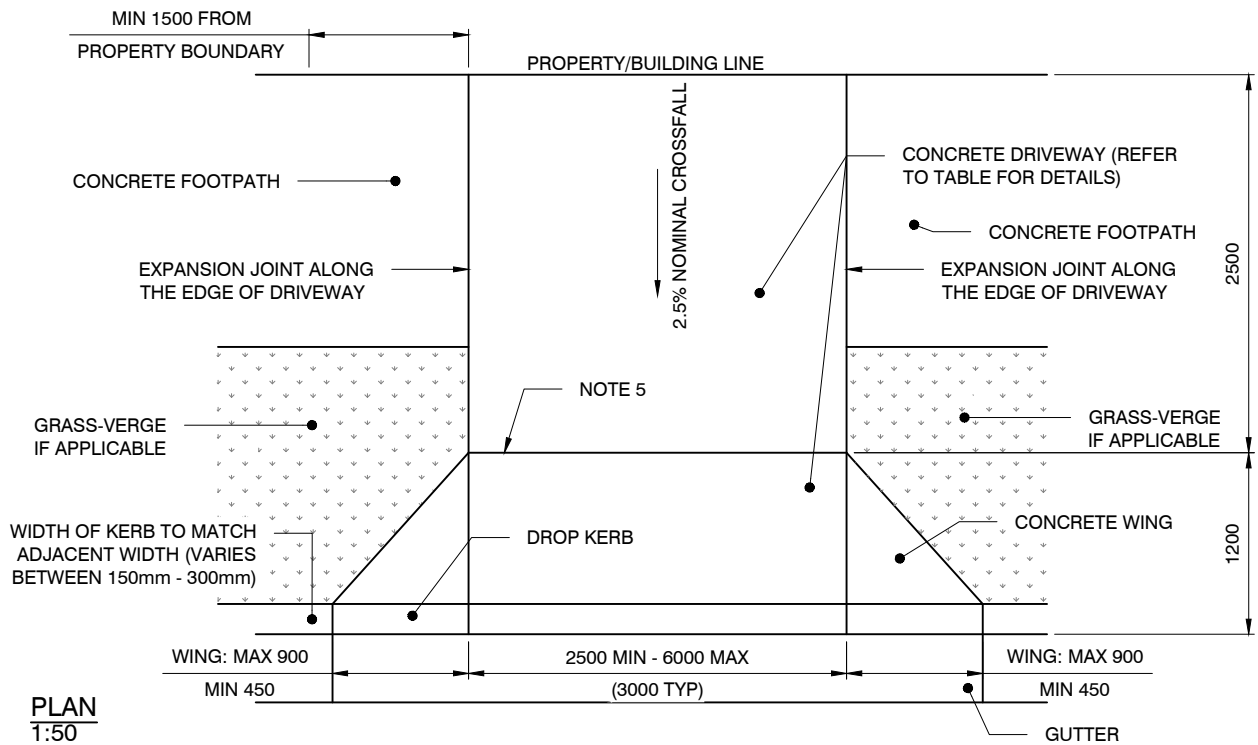


PLAN 1:20



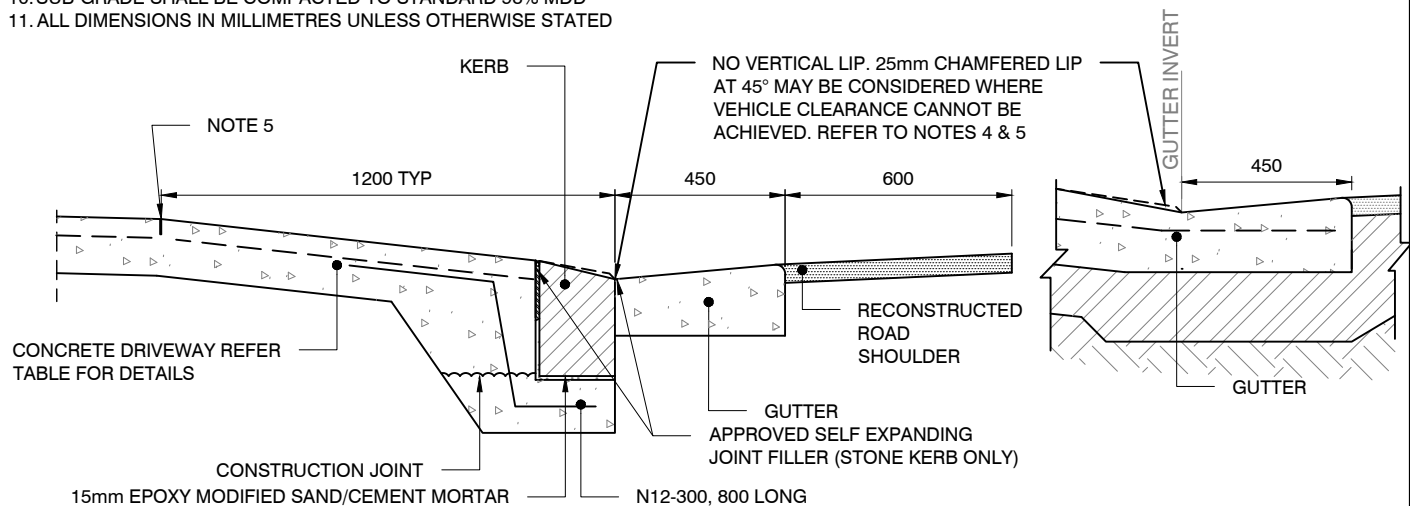
SECTION 1:20

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

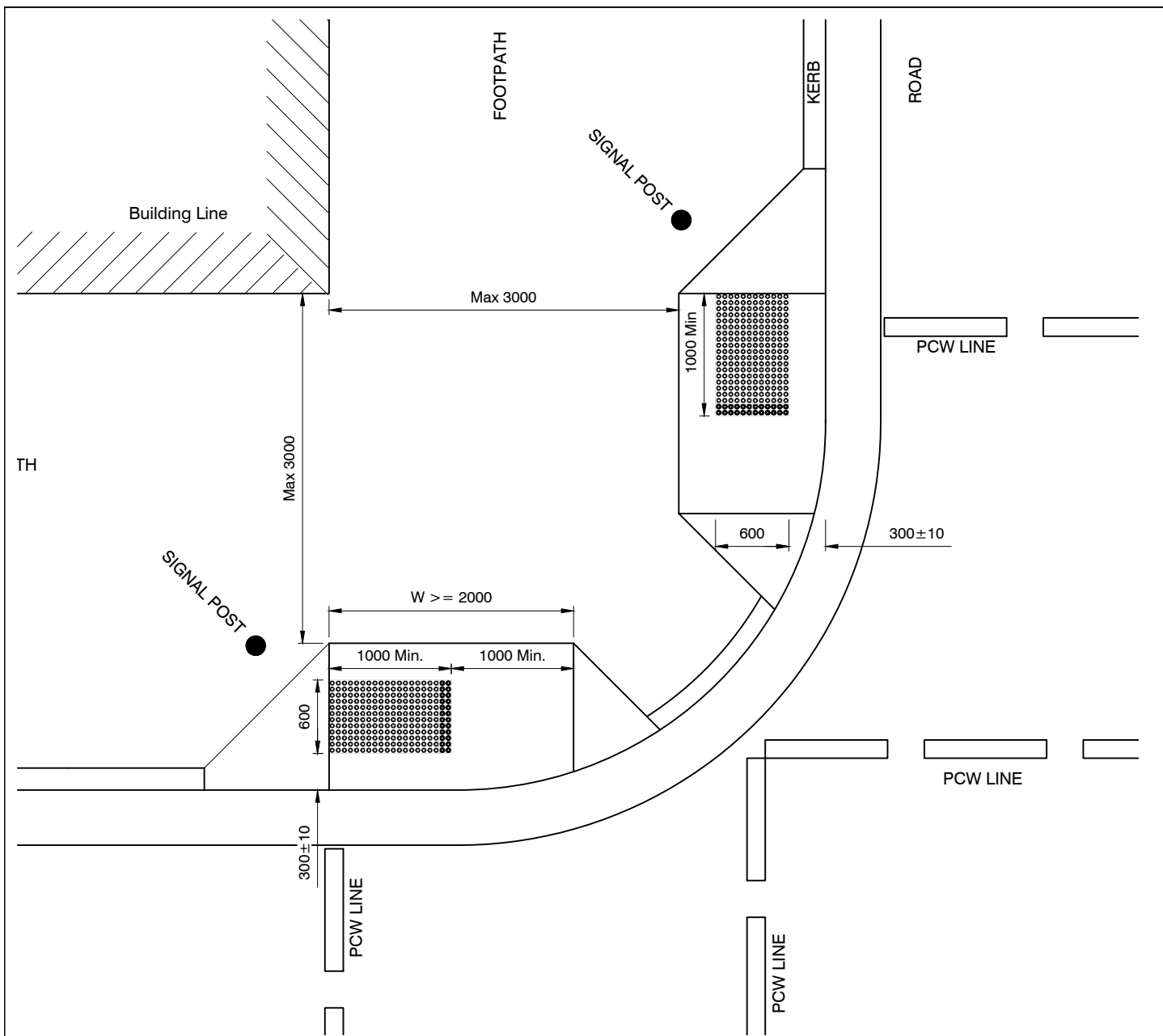


NOTES:

1. ALIGN CENTRE OF DRIVEWAY WITH ENTRY
2. DRIVEWAY TO BE GENERALLY PERPENDICULAR TO KERB LINE, UNLESS APPROVED OTHERWISE.
3. DRIVEWAY CONCRETE SHALL BE WOOD FLOAT FINISHED.
4. VERTICAL AND HORIZONTAL CLEARANCE SHALL BE CHECKED BY THE DESIGNER IN ACCORDANCE WITH AS2890.1.
5. VERTICAL LIP ADJACENT TO CYCLEWAY MUST BE APPROVED BY CITY'S REPRESENTATIVE
6. FOR NARROW FOOTPATHS LENGTH OF LAYBACK TO BE AS SHORT AS POSSIBLE, SUBJECT TO VEHICLE CLEARANCE OR LAYBACK ONLY TO BE USED IN APPROVED APPLICATIONS.
7. FOR DRIVEWAYS WIDER THAN 6.0m A TOOL JOINT SHALL BE PROVIDED ALONG THE CENTRE OF THE DRIVEWAY.
8. PROVIDE CONTRACTION/ CONTROL JOINT AT CHANGE IN GRADE AND IN LINE WITH FOOTPATH
9. SUB-BASE SHALL BE 100mm THICK DGB20
10. SUB-GRADE SHALL BE COMPACTED TO STANDARD 98% MDD
11. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



DRIVEWAY SPECIFICATIONS			
DRIVEWAY USE	CONCRETE STRENGTH	THICKNESS	REINFORCEMENT
SINGLE RESIDENTIAL	32MPa	150	SL82, 50 TOP COVER
MULTI RESIDENTIAL	32MPa	200	SL82, 50 TOP COVER
COMMERCIAL/ INDUSTRIAL	32MPa	250	TWO LAYERS SL82 50 COVER TOP & BOTTOM



LEGEND



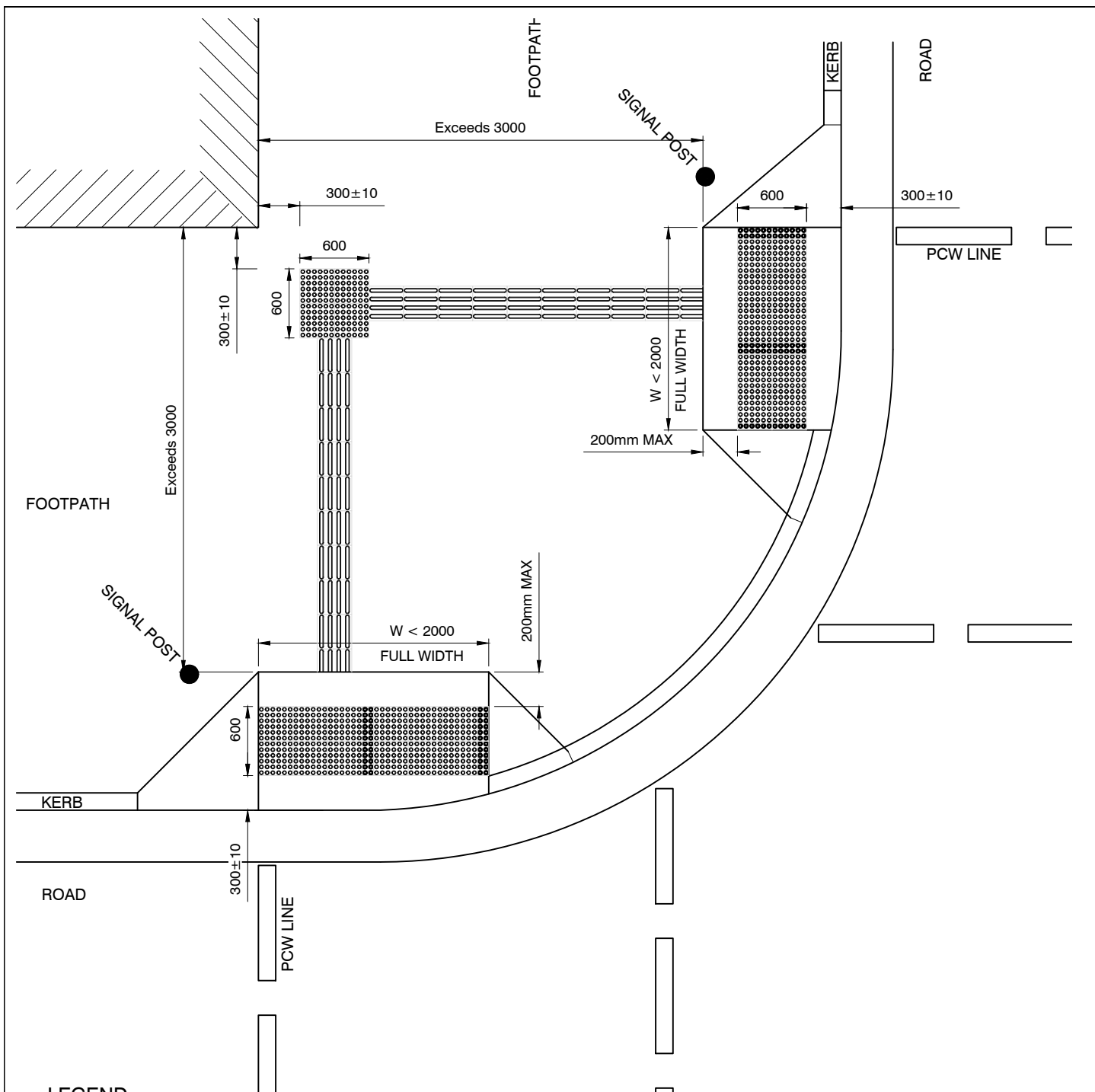
WARNING TGSIs



DIRECTIONAL TGSIs

NOTES:

- ALL WORK SHOULD BE CARRIED OUT IN ACCORDANCE WITH THE AS/NZS 1428.4.1 2009 & CITY'S "SYDNEY STREETS TECHNICAL SPECIFICATION", OR AS DIRECTED BY THE CITY'S REPRESENTATIVE.
- FOR RAMPs WIDER THAN 2 METRES, WIDTH OF TACTILE INDICATORS ON THE RAMP SHOULD BE MINIMUM 1000MM. WHEN WIDTH OF RAMP IS LESS THAN OR EQUAL TO TWO (2) METRES WIDTH OF TACTILE INDICATOR'S BLOCK SHALL MATCH THE RAMP.
- TYPE AND COLOUR OF TGSIs SHALL BE IN ACCORDANCE WITH CITY OF SYDNEY'S TECHNICAL SPECIFICATIONS UNLESS SPECIFIED FOR THE PROJECT. REFER TO DRAWING# 2.7.4 FOR THE COLOUR AND TYPES.
- DIFFERENT COLOURS AND TYPES OF TGSIs MAY BE USED FOR SPECIFIC PROJECTS, IN WHICH CASE CITY OF SYDNEY'S APPROVAL MUST BE OBTAINED IN THE DESIGN PHASE.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



LEGEND



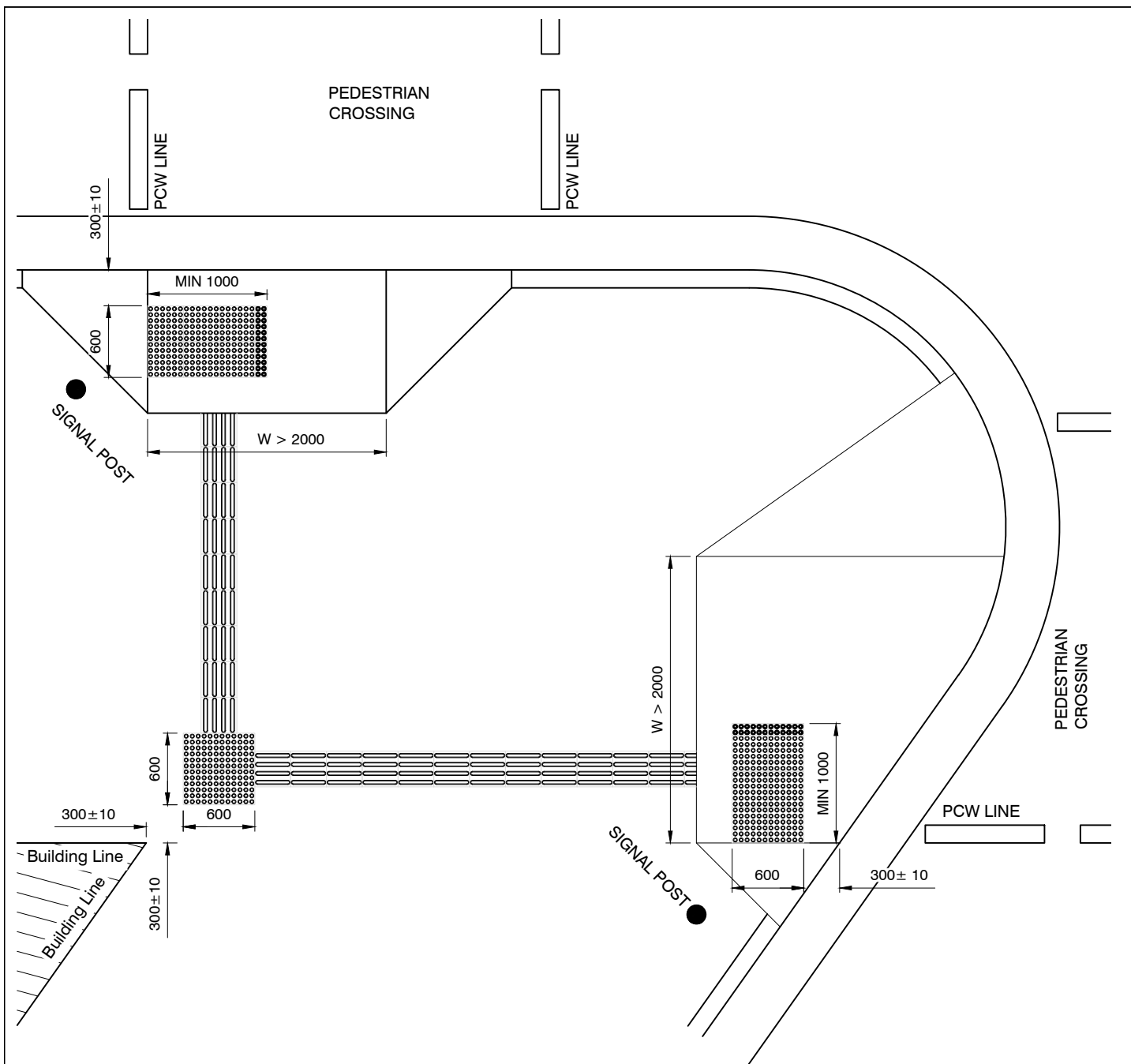
WARNING TGSIs



DIRECTIONAL TGSIs

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3. TYPE AND COLOUR OF TGSIs SHALL BE IN ACCORDANCE WITH CITY OF SYDNEY'S TECHNICAL SPECIFICATIONS UNLESS SPECIFIED FOR THE PROJECT. REFER TO DRAWING# 2.7.4 FOR THE COLOUR AND TYPES.
4. DIFFERENT COLOURS AND TYPES OF TGSIs MAY BE USED FOR SPECIFIC PROJECTS, IN WHICH CASE CITY OF SYDNEY'S APPROVAL MUST BE OBTAINED IN THE DESIGN PHASE.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



LEGEND



WARNING Tactile Guide Surface Indicator



DIRECTIONAL Tactile Guide Surface Indicator

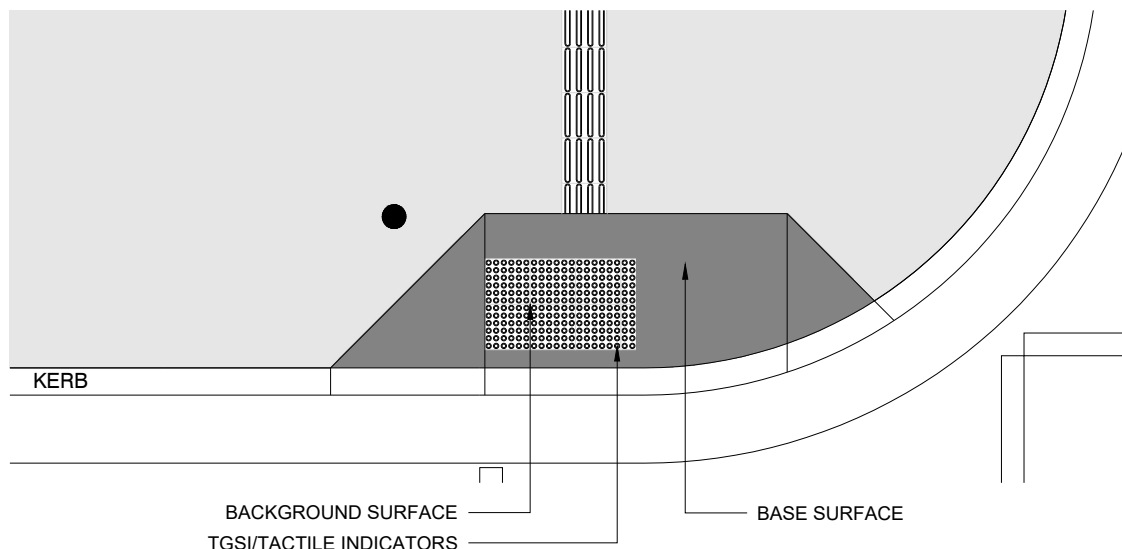
NOTES:

1. ALL WORK SHOULD BE CARRIED OUT IN ACCORDANCE WITH THE AS/NZS 1428.4.1 2009 & CITY'S "SYDNEY STREETS TECHNICAL SPECIFICATION", OR AS DIRECTED BY THE CITY'S REPRESENTATIVE.
2. FOR RAMPS WIDER THAN 2 METRES, WIDTH OF TACTILE INDICATORS ON THE RAMP SHOULD BE MINIMUM 1000MM. WHEN WIDTH OF RAMP IS LESS THAN OR EQUAL TO TWO (2) METRES WIDTH OF TACTILE INDICATOR'S BLOCK SHALL MATCH THE RAMP.
3. TYPE AND COLOUR OF Tactile Guide Surface Indicators SHALL BE IN ACCORDANCE WITH CITY OF SYDNEY'S TECHNICAL SPECIFICATIONS UNLESS SPECIFIED FOR THE PROJECT. REFER TO DRAWING# 2.7.4 FOR THE COLOUR AND TYPES.
4. DIFFERENT COLOURS AND TYPES OF Tactile Guide Surface Indicators MAY BE USED FOR SPECIFIC PROJECTS, IN WHICH CASE CITY OF SYDNEY'S APPROVAL MUST BE OBTAINED IN THE DESIGN PHASE.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

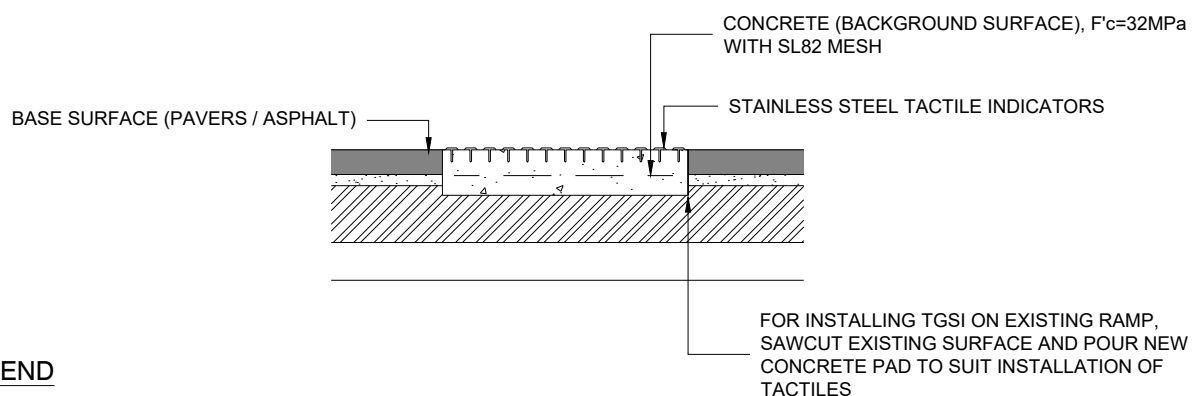
TGSI /TACTILE INDICATORS MATERIAL

Item	Base Surface	Background Surface	Tactile Type
1	Granite	Granite	Stainless Steel
2	Brick Pavers	Concrete	Stainless Steel
3	Concrete Pavers	Concrete/Concrete Pavers ⁽⁴⁾	Stainless Steel
4	Asphalt	Concrete	Stainless Steel
5	Concrete	Concrete coloured in with black oxide	Stainless Steel

PLAN



TGSI INSTALLATION DETAIL FOR CONCRETE PAVERS/BRICK PAVERS/ ASPHALT



LEGEND



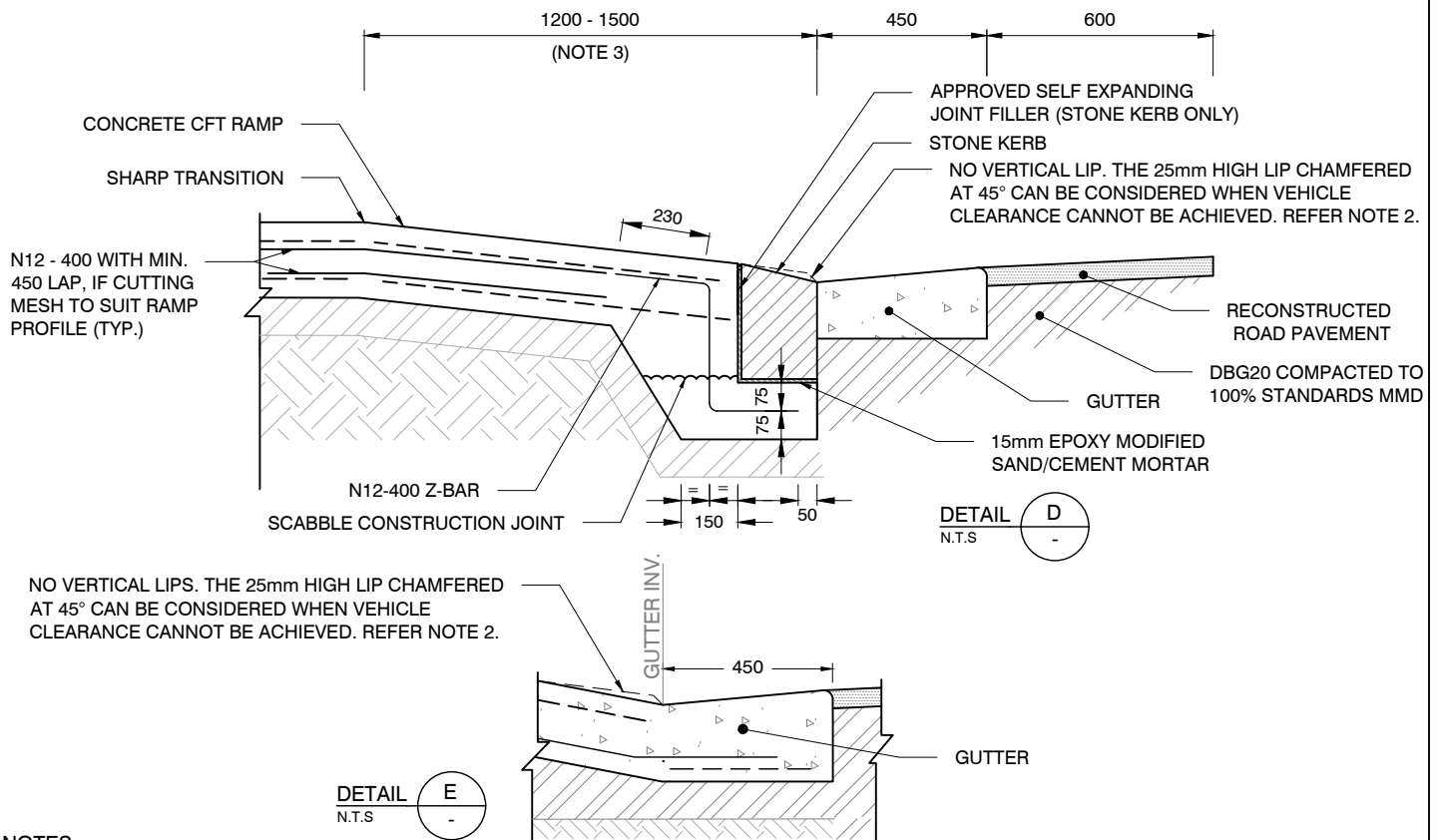
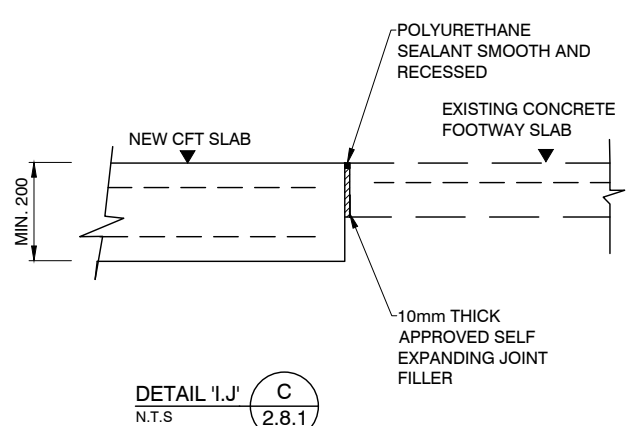
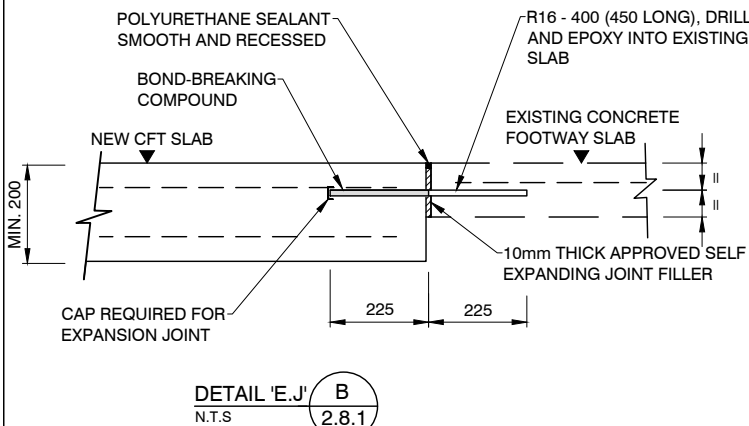
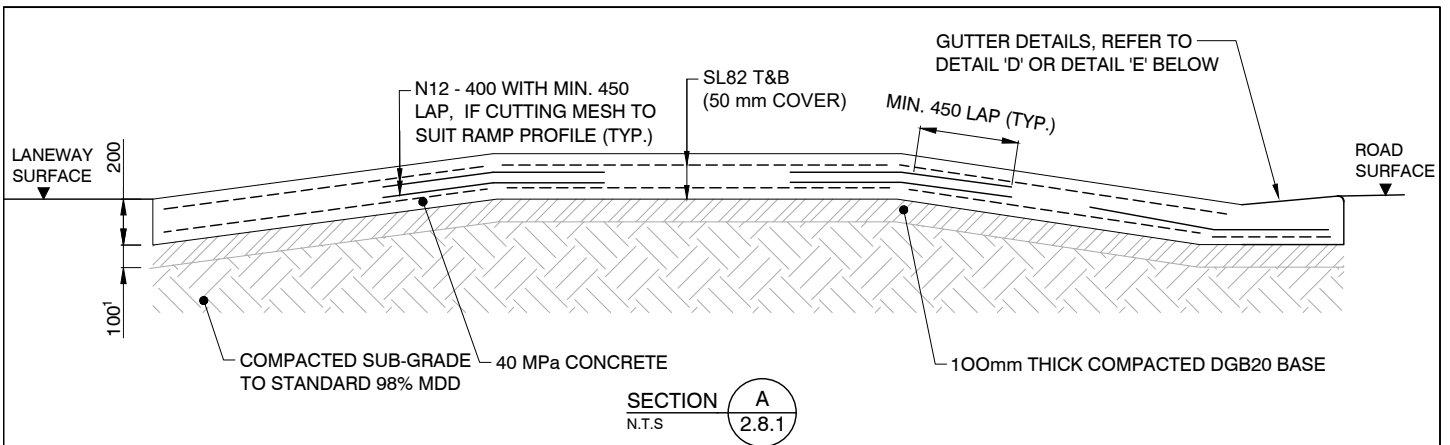
WARNING TGSI



DIRECTIONAL TGSI

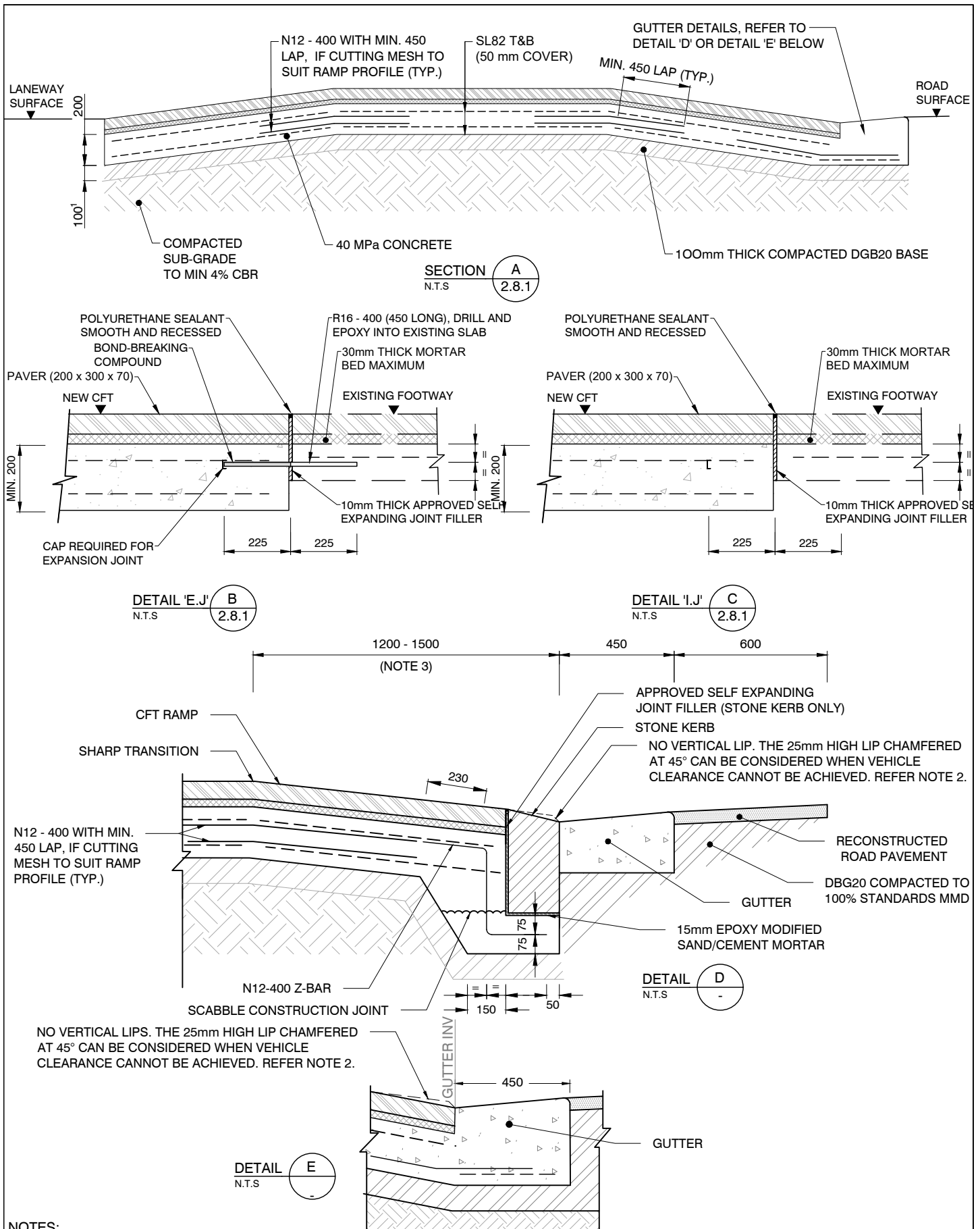
NOTES:

1. TYPE AND COLOUR OF TGSI'S SHALL BE IN ACCORDANCE WITH THIS DRAWINGS UNLESS SPECIFIED FOR THE PROJECT. REFER TO DRAWINGS #2.7.1, #2.7.2 AND #2.7.3 FOR TYPICAL LAYOUT OF TGSI INSTALLATION.
2. DIFFERENT COLOURS AND TYPES OF TGSI'S MAY BE USED FOR SPECIFIC PROJECTS, IN WHICH CASE CITY OF SYDNEY'S APPROVAL MUST BE OBTAINED PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES.
3. WHERE THE BASE SURFACE IS CONCRETE, THE BACKGROUND COLOUR SHALL BE BLACK. EXTENT OF THE BLACK COLOUR SHALL MATCH TGSI'S BLOCK. PLEASE NOTE THAT THE BASE COLOUR SHALL REMAIN AS NATURAL CONCRETE COLOUR.
4. TGSI'S MAY BE INSTALLED ON CONCRETE PAVERS, IF IT COMPLIES WITH LUMINANCE CONTRAST REQUIREMENT SET IN AS 1428.4.1
5. WARNING TGSI'S ONLY TO BE USED ON KERB RAMPS WHERE THE GRADIENT IS SHALLOWER THAN 1:8.5 OR WHERE A NEED IS DEEMED TO EXIST AS PER AS 1428.4.1
6. EXTENT OF THE TGSI'S MAY VARY TO SUIT ANY SPECIFIC DESIGN PENDING CITY'S APPROVAL.
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



NOTES:

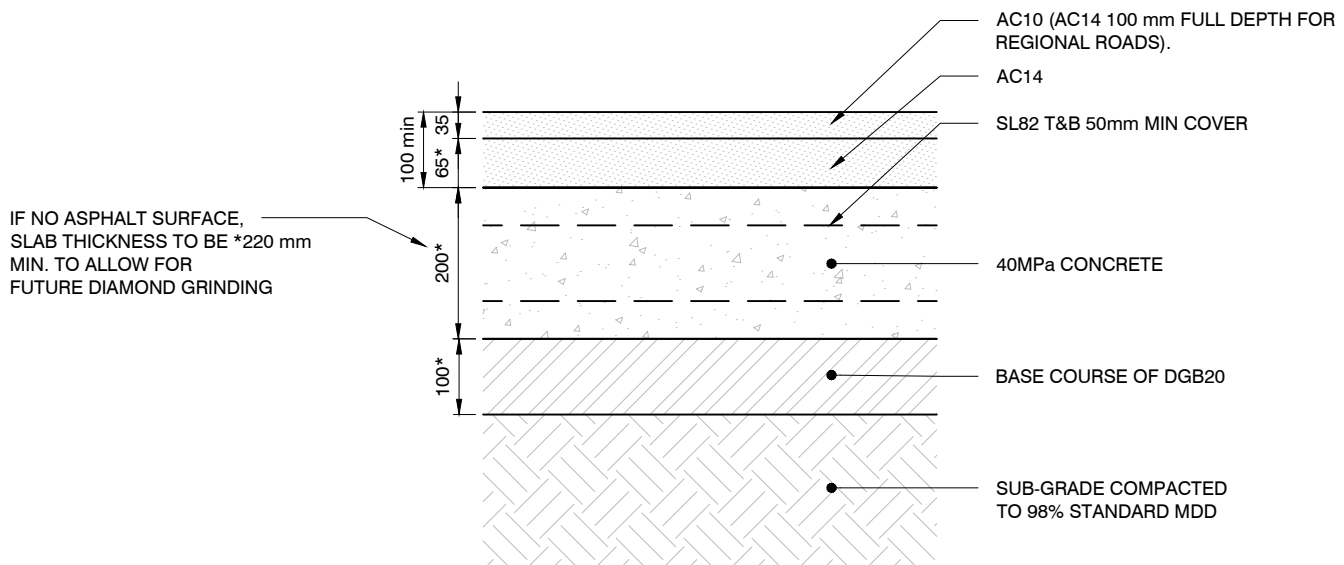
- FOR NEW ROADS, PAVEMENT THICKNESSES SHALL BE DETERMINED BY DESIGN BASED ON SUBGRADE CBR, MATERIAL TYPE AND PROPERTIES, AND ESAS (EQUIVALENT STANDARD AXLES). ALL PAVEMENT DESIGN REQUIREMENTS AND PROCEDURES SET IN SECTION "A3 ROADS, STREET AND STRUCTURES DESIGN" SHALL BE MET IN DESIGN AND JUSTIFIED IN DESIGN REPORT.
- CHAMFERED 45° LIP CAN BE CONSIDERED TO REDUCE LAYBACK LENGTH FOR NARROW FOOTPATH.
- LAYBACK LENGTH CAN BE INCREASED TO ALLOW FOR VEHICLE VERTICAL CLEARANCE, ONCE APPROVED BY CITY'S REPRESENTATIVE.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



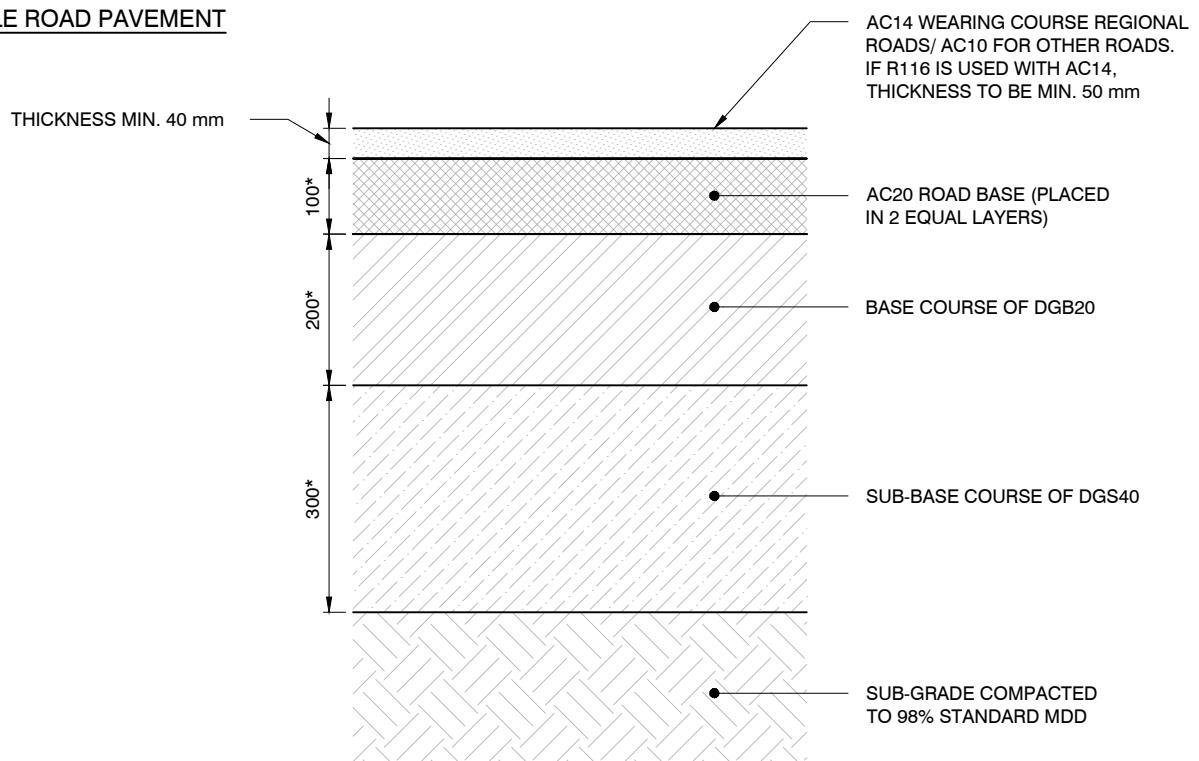
NOTES:

- FOR NEW ROADS, PAVEMENT THICKNESSES SHALL BE DETERMINED BY DESIGN BASED ON SUBGRADE CBR, MATERIAL TYPE AND PROPERTIES, AND ESAS (EQUIVALENT STANDARD AXLES). ALL PAVEMENT DESIGN REQUIREMENTS AND PROCEDURES SET IN SECTION "A3 ROADS, STREET AND STRUCTURES DESIGN" SHALL BE MET IN DESIGN AND JUSTIFIED IN DESIGN REPORT.
- CHAMFERED 45° LIP CAN BE CONSIDERED TO REDUCE LAYBACK LENGTH FOR NARROW FOOTPATH.
- LAYBACK LENGTH CAN BE INCREASED TO ALLOW FOR VEHICLE VERTICAL CLEARANCE, ONCE APPROVED BY CITY'S REPRESENTATIVE.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

RIGID ROAD PAVEMENT (ASPHALT SURFACE OPTIONAL)



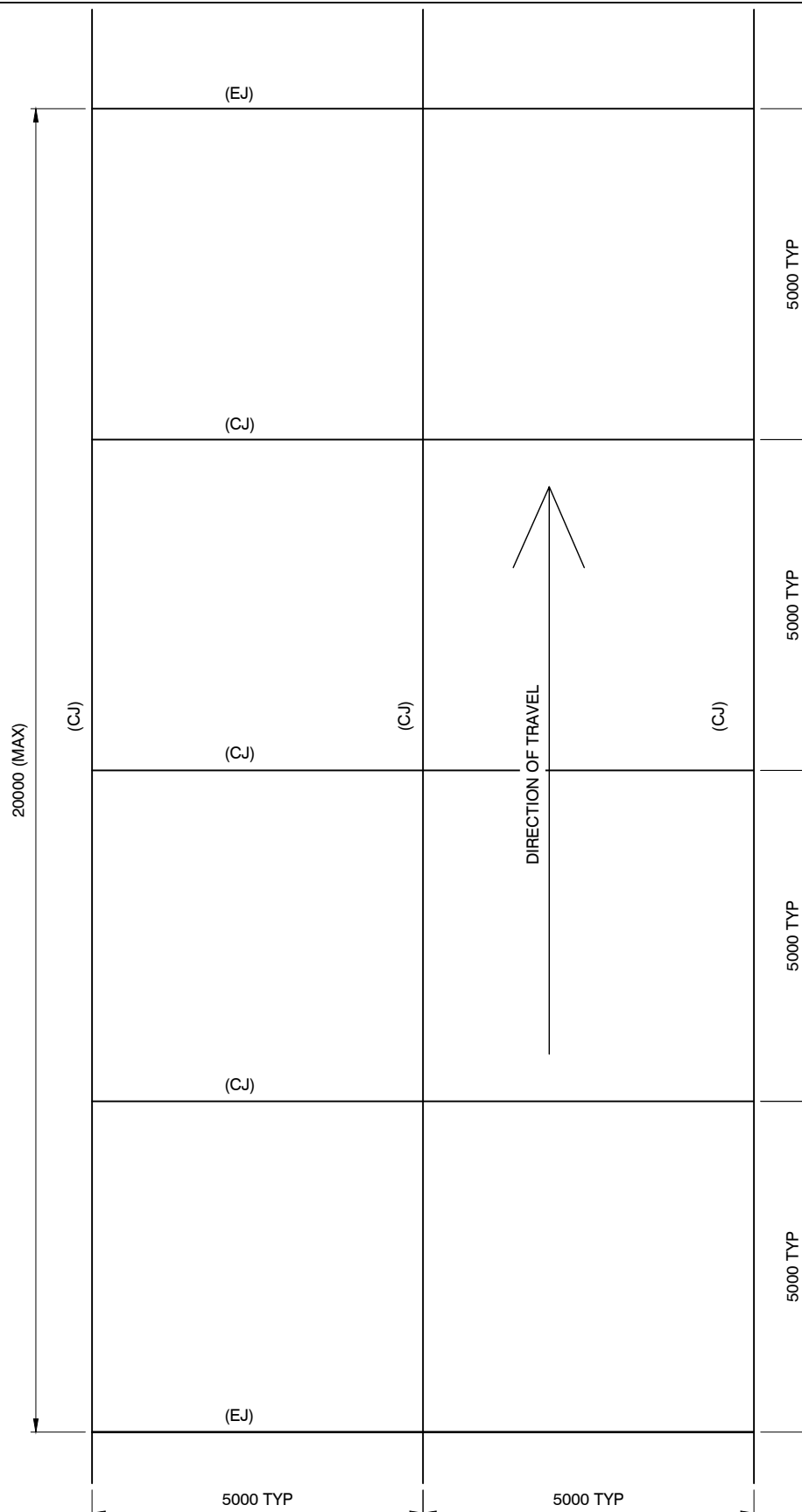
FLEXIBLE ROAD PAVEMENT



SECTION 1:10

NOTES:

1. THESE ARE TYPICAL DRAWINGS ONLY.
- *2. FOR NEW ROADS, PAVEMENT THICKNESSES SHALL BE DETERMINED BY DESIGN BASED ON SUBGRADE CBR, MATERIAL TYPE AND PROPERTIES, AND ESAS (EQUIVALENT STANDARD AXLES). ALL PAVEMENT DESIGN REQUIREMENTS AND PROCEDURES SET IN SECTION "A3 ROADS AND STRUCTURES DESIGN" SHALL BE MET IN DESIGN AND JUSTIFIED IN DESIGN REPORT.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

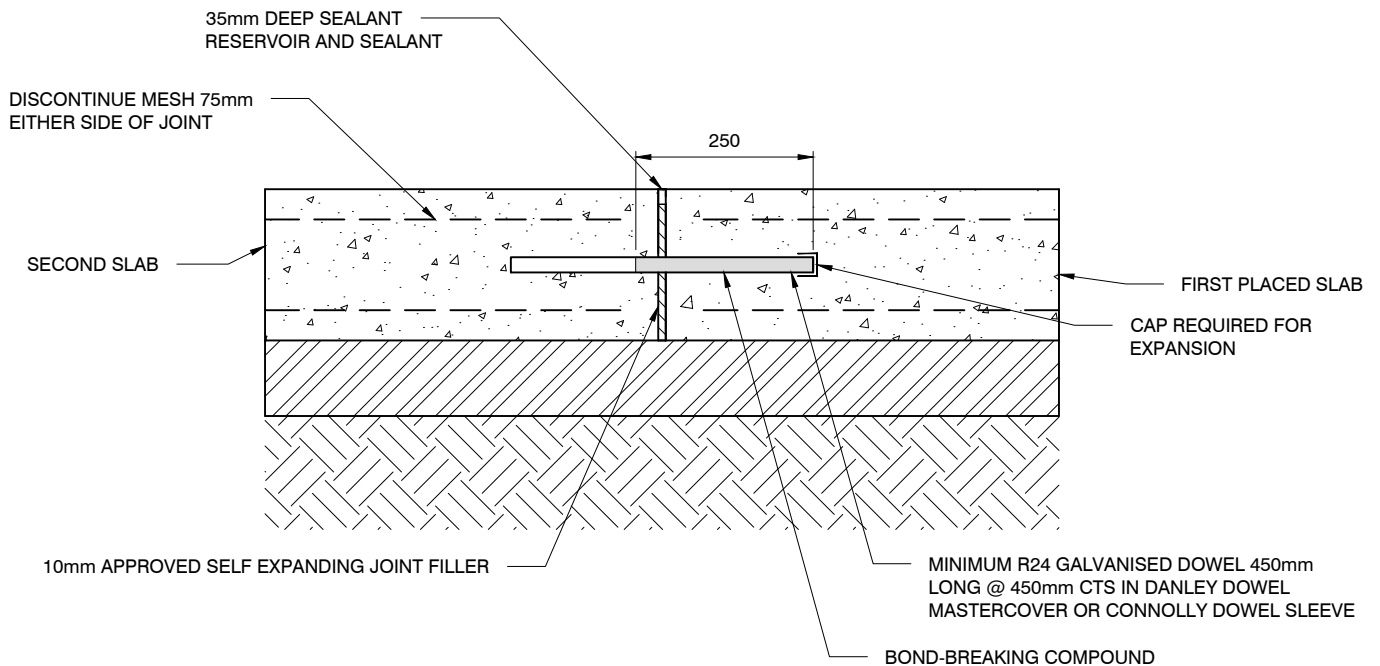


NOTES:

1. TRANSVERSE EXPANSION JOINTS SHALL BE PLACED AT 20m MAXIMUM SPACING ON CONTINUOUS PAVEMENT.
2. TRANSVERSE CONTRACTION JOINTS TO BE PLACED AT 5m MAXIMUM SPACING ON CONTINUOUS PAVEMENT.
3. CONSTRUCTION JOINTS SHALL BE PLACED AT WORK EXTENTS WHEN JOINING ONTO ADJACENT RIGID PAVEMENTS.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

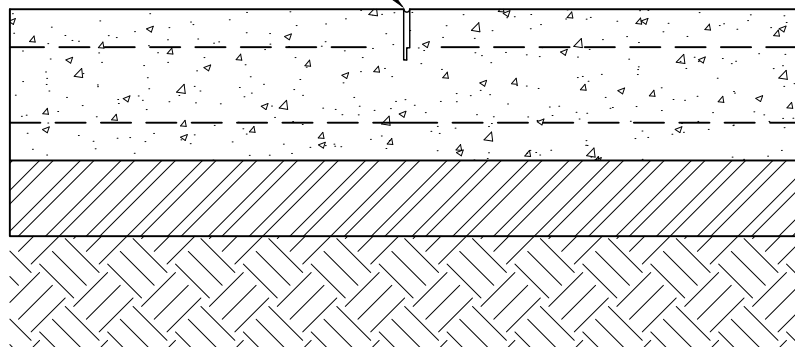
SCALE 1:100

EXPANSION JOINT (EJ)



CONTRACTION JOINT/CONTROL JOINT (CJ)

10mm WIDE SAW CUT. DEPTH TO EQUAL $\frac{1}{3}$ TO $\frac{1}{4}$ THE DEPTH OF THE SLAB. CUT TO BE SEALED WITH FLEXIBLE SEALANT AND BACKING ROD

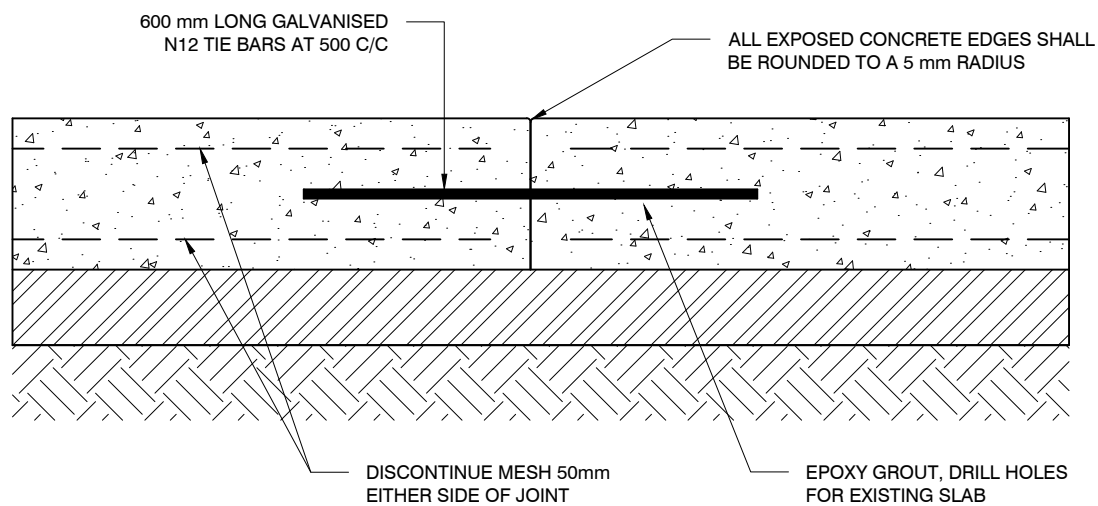


SECTION 1:10

NOTES:

1. TRANSVERSE EXPANSION JOINTS SHALL BE PLACED AT 20m MAXIMUM SPACING.
2. TRANSVERSE CONTRACTION JOINTS TO BE PLACED AT 5m MAXIMUM SPACING.
3. BOND-BREAKING COMPONENT AND END CAP MAY BE REPLACED WITH A PURPOSE-MADE DOWEL SLEEVE.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

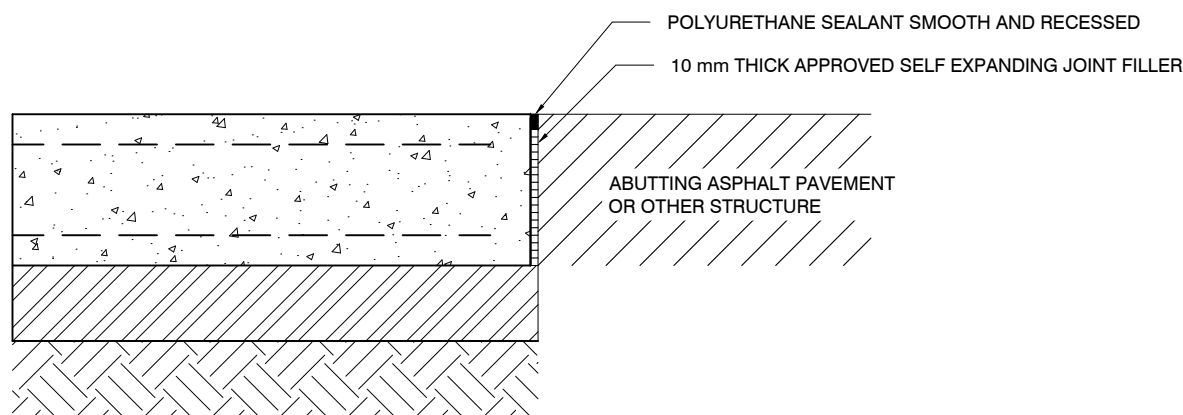
CONSTRUCTION JOINT (DCJ) / JUNCTION WITH EXISTING CONCRETE PAVEMENT



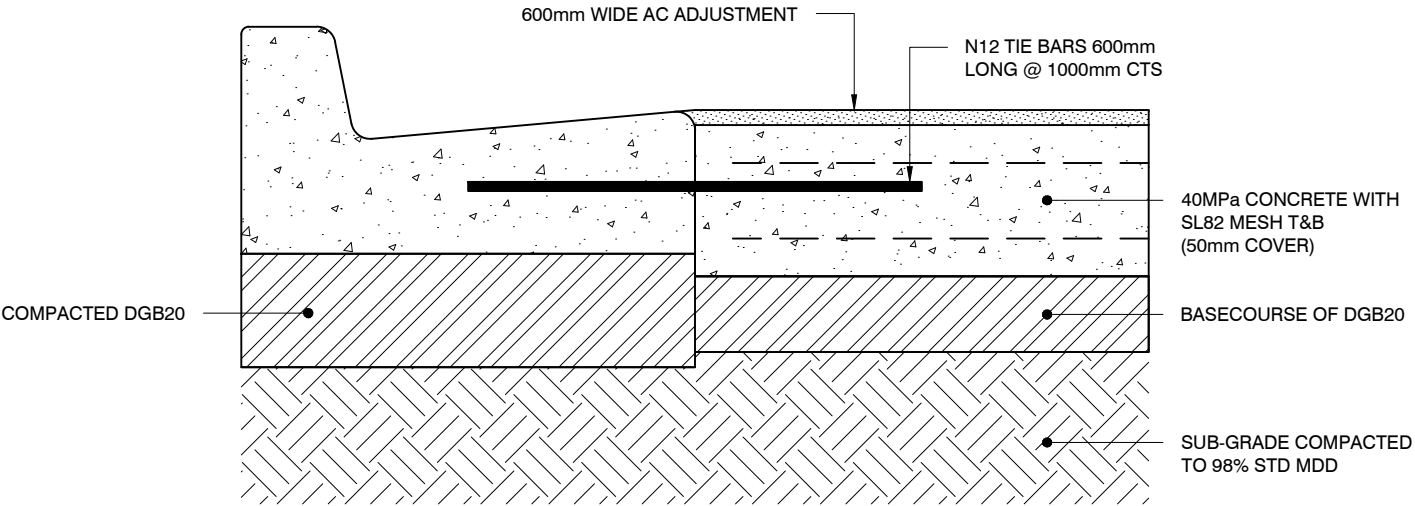
- NOTES:
- 1. ALL TIE BARS TO BE DEFORMED BARS.
 - 2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SECTION 1:10

ISOLATION JOINT

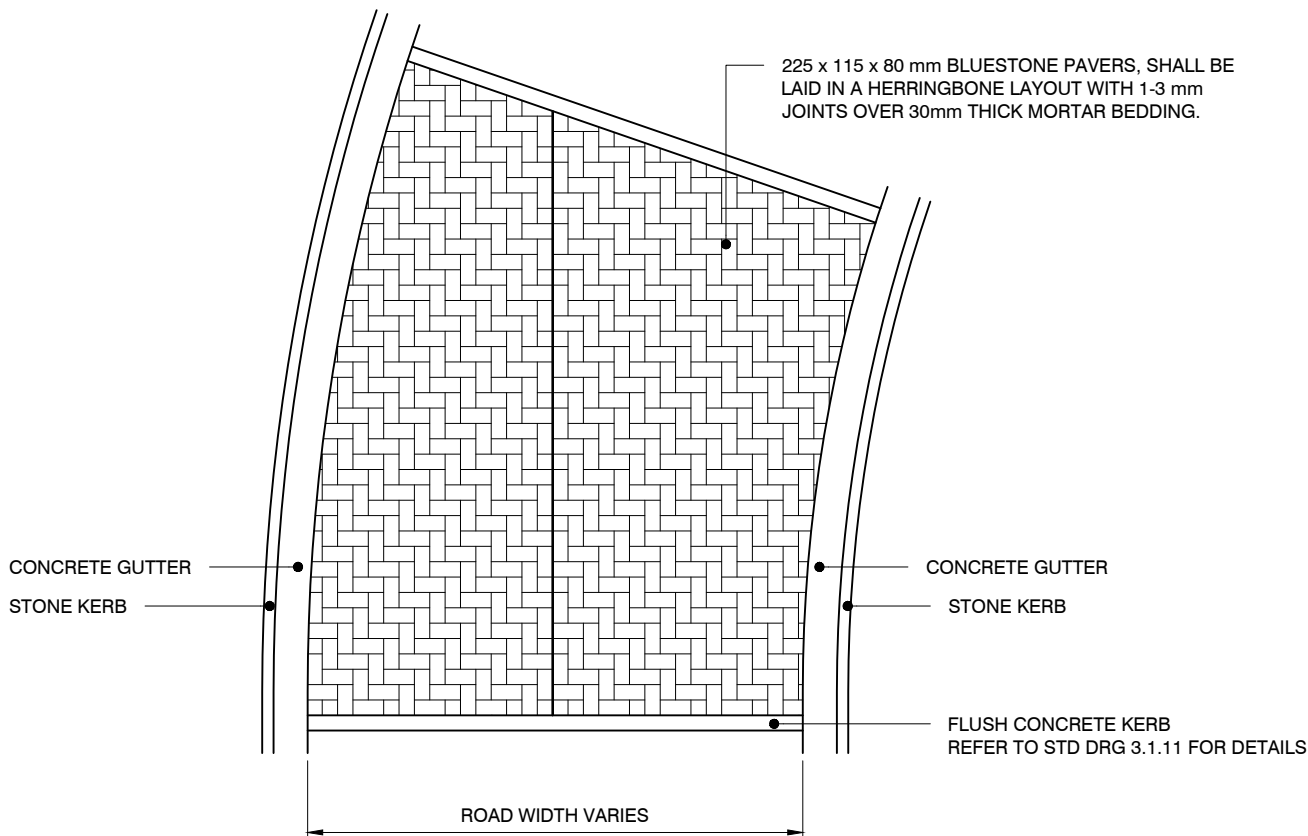


CONCRETE ROAD

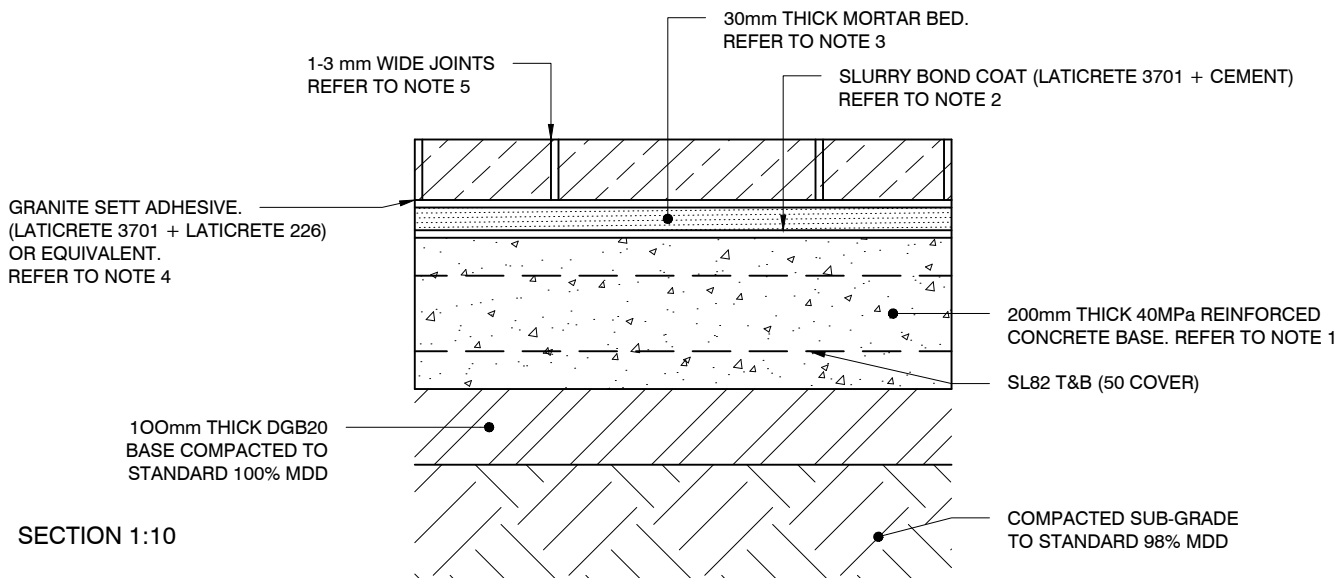


SECTION 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



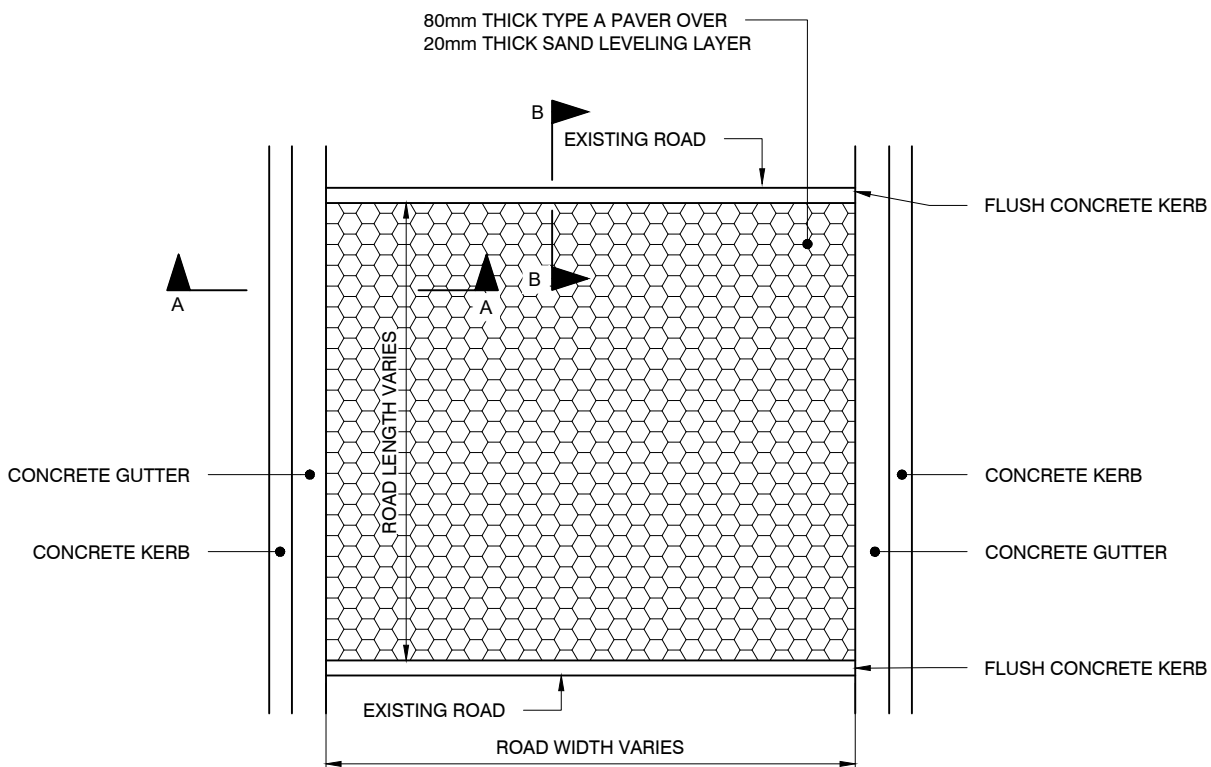
PLAN 1:100



SECTION 1:10

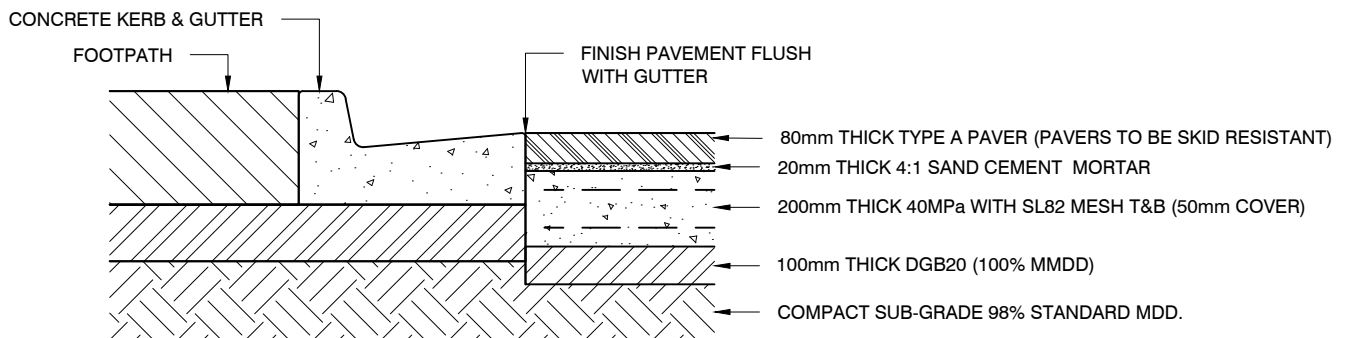
NOTES:

1. SURFACE SHALL BE MADE PLUMB & TRUE WITHIN 3mm AND SHALL HAVE A WOODEN FLOAT FINISH.
2. MORTAR BED SHALL BE LAID WHILE THE SLURRY BOND COAT IS STILL WET & TACKY.
3. 30mm THICK MORTAR BEDDING FINISHED TO A WOOD FLOAT QUALITY. THE BEDDING SHALL BE OF MODIFIED MORTAR (3:1 SAND:CEMENT) MIXED WITH LATICRETE 3701 MORTAR ADMIX & LATICRETE 226 OR EQUIVALENT THICK BED MORTAR AS PER THE MANUFACTURER'S SPECIFICATIONS.
4. THE GRANITE SETTS SHALL BE ADHERED TO THE CURED BEDDING USING A MIX OF LATICRETE 226 PREMIUM MORTAR & LATICRETE 3701 MORTAR ADMIX AS PER THE MANUFACTURER'S SPECIFICATIONS.
5. USE MODIFIED MORTAR (3:1 SAND:CEMENT) MIXED WITH LATICRETE 3701 OR EQUIVALENT MORTAR ADMIX, LATICRETE 226 OR EQUIVALENT THICK BED MORTAR TO MATCH SETTS COLOUR.
6. FOR REINSTATEMENT OF EXISTING STONE SETTS, USE A HIGH EARLY STRENGTH RAPID-SET MORTAR CAPABLE OF ACHIEVING A MINIMUM COMPRESSIVE STRENGTH OF 20 MPA WITHIN ONE HOUR.
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

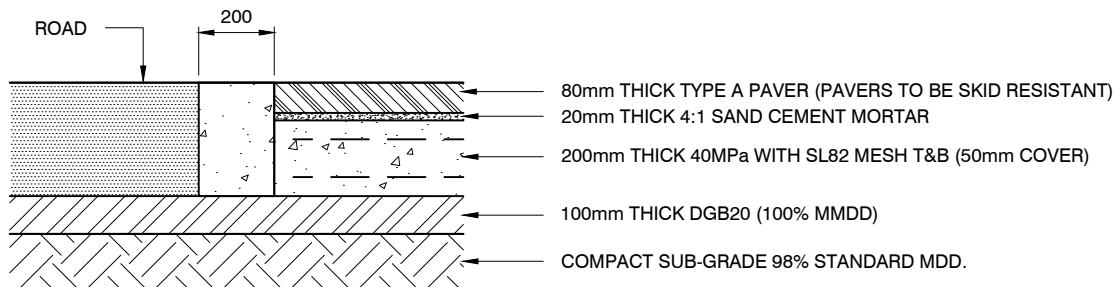


PLAN 1:100

SECTION A-A



SECTION B-B



SECTION 1:20

NOTES:

1. THESE ARE TYPICAL DRAWINGS ONLY.
2. FOR NEW ROADS, PAVEMENT THICKNESSES SHALL BE DETERMINED BY DESIGN BASED ON SUBGRADE CBR, MATERIAL TYPE AND PROPERTIES, AND ESAS (EQUIVALENT STANDARD AXLES). ALL PAVEMENT DESIGN REQUIREMENTS AND PROCEDURES SET IN SECTION "A3 ROADS AND STRUCTURES DESIGN" SHALL BE MET IN DESIGN AND JUSTIFIED IN DESIGN REPORT.
3. FOR KERB AND GUTTER DETAILS REFER TO STANDARD DRAWING # 1.1.1 & 1.1.2
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

NOTE:
THE USE OF THIS TREATMENT IS
SUBJECT TO TNSW APPROVAL

FLUSH CONCRETE KERB

225 x 115 x 80 mm BLUESTONE PAVERS, SHALL BE
LAID IN A HERRINGBONE LAYOUT WITH 1-3 mm
JOINTS OVER 30mm THICK MORTAR BEDDING.

STONE KERB

CONCRETE GUTTER

KERB RAMP

FOOTPATH

1500

FOOTPATH

KERB RAMP

CONCRETE GUTTER

STONE KERB

FLUSH CONCRETE KERB

ROAD WIDTH VARIES

PLAN 1:100

30mm THICK MORTAR BED.
REFER TO NOTE 3

1 - 3 mm WIDE JOINTS
REFER TO NOTE 5

SLURRY BOND COAT (LATICRETE 3701 + CEMENT)
REFER TO NOTE 2

150mm WIDE FLUSH
CONCRETE KERB

ROAD

GRANITE SETT ADHESIVE.
LATICRETE 3701 + LATICRETE 226.
REFER TO NOTE 4

200mm THICK 40MPa REINFORCED
CONCRETE BASE. REFER TO NOTE 1

SL82 T&B (50 COVER)

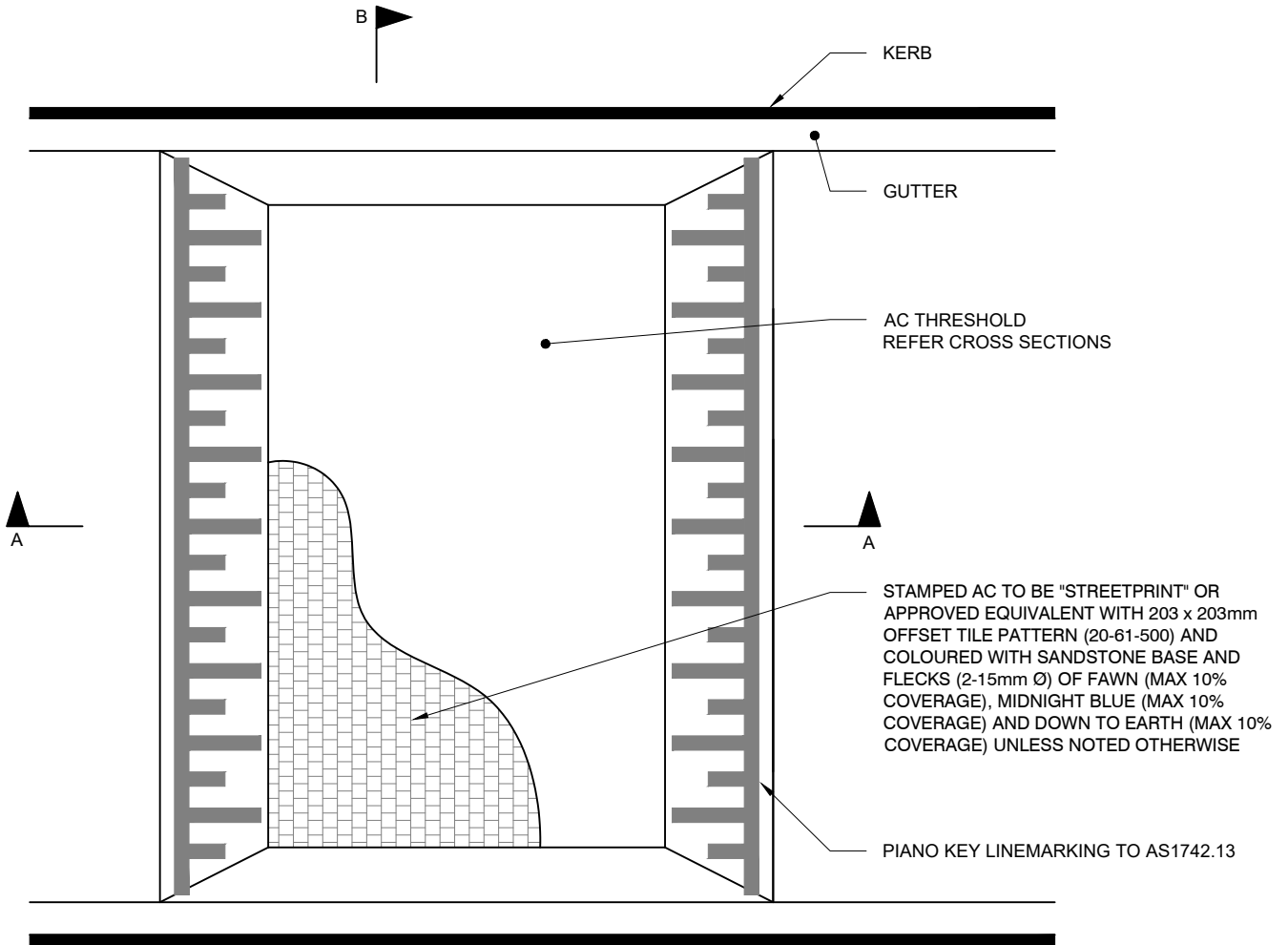
100mm THICK DGB20 (100% MMDD)

COMPACTED SUB-GRADE
TO STANDARD 98% MDD

SECTION 1:10

NOTES:

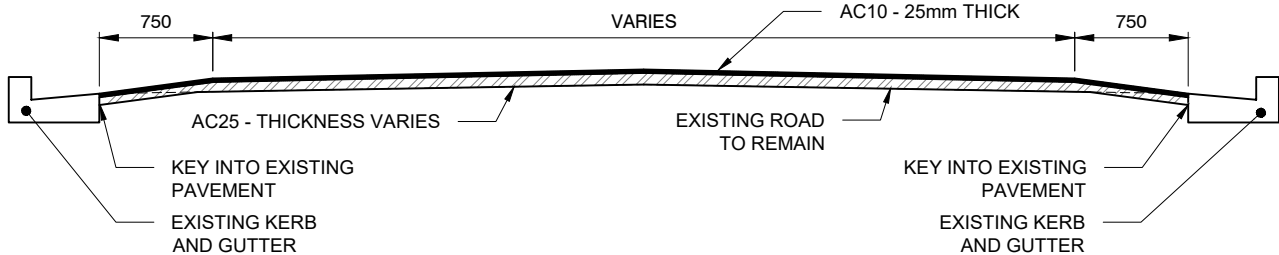
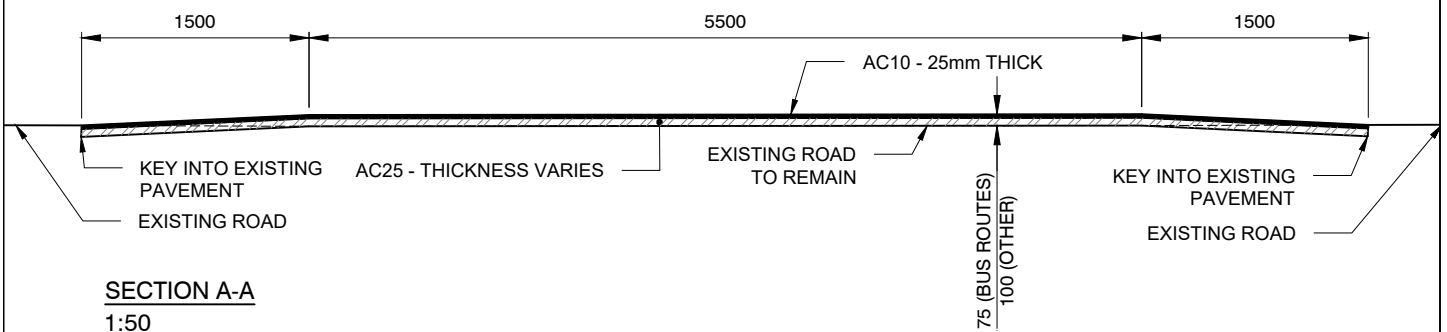
1. SURFACE SHALL BE MADE PLUMB & TRUE WITHIN 3mm AND SHALL HAVE A WOODEN FLOAT FINISH.
2. MORTAR BED SHALL BE LAID WHILE THE SLURRY BOND COAT IS STILL WET & TACKY.
3. 30mm THICK MORTAR BEDDING FINISHED TO A WOOD FLOAT QUALITY. THE BEDDING SHALL BE OF MODIFIED MORTAR (3:1 SAND:CEMENT) MIXED WITH LATICRETE 3701 MORTAR ADMIX & LATICRETE 226 THICK BED MORTAR AS PER THE MANUFACTURER'S SPECIFICATIONS.
4. THE GRANITE SETTS SHALL BE ADHERED TO THE CURED BEDDING USING A MIX OF LATICRETE 290 PREMIUM MORTAR & LATICRETE 3701 MORTAR ADMIX AS PER THE MANUFACTURER'S SPECIFICATIONS.
5. USE MODIFIED MORTAR (3:1 SAND:CEMENT) MIXED WITH LATICRETE 3701 MORTAR ADMIX, LATICRETE 226 THICK BED MORTAR TO MATCH SETTS COLOUR.
6. FOR REINSTATEMENT OF EXISTING STONE SETTS, USE A HIGH EARLY STRENGTH RAPID-SET MORTAR CAPABLE OF ACHIEVING A MINIMUM COMPRESSIVE STRENGTH OF 20 MPA WITHIN ONE HOUR.
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

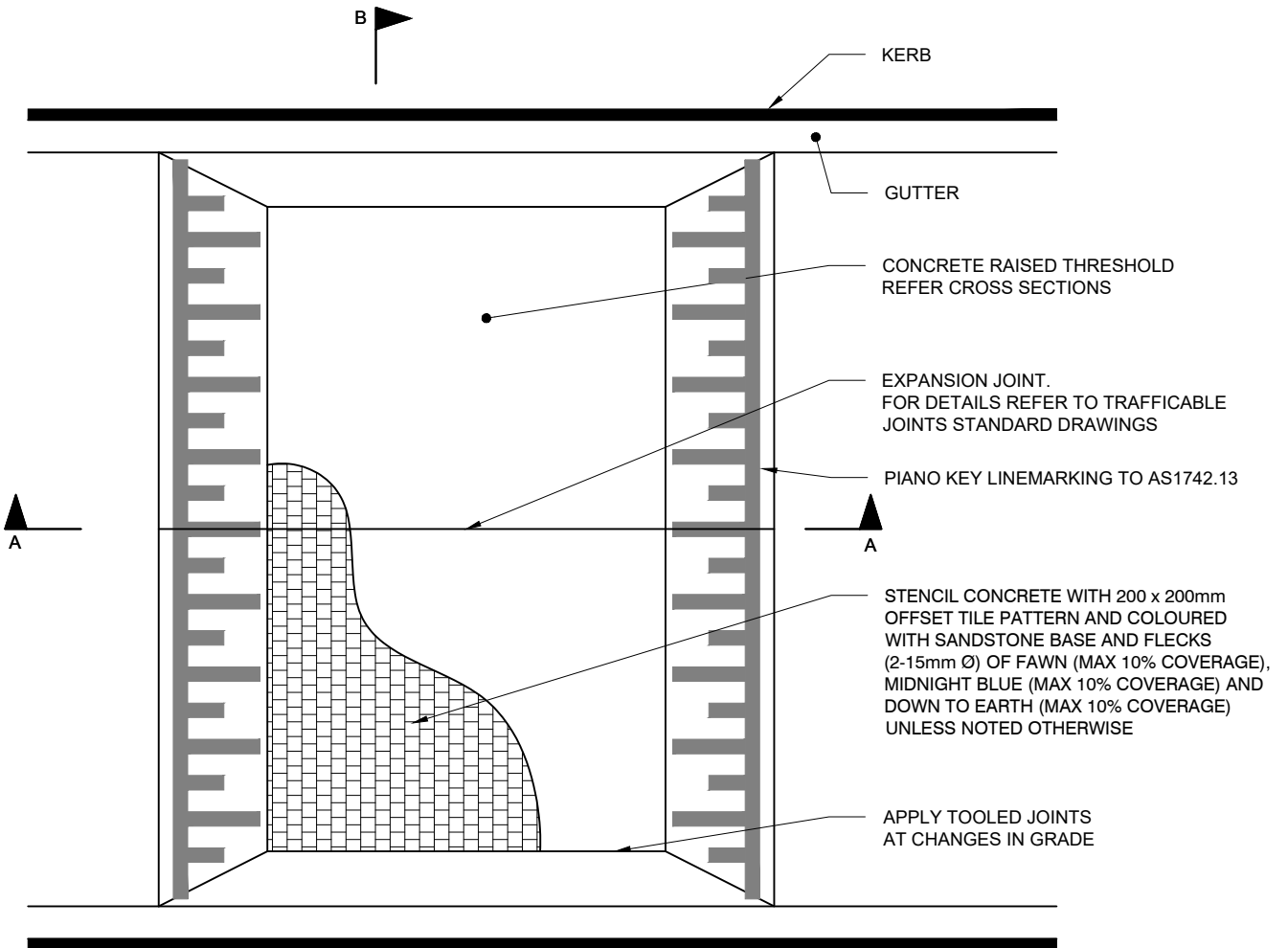


PLAN
1:100

NOTES:

1. PIANO MARKINGS ARE TO BE APPLIED ONTO THE RAMP AND NOT ONTO THE PRECEDING ASPHALT.
2. ON FLAT GRADES A 0.3m DRAINAGE CHANNEL MAY BE REQUIRED.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.





NOTES:

1. PIANO MARKINGS ARE TO BE APPLIED ONTO THE RAMP AND NOT ONTO THE PRECEDING ASPHALT.
2. ON FLAT GRADES A 0.3m DRAINAGE CHANNEL MAY BE REQUIRED.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

CONCRETE ROAD 200mm THICK, 40MPa.
FINISHED SURFACE RAISED ABOVE
EXISTING SURFACE BY 75mm FOR BUS
ROUTES OR 100mm FOR NON-BUS ROUTES

1500

5500

1500

75 (BUS ROUTES)
100 (OTHER)

EXISTING ROAD
SECTION A-A
1:50

COMPACTED SUB-GRADE
TO STANDARD 98% MDD

SL82 T&B - 50 COVER

EXISTING ROAD
100mm THICK DGB20 BASE
COMPACTED TO STANDARD 100% MDD

CONCRETE ROAD 200mm THICK, 40MPa. FINISHED
SURFACE RAISED ABOVE EXISTING SURFACE BY 75mm
FOR BUS ROUTES OR 100mm FOR NON-BUS ROUTES

VARIES

750

750

EXISTING KERB
AND GUTTER

COMPACTED SUB-GRADE
TO STANDARD 98% MDD

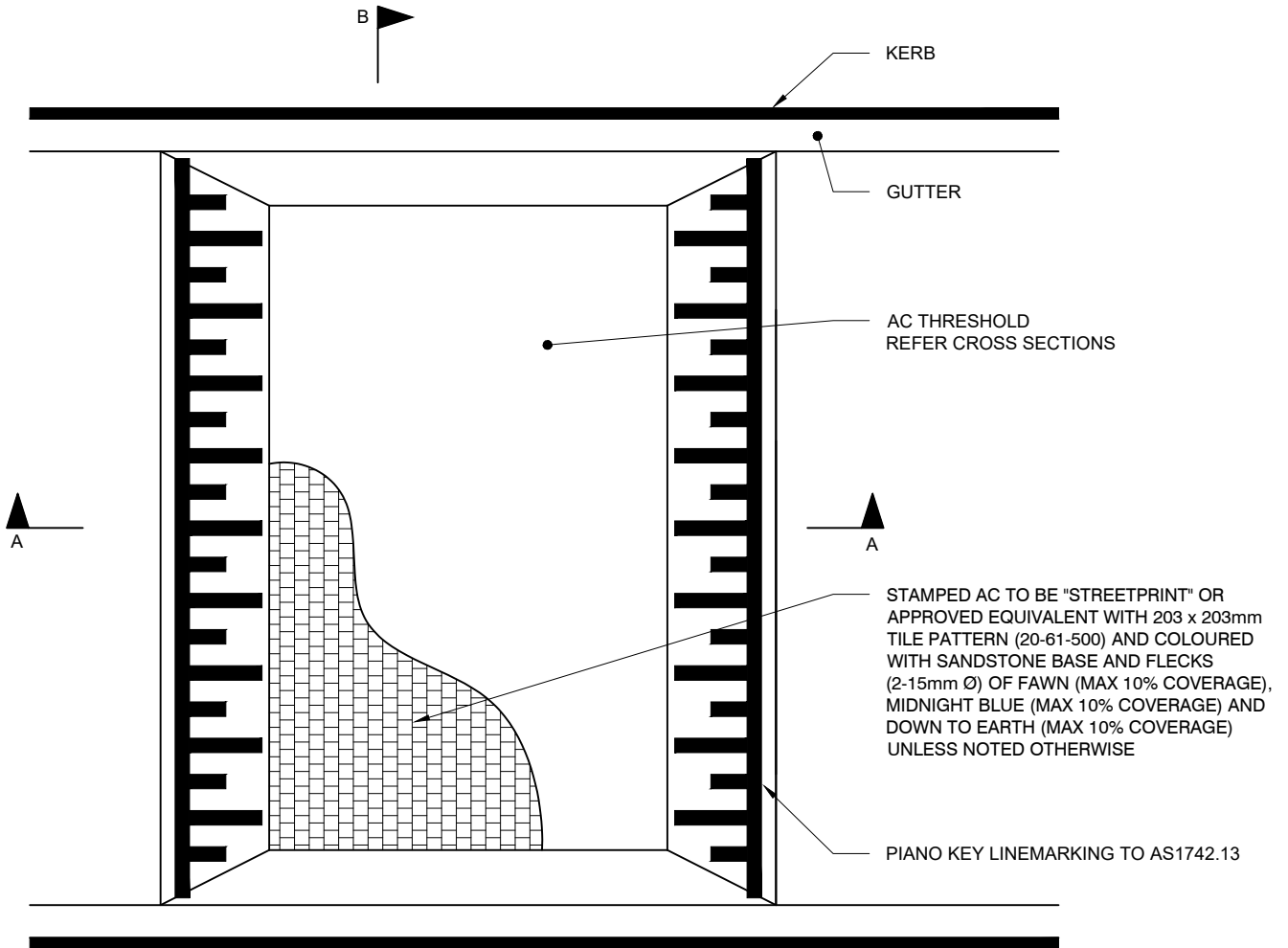
INSTALL N12 LAP BAR @ 400
CTS, IF CUTTING MESH TO SUIT
THRESHOLD PROFILE TYP.

SL82 T&B - 50 COVER

EXISTING KERB
AND GUTTER

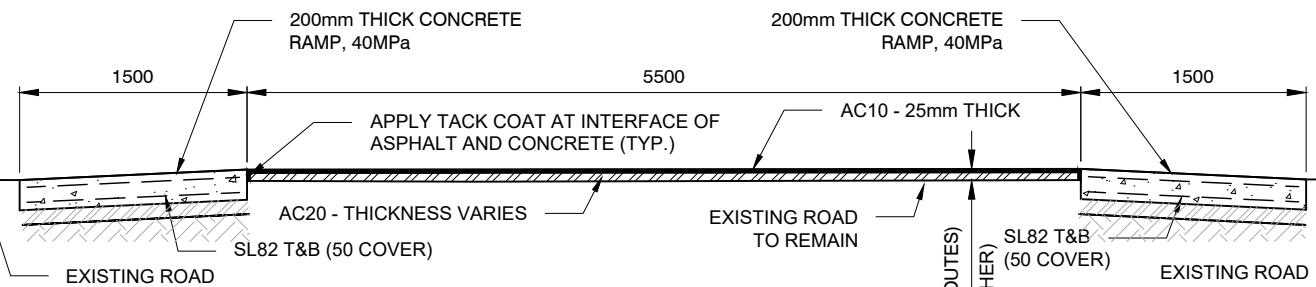
100mm THICK DGB20
BASE COMPACTED TO
STANDARD 100% MDD

SECTION B-B
1:50



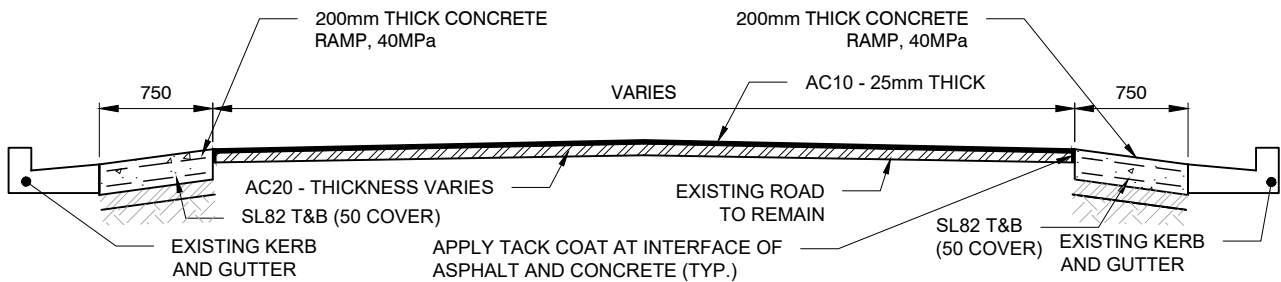
NOTES:

1. PIANO MARKINGS ARE TO BE APPLIED ONTO THE RAMP AND NOT ONTO THE PRECEDING ASPHALT.
2. ON FLAT GRADES A 0.3m DRAINAGE CHANNEL MAY BE REQUIRED.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



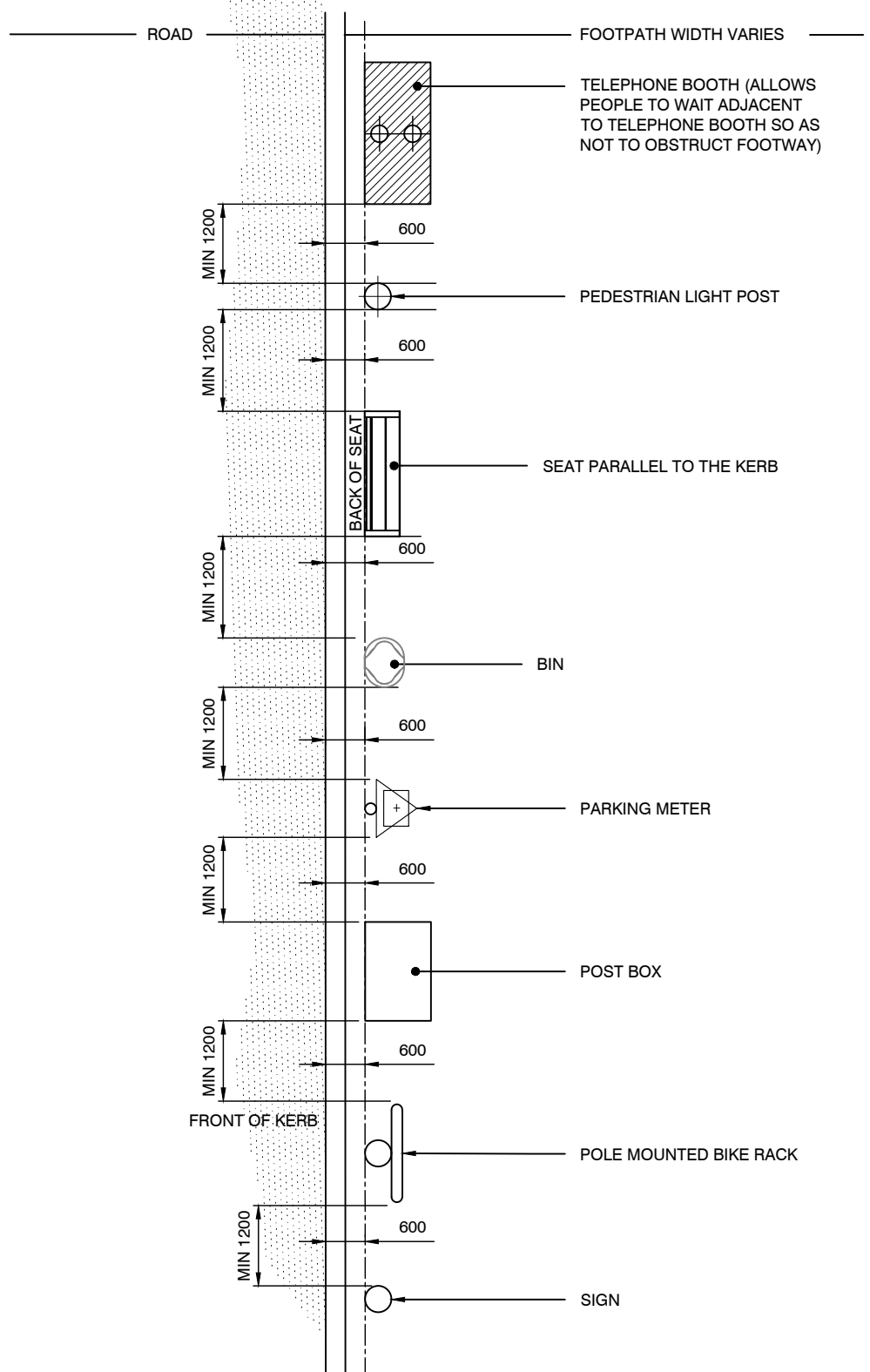
SECTION A-A

1:50



SECTION B-B

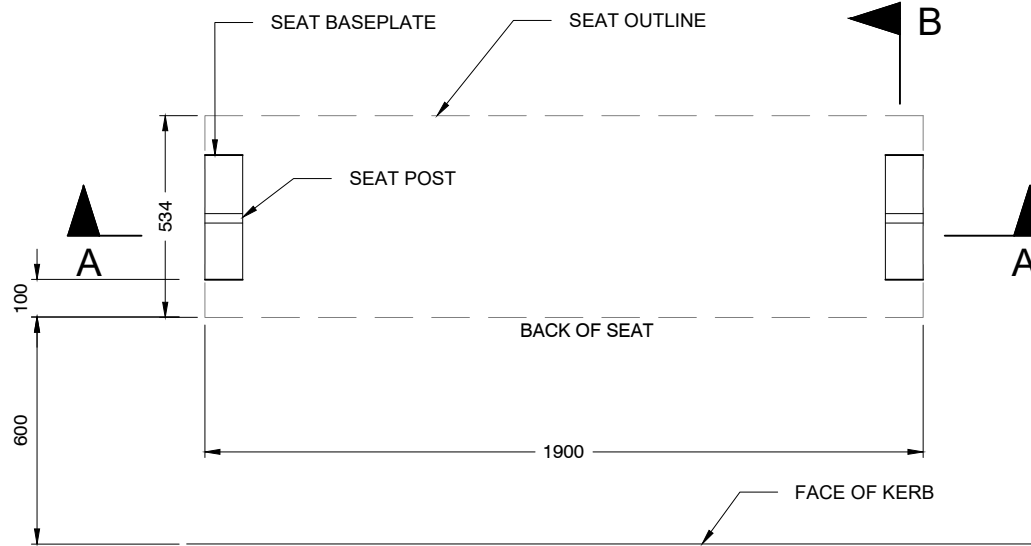
1:50



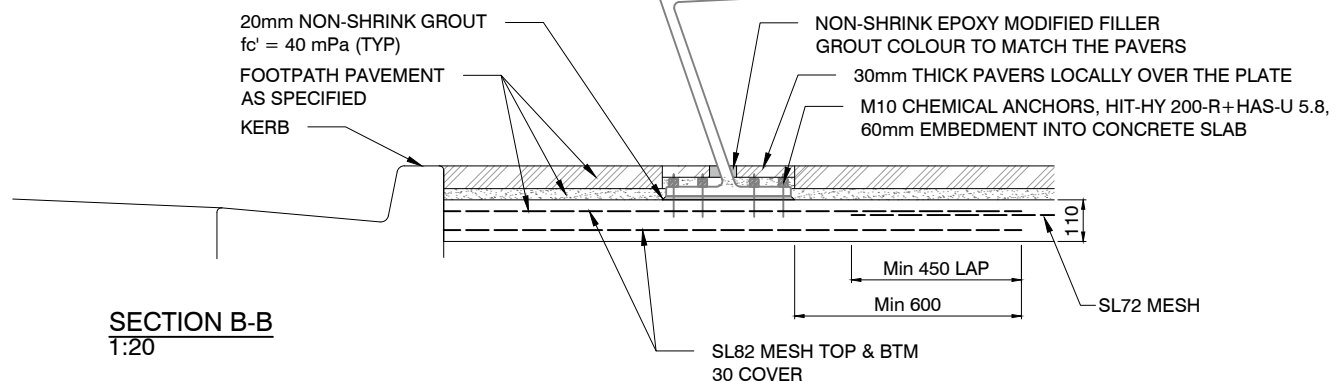
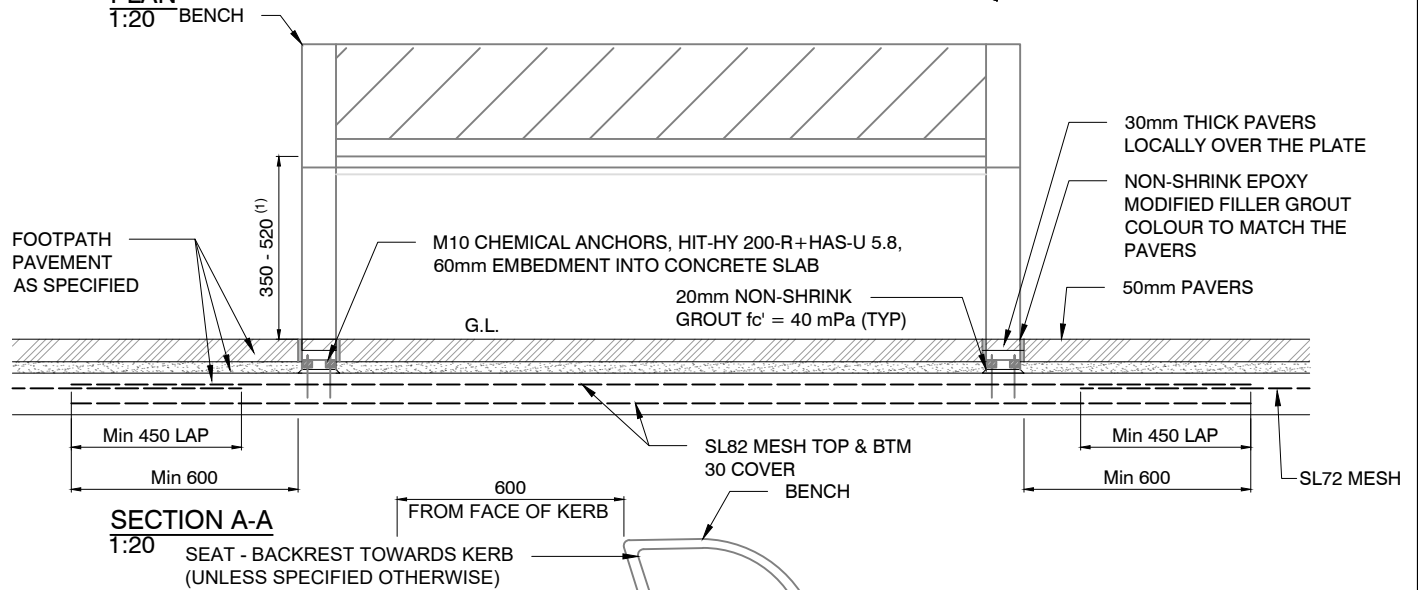
PLAN 1:100

NOTES:

1. WHEREVER POSSIBLE FIXTURES SHOULD BE LOCATED TO ALIGN FIXING POINTS WITH PAVING JOINTS TO MINIMISE CUTTING THROUGH PAVING UNITS. SET FIXTURES FURTHER APART (CLEARANCE MIN. 300 mm) AS REQUIRED TO ALIGN WITH PAVING JOINTS.
2. SUFFICIENT SPACE SHOULD BE ALLOWED BETWEEN FIXTURES ACCOMMODATE BELOW GROUND FOOTING AND FIXINGS.
3. ALLOW FOR MORE ADEQUATE CLEARANCE IN RELATION TO FIXTURE USAGE AS REQUIRED.
4. EG. AT TELEPHONE BOOTHS AND FOR BIKE RACK/RING.
5. FIXTURES SHOULD BE LIMITED IN CLOSE PROXIMITY TO PEDESTRIAN CROSSING POINTS TO MINIMISE OBSTRUCTION.
6. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

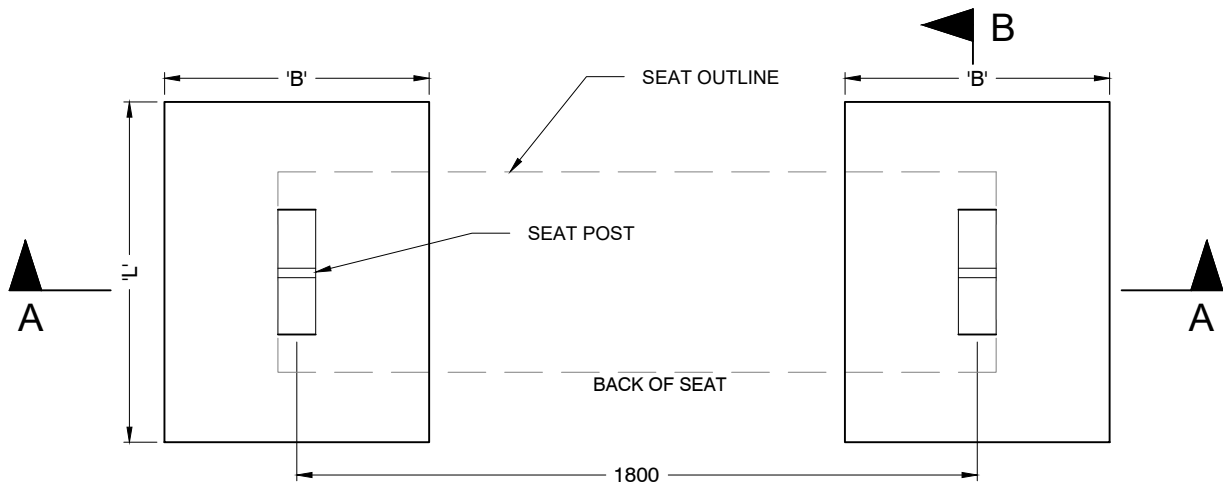


PLAN
1:20

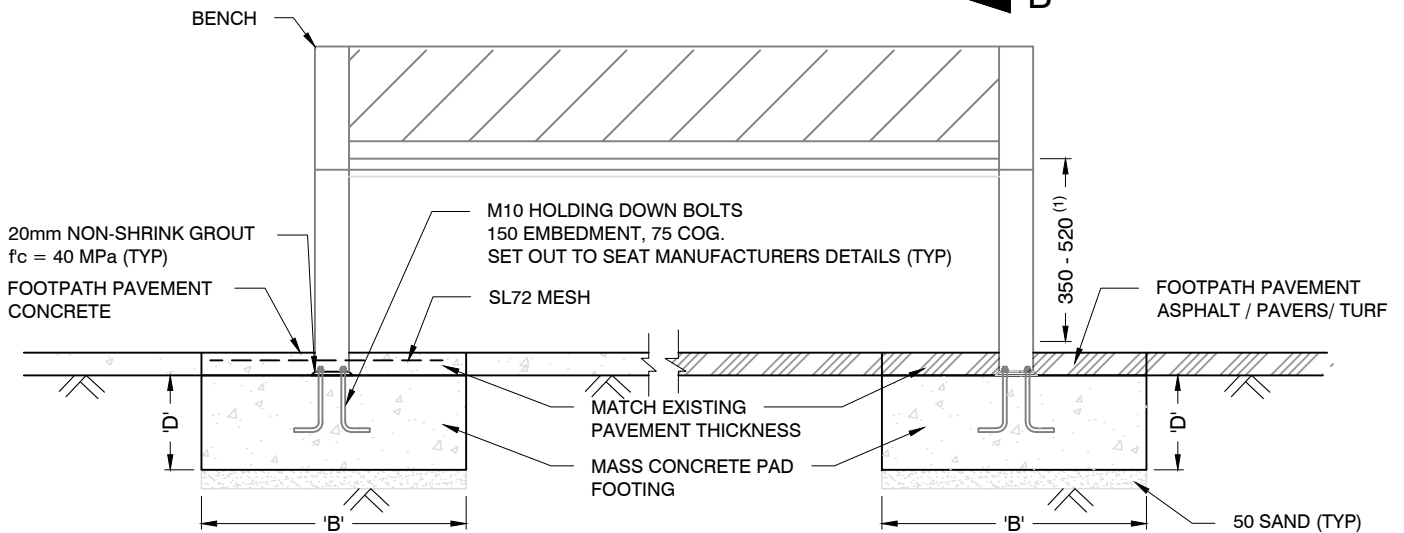


NOTES:

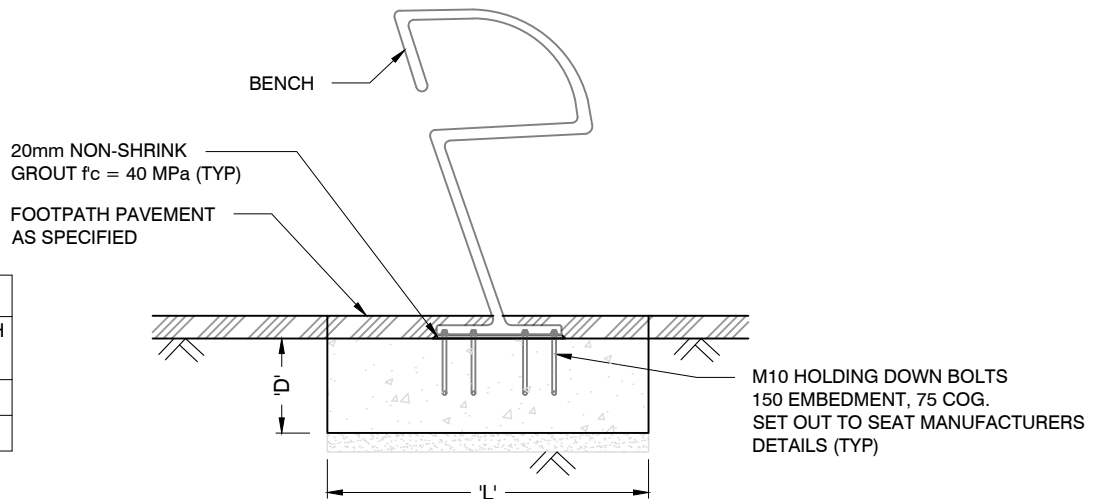
- SEATS SHALL ONLY BE INSTALLED WHERE A HEIGHT OF 400-450 mm, MEASURED FROM THE CENTER OF THE SEAT, CAN BE ACHIEVED, AND WHERE THE HEIGHT OF THE SEAT EDGES FALLS WITHIN THE RANGE OF 350-520 mm.
- CONCRETE GRADE $f_c' = 32 \text{ MPa}$.
- ALL HOLDING DOWN BOLTS, BASE PLATE AND BIN DETAILS, REFER TO MANUFACTURER'S SPECIFICATIONS.
- THE STRUCTURAL ELEMENTS SHOWN ON THESE DRAWINGS HAVE BEEN DESIGNED FOR THE FOLLOWING LIVE LOADS:
 - 1.0 kN/m APPLIED Laterally TO THE TOP EDGE OF THE SEAT.
 - 1.5 kN/m APPLIED Vertically TO THE SEAT BENCH.
- THE FOUNDATION HAVE BEEN DESIGNED FOR AN MINIMUM ALLOWABLE BEARING PRESSURE OF 150 kPa.
- IF ANCHORING TO AN EXISTING CONCRETE SLAB, REFER TO STD DRG # 4.2.2
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



PLAN
1:20



SECTION A-A
1:20

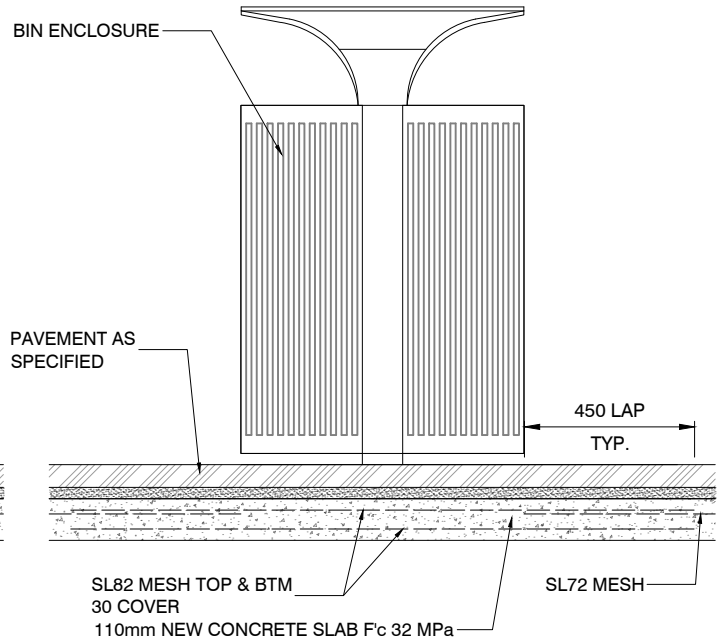
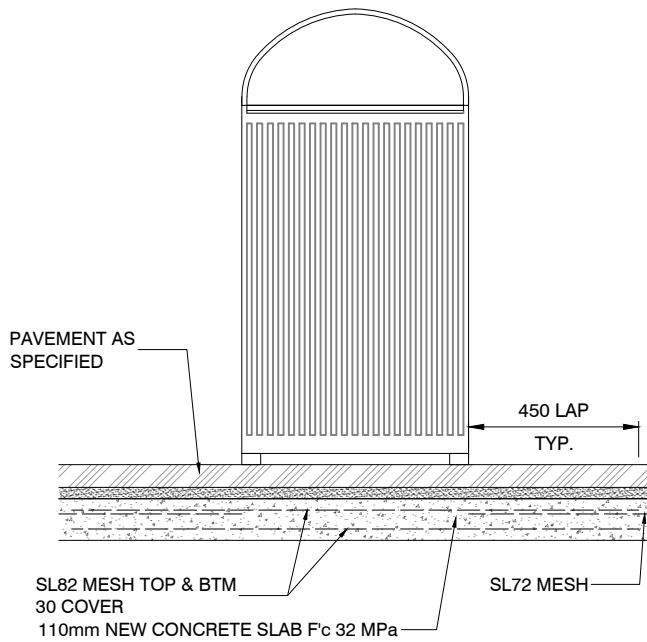


SECTION B-B
1:20

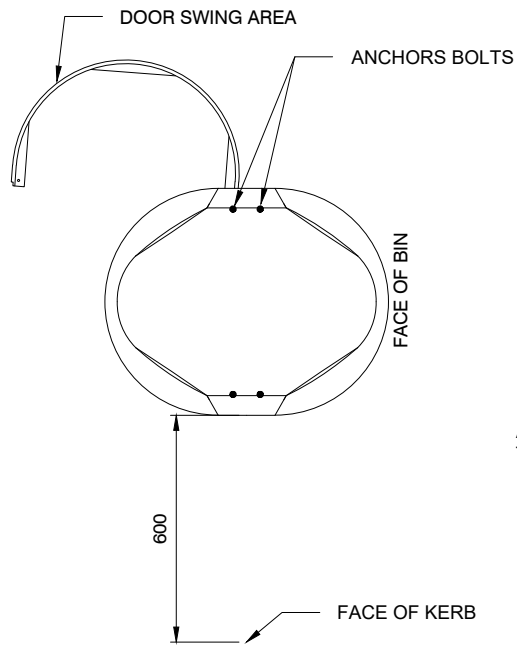
FOOTING SCHEDULE		
'B' WIDTH (mm)	'L' LENGTH (mm)	'D' DEPTH (mm)
700	900	250
700	800	300

NOTES:

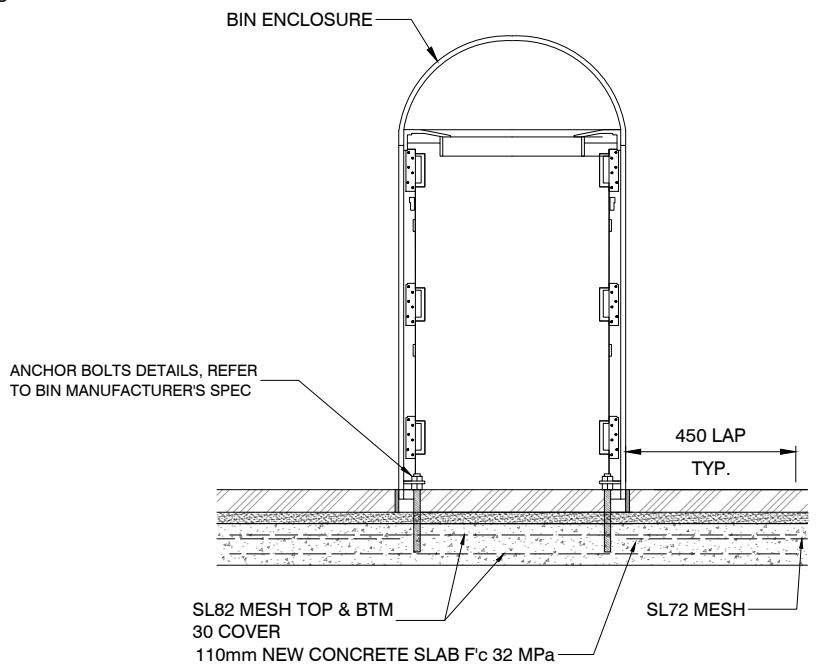
- SEAT SHALL ONLY BE INSTALLED WHERE A HEIGHT OF 400-450 mm, MEASURED FROM THE CENTER OF THE SEAT, CAN BE ACHIEVED, AND WHERE THE HEIGHT OF THE SEAT EDGES FALLS WITHIN THE RANGE OF 350-520 mm.
- CONCRETE GRADE $f_c = 32$ MPa.
- ALL CONCRETE COVER TO BE 50 mm.
- ALL HOLDING DOWN BOLTS, BASE PLATE AND BIN DETAILS, REFER TO MANUFACTURER'S SPECIFICATIONS.
- THE STRUCTURAL ELEMENTS SHOWN ON THESE DRAWINGS HAVE BEEN DESIGNED FOR THE FOLLOWING LIVE LOADS:
 - 1.0 kN/m APPLIED LATERALLY TO THE TOP EDGE OF THE SEAT.
 - 1.5 kN/m APPLIED VERTICALLY TO THE SEAT BENCH.
- THE FOUNDATION HAVE BEEN DESIGNED FOR AN MINIMUM ALLOWABLE BEARING PRESSURE OF 150 kPa.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



VIEWS 1:20



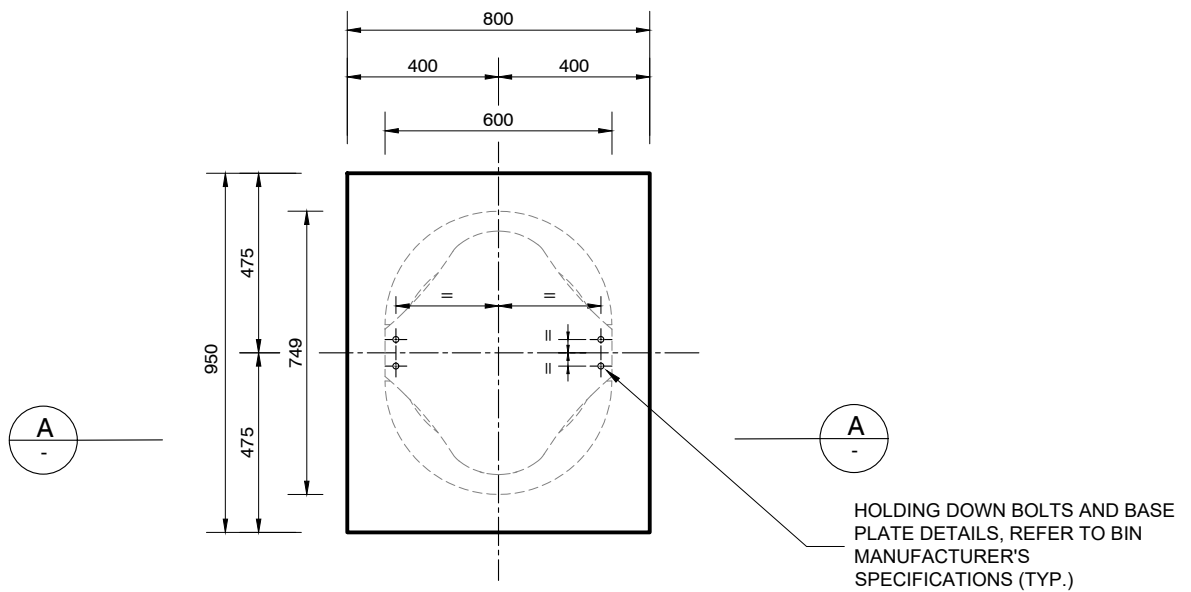
PLAN 1:20



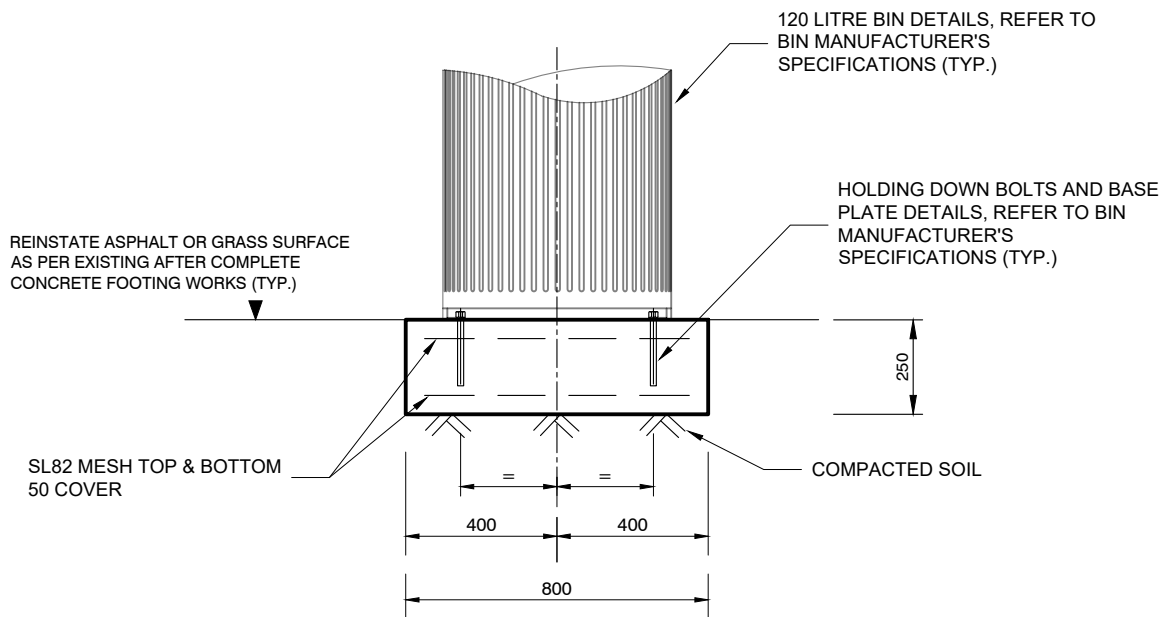
SECTION 1:20

NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. ALL HOLDING DOWN BOLTS, BASE PLATE AND BIN DETAILS, REFER TO MANUFACTURER'S SPECIFICATIONS.
3. IF ANCHORING TO AN EXISTING CONCRETE SLAB, ENSURE SLAB IS MINIMUM 110 mm THICK. IF SLAB THICKNESS IS LESS THAN 110 mm, REFER TO STD DRG # 4.3.3
4. REFER TO CITY OF SYDNEY TECHNICAL SPECIFICATIONS FOR ALL WORKS.



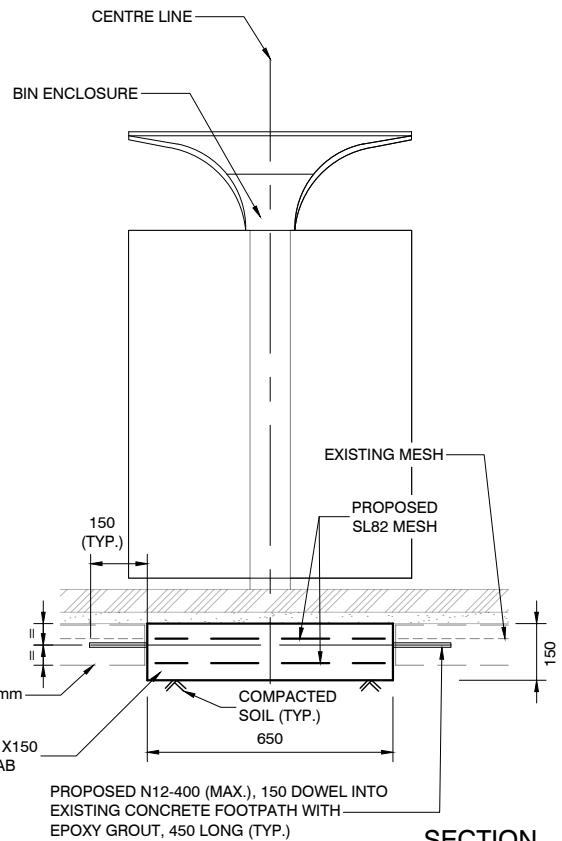
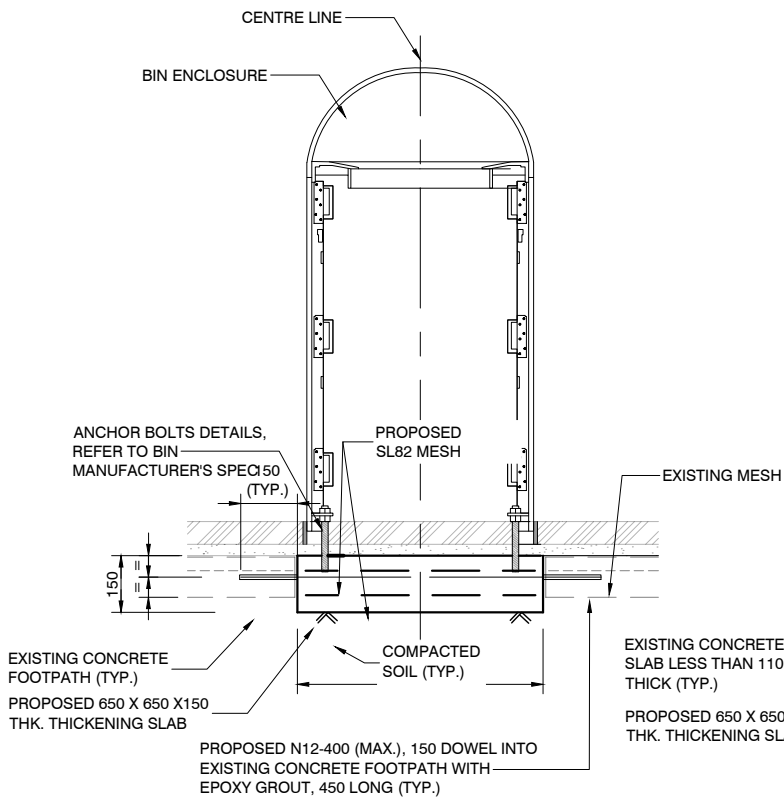
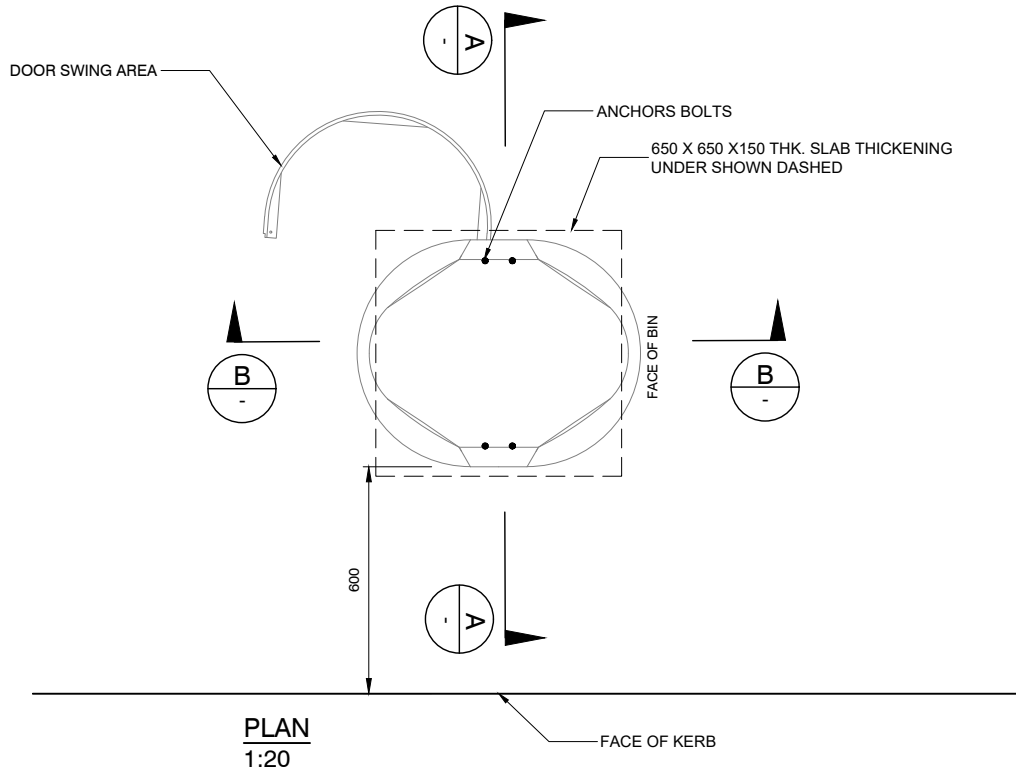
120L BIN FOOTING PLAN
1:20



SECTION A
1:20

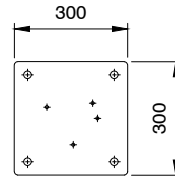
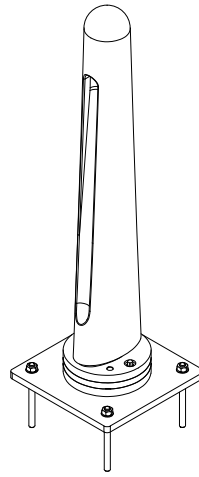
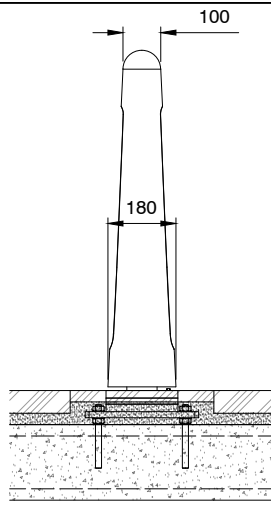
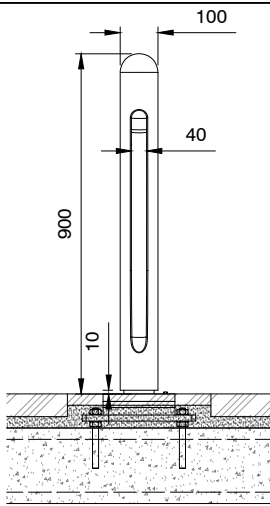
NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. CONCRETE GRADE $f_c = 32$ MPa.
3. ALL HOLDING DOWN BOLTS, BASE PLATE AND BIN DETAILS, REFER TO MANUFACTURER'S SPECIFICATIONS.
4. REFER TO CITY OF SYDNEY TECHNICAL SPECIFICATIONS FOR ALL WORKS.

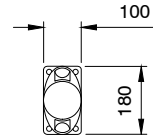
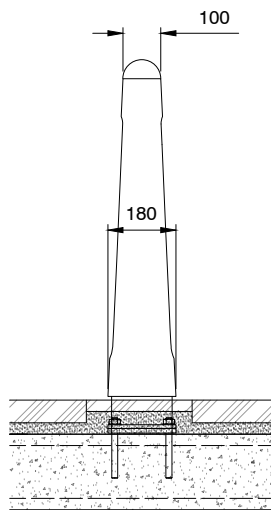
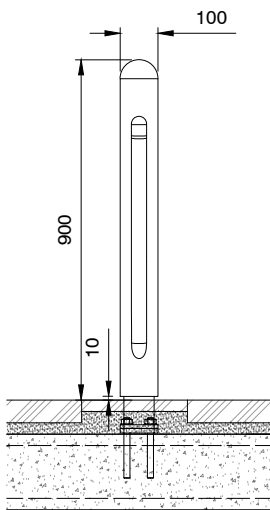


NOTES:

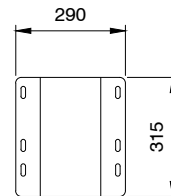
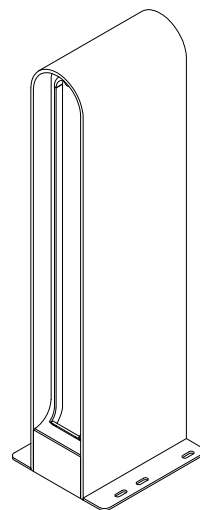
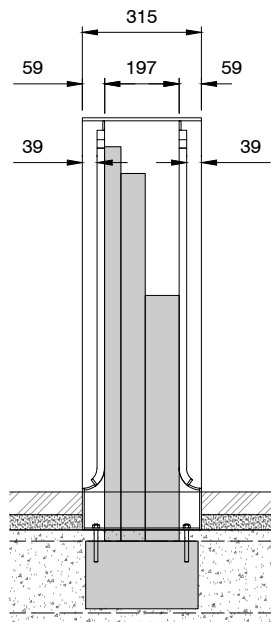
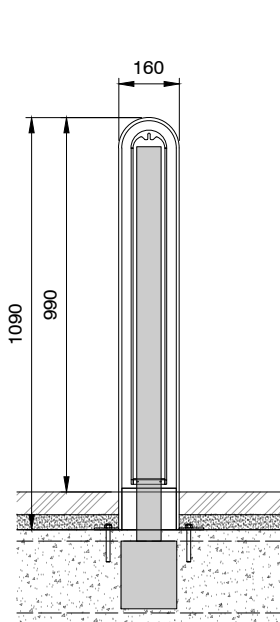
1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. CONCRETE GRADE $f_c = 32$ MPa.
3. ALL CONCRETE COVER TO BE 40mm TOP & 50mm BOTTOM
4. ALL HOLDING DOWN BOLTS, BASE PLATE AND BIN DETAILS, REFER TO MANUFACTURER'S SPECIFICATIONS.
5. REFER TO CITY OF SYDNEY TECHNICAL SPECIFICATIONS FOR ALL WORKS.



REMOVABLE BOLLARD - LOW IMPACT
1:20



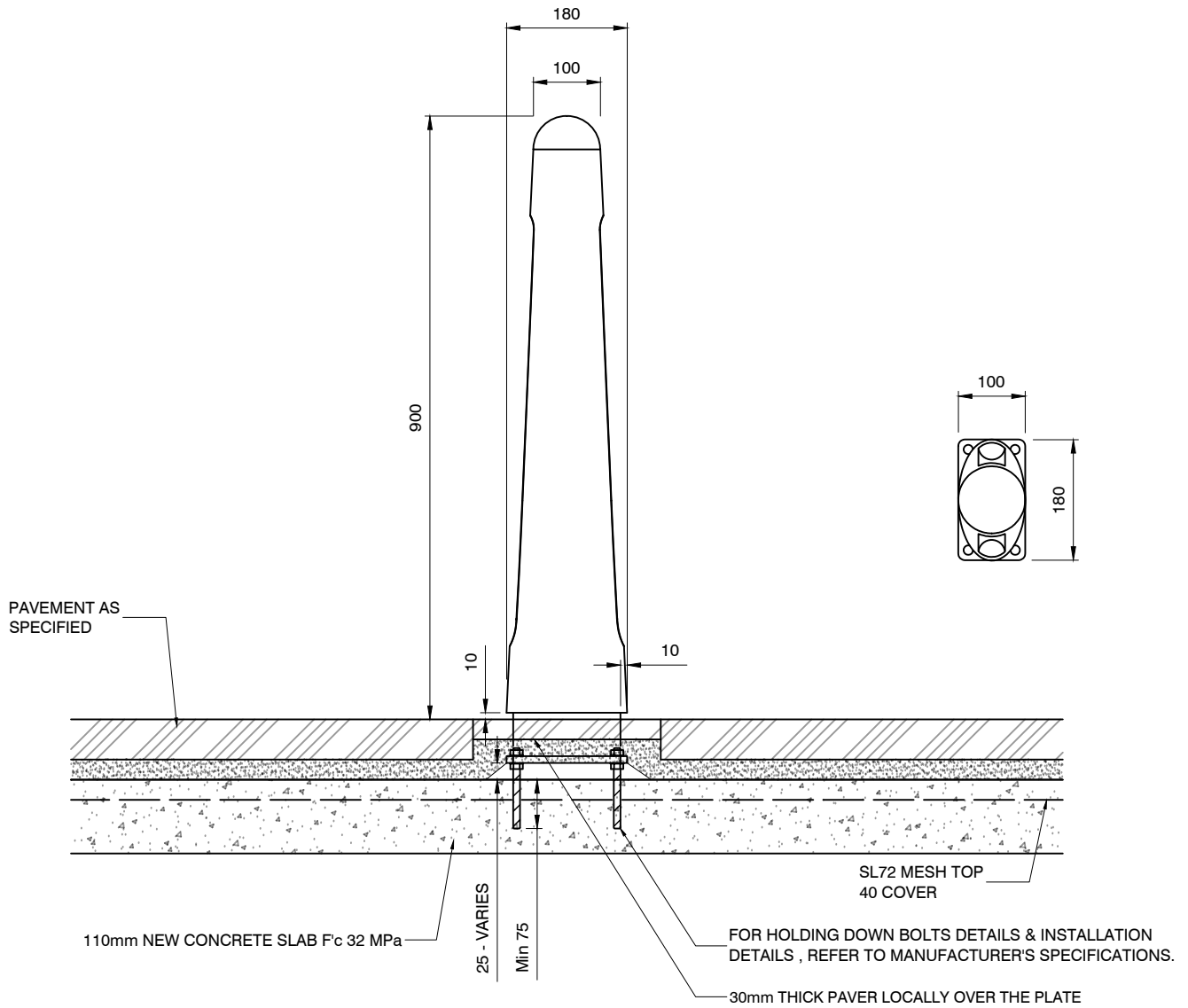
SOLID BOLLARD - MEDIUM IMPACT
1:20



ANTI-VEHICULAR BOLLARD
1:20

NOTES:

1. ALL IMPACT BOLLARD INSTALLATIONS, INCLUDING FOOTING DESIGN, SHALL BE CARRIED OUT BY THE SUPPLIER OR A NOMINATED CONTRACTOR APPROVED BY THE SUPPLIER. THE SUPPLIER SHALL INSPECT AND CERTIFY THE INSTALLATION.
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

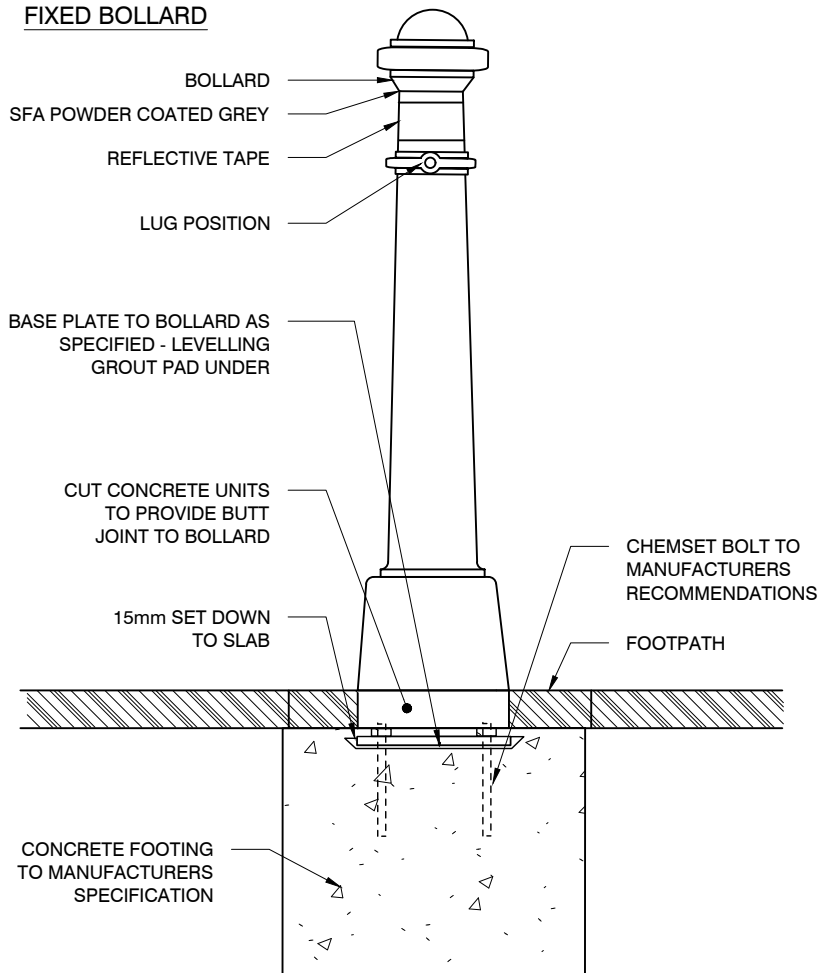


SOLID BOLLARD SECTION
1:10

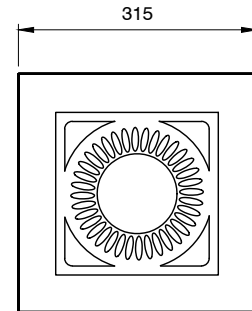
NOTES:

1. IF INSTALLING BOLLARD ON EXISTING CONCRETE SLAB, ENSURE SLAB IS MINIMUM 110 mm THICK.
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

FIXED BOLLARD

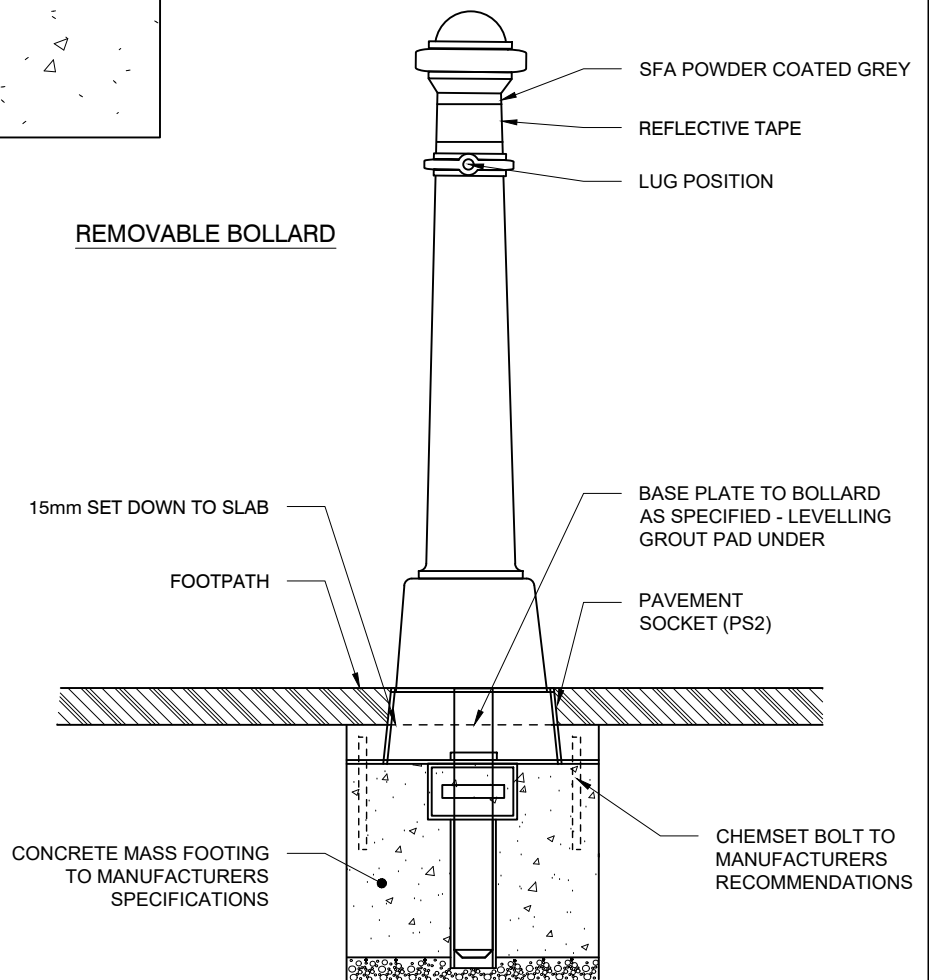
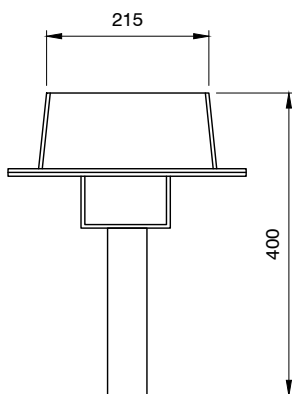


PAVEMENT SOCKET PLAN



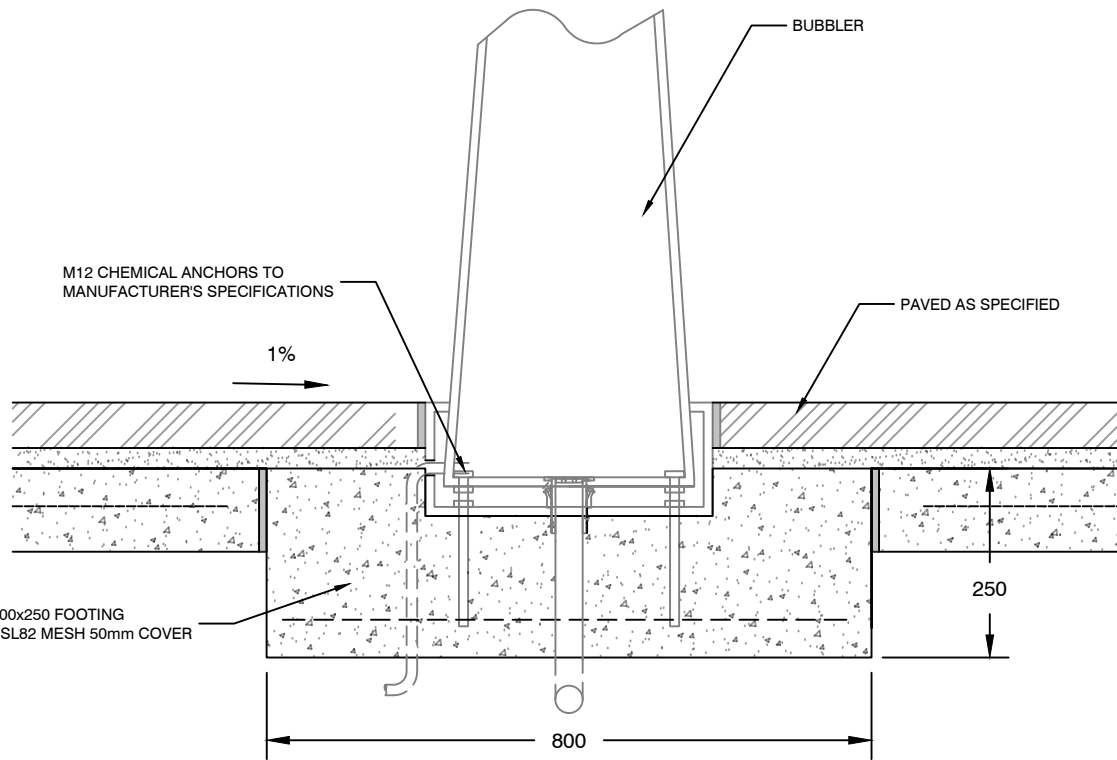
REMOVABLE BOLLARD

PAVEMENT SOCKET ELEVATION



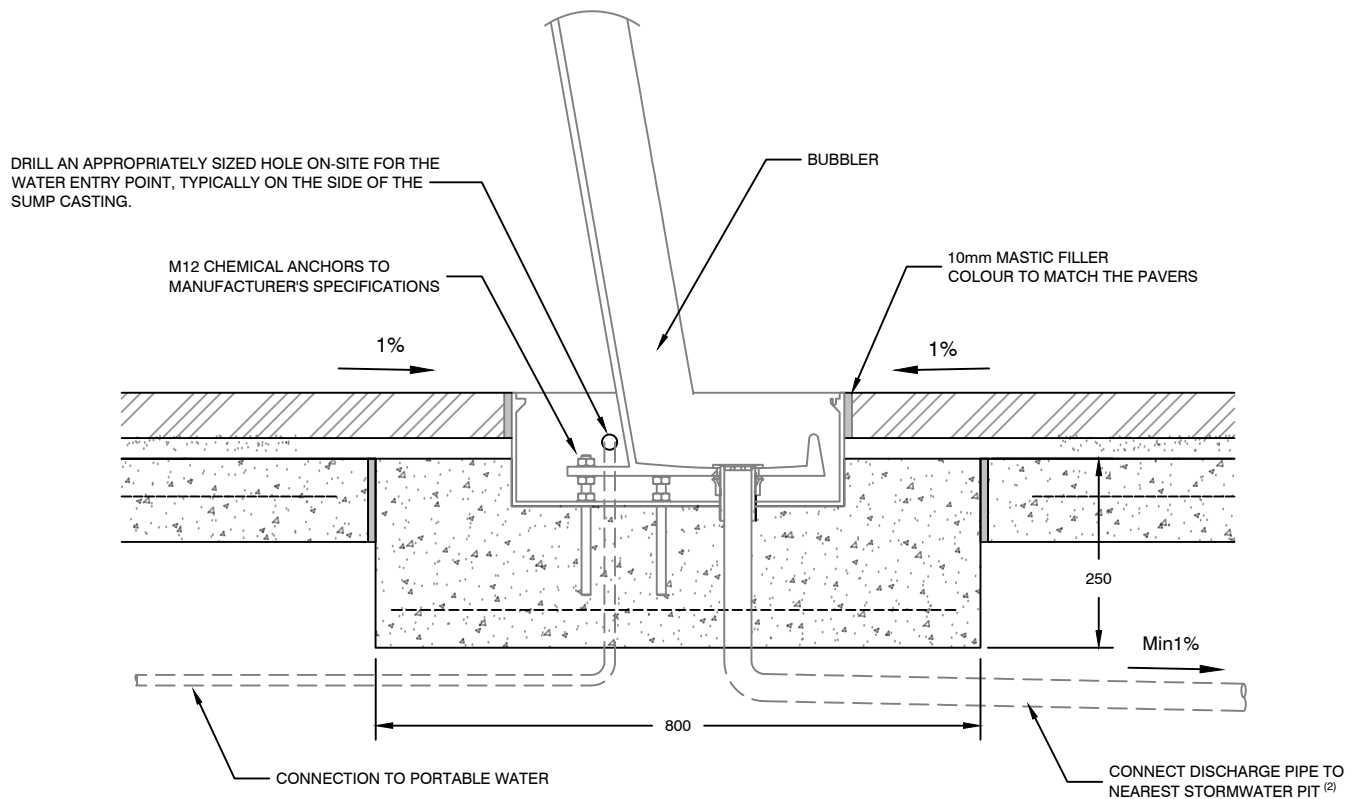
SCALE 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



FRONT VIEW

SECTION 1:10

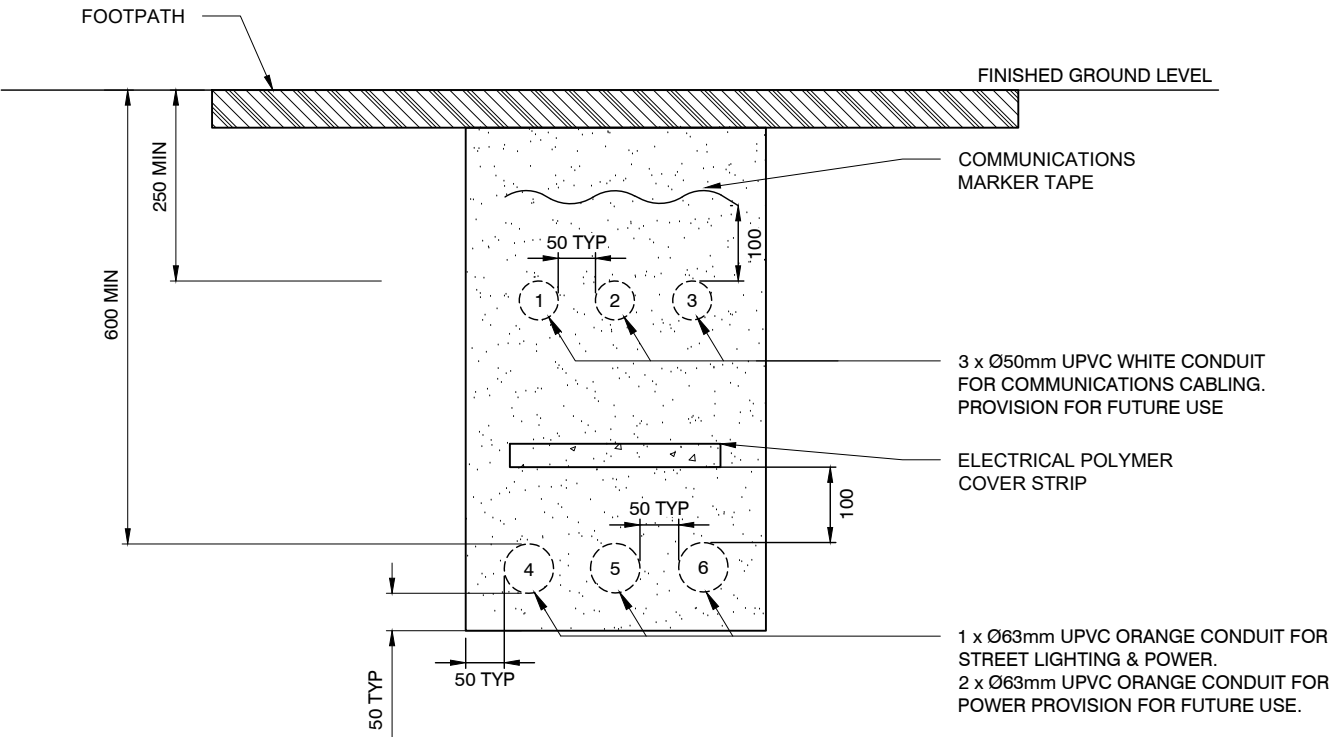


SIDE VIEW

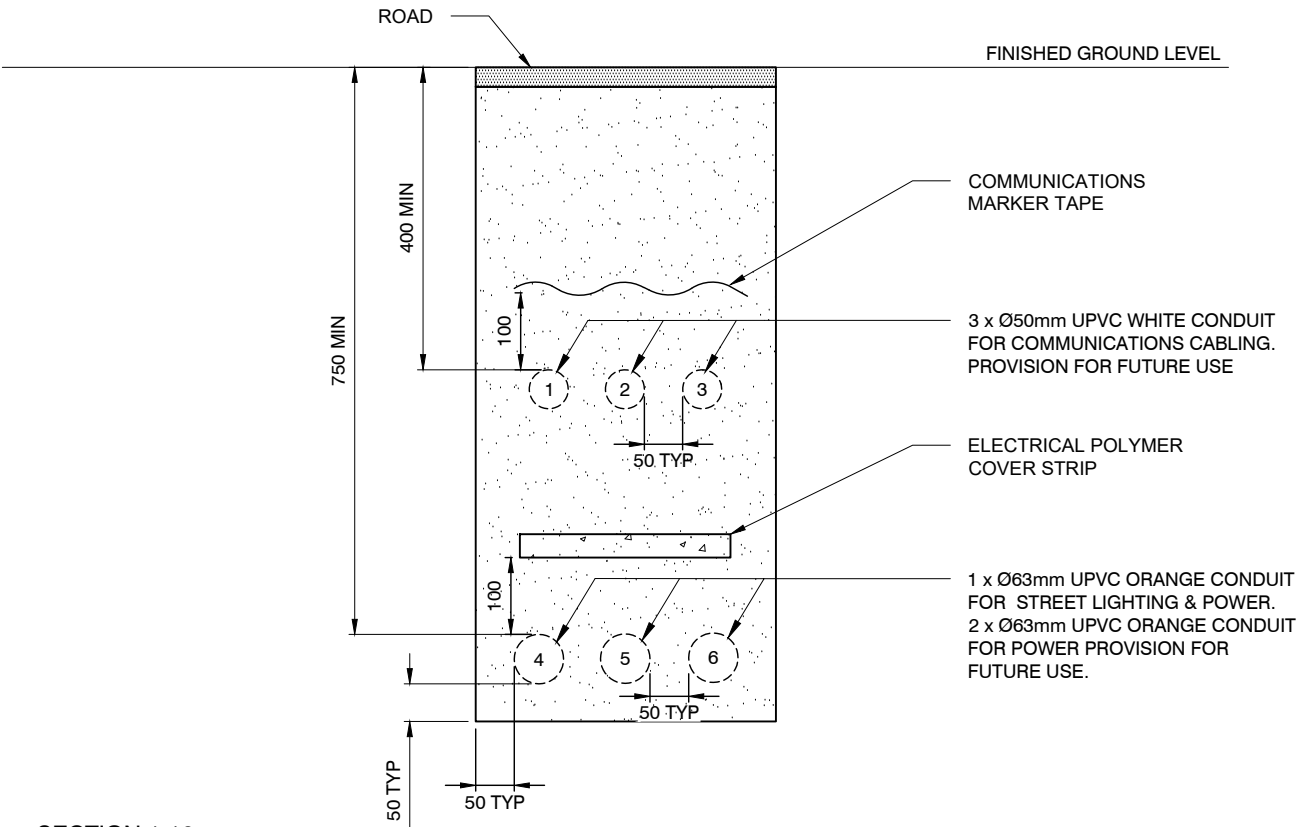
NOTES:

1. INDICATIVE LOCATIONS FOR THE POTABLE WATER ENTRY POINT AND DISCHARGE PIPE ARE SHOWN AND SHALL BE ADJUSTED AS REQUIRED BASED ON SITE CONDITIONS
2. DISCHARGE PIPE MAY BE CONNECTED TO KERB OUTLET IF NO NEARBY STORMWATER PIT EXISTS, SUBJECT TO SUFFICIENT FALL AND CITY'S REPRESENTATIVE APPROVAL
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

TYPICAL ELECTRICAL & COMMS
CONDUIT ARRANGEMENT FOOTPATH & GARDEN AREA

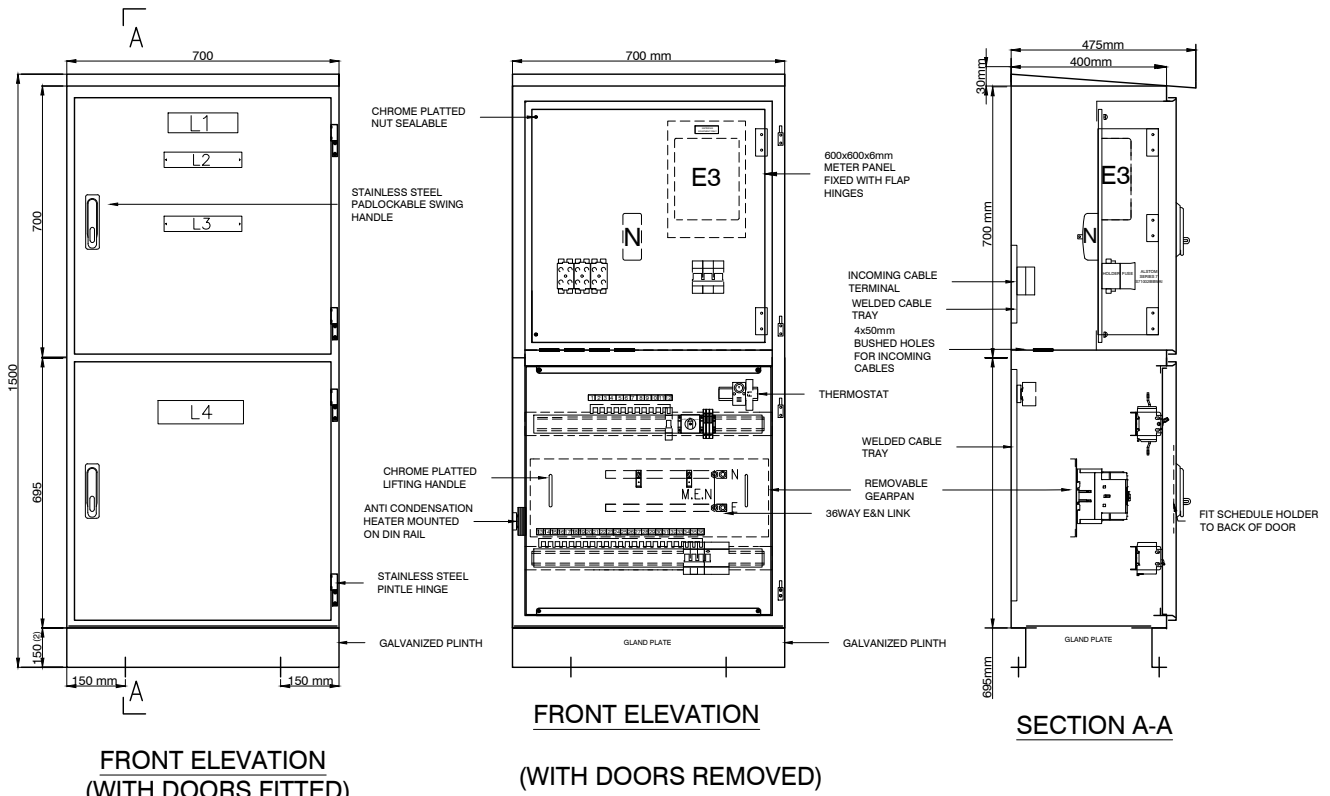


TYPICAL ELECTRICAL & COMMS
CONDUIT ARRANGEMENT (ROAD)

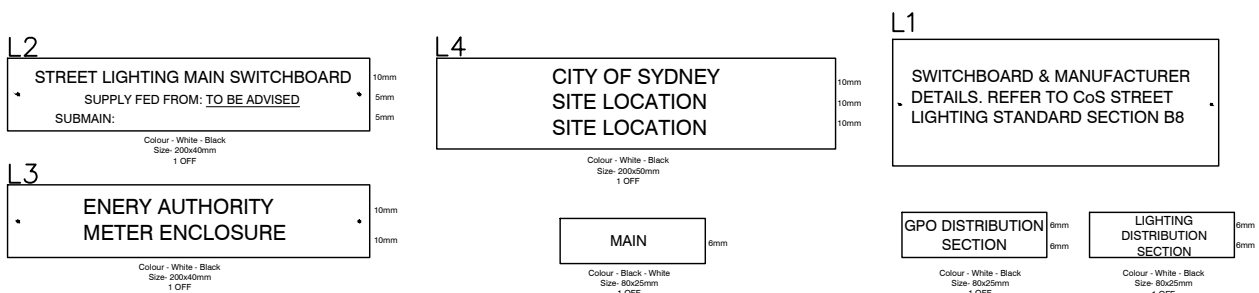


SECTION 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

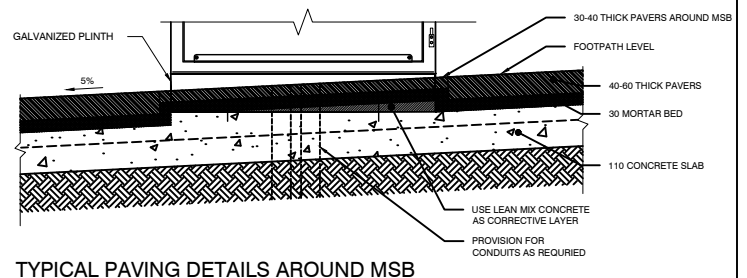


MINOR EQUIPMENT SCHEDULE				
ITEMS	DESCRIPTION	REFERENCE	PART NO.	QTY
SERVICE FUSES	IPD SERIES 7 SERVICE FUSE BACK CONNECTION + FUSE LINK	#S71002BBWAI+RHLP100		3
THREE PHASE METER	SUPPLIED AND FITTED BY OTHERS			1
SERVICE METER NEUTRAL LINK	NETEC SEALABLE LINKS 3x35 & 2x10mm	#AN100S-535-B		1
MAIN SWITCH CIRCUIT BREAKER				1
3P 12-WAY COMB BUSBAR	3P 12-WAY 100A COMB BUSBAR WITH ENDCAP	#SN-A9XPH312		1
3P 24-WAY COMB BUSBAR	3P 12-WAY 100A COMB BUSBAR WITH ENDCAP	#SN-A9XPH324		1
36 WAY BRASS LINKS	36 WAY N&E BRIBAR LINKS	#DB-BRASSLINK-36		2
ANTI CONDENSATION HEATER	IPD - 15W ANTICONDENSATION HEATER	#RACP-15		1
THERMOSTAT	IPD - 100 - 250V AC 1NC THERMOSTAT	#TRT-10A230V-NC		1
A-O-M SWITCH	TELUX DIN RAIL MOUNTED 20A NON-LOCABLE 2P SWITCH	#M10HEU1-SMA		1
TERMINAL	TERMINAL 2.5MM GREY	#ACC-TERM-GREY-2.5		3
SS316 PADLOCKABLE HANDLE	SWING HANDLE PADLOCKABLE - SS316	#GEN-HANDLE - SH-PL-316		2
MCB	1P 16A 6KA MCB C - CURVE	#SN-A9F44106		1
RCBO	3P 25A 6KA RCBO 30mA C Curve	#SN-A9F44350		1
RCBO	2P 20A 6KA RCBO 30mA C-CURVE			1



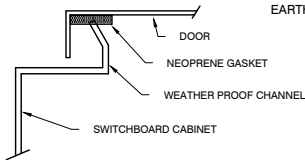
NOTES:

1. TO BE READ IN CONJUNCTION WITH DRAWING 5.1.4
2. 75 mm GALVANIZED CHANNEL PLINTH MAY BE USED WITH CONCRETE / ASPHALT FOOTWAY
3. CONCRETE PLINTH SHALL BE USED INSTEAD OF GALVANISED PLINTH IF LONGITUDINAL / CROSS FALL EXCEEDS 5%
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



CONSTRUCTION NOTES:

CONSTRUCTION:	FLOOR MOUNTED, FRONT CONNECTED, RATED AT 100A
CABINET:	2mm 316 STAINLESS STEEL, FOLDED & WELDED/MODULAR BOLTED CONSTRUCTION
DOORS AND ESCUTCHEONS:	1.5mm GRADE 316 STAINLESS STEEL
OTHER BRACKETS:	1.6mm ZINC SEALED MILD STEEL MIN
BOTTOM GLAND PLATE:	6mm METER PANEL
FORM OF SEGREGATION:	FORM 1 - TO AS3439.1 - 2002
DEGREE OF PROTECTION:	IP56 - REFER TO WEATHER PROOFING DETAIL
FAULT RATING:	6kA FOR 1 SECOND
FINISH:	
PREPERATION:	DE-SCALE & DE-GREASE
EXTENRAL COLOUR:	NATURAL FINISH
INTERNAL COLOUR:	NATURAL FINISH
REMOVALBLE GEAR PANS & ESCUTCHEON:	GLOSS WHITE
PLINTH:	GALVANISED
LABELS:	ENGRAVED PLASTIC LAMINATE
FIXING:	DOUBLE SIDED ADHESIVE & STAINLESS STEEL SCREWS
COLOUR:	AS SHOWN
CONTROL WIRING:	MINIMUM 1.5mm Cu V90
POWER WIRING:	MINIMUM 2.5mm Cu RE110 DOUBLE INSULATED
WIRE MARKERS:	STANDARD FERRULES
TERMINATIONS:	BARE CABLE ENDS. WHERE TERMINALS REQUIRE LUGS, BOOTLACE FERRULES OR PRE-INSULATED RING LUGS. <u>FORK LUGS SHALL NOT BE USED</u>
COLOUR:	240 AC - PHASE COLOURED NEUTRAL - BLACK EARTH - GREEN/YELLOW

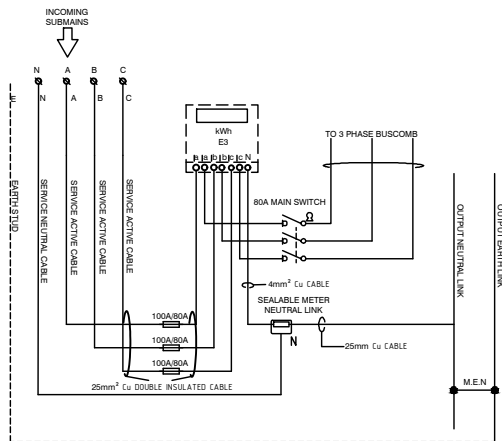


WEATHER PROOFING DETAIL

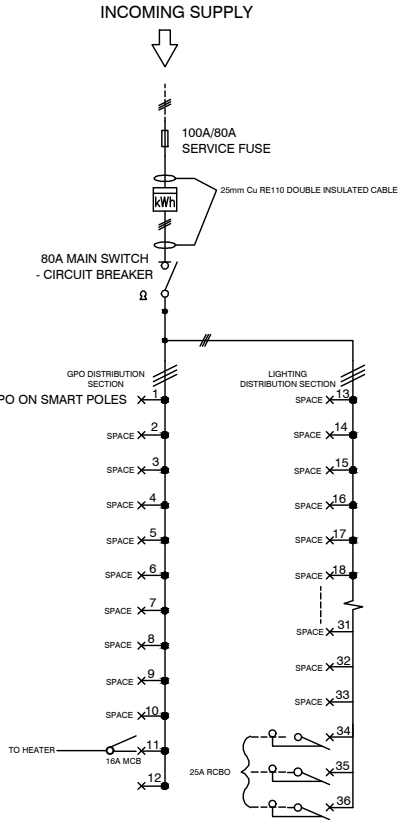
1 OFF

NOTES:

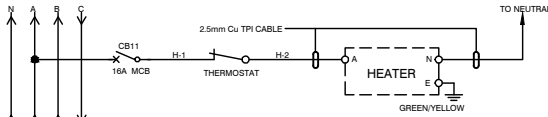
MAIN ISOLATOR TO BE LOCATED IN THE ENERGY AUTHORITY SECTION. BUS COMB TO BE FIXED SECURELY TO THE BUSBAR.



E3 METER WIRING SCHEMATIC



SINGLE LINE DIAGRAM

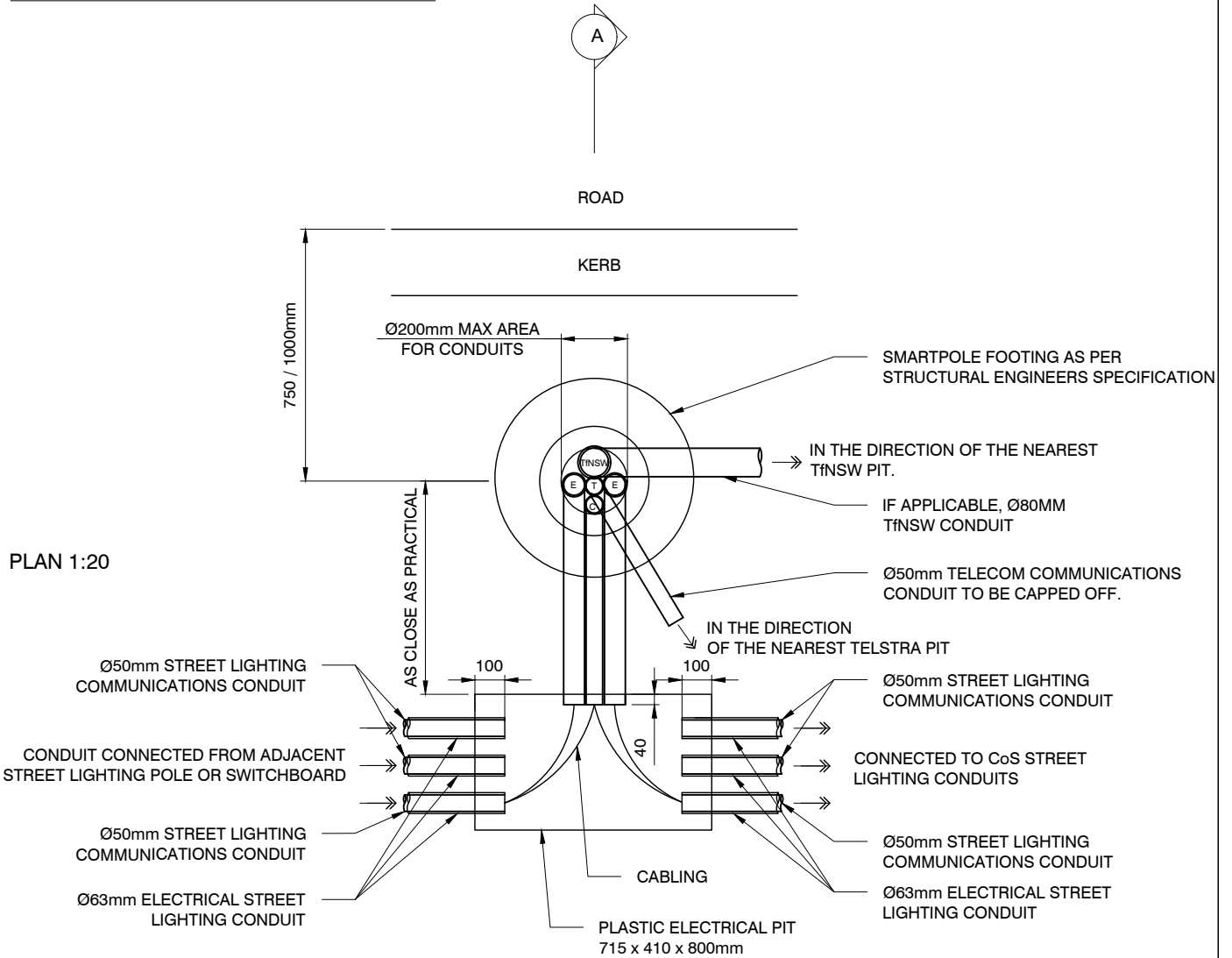


ANTICONDENSATION HEATER SCHEMATIC DIAGRAM

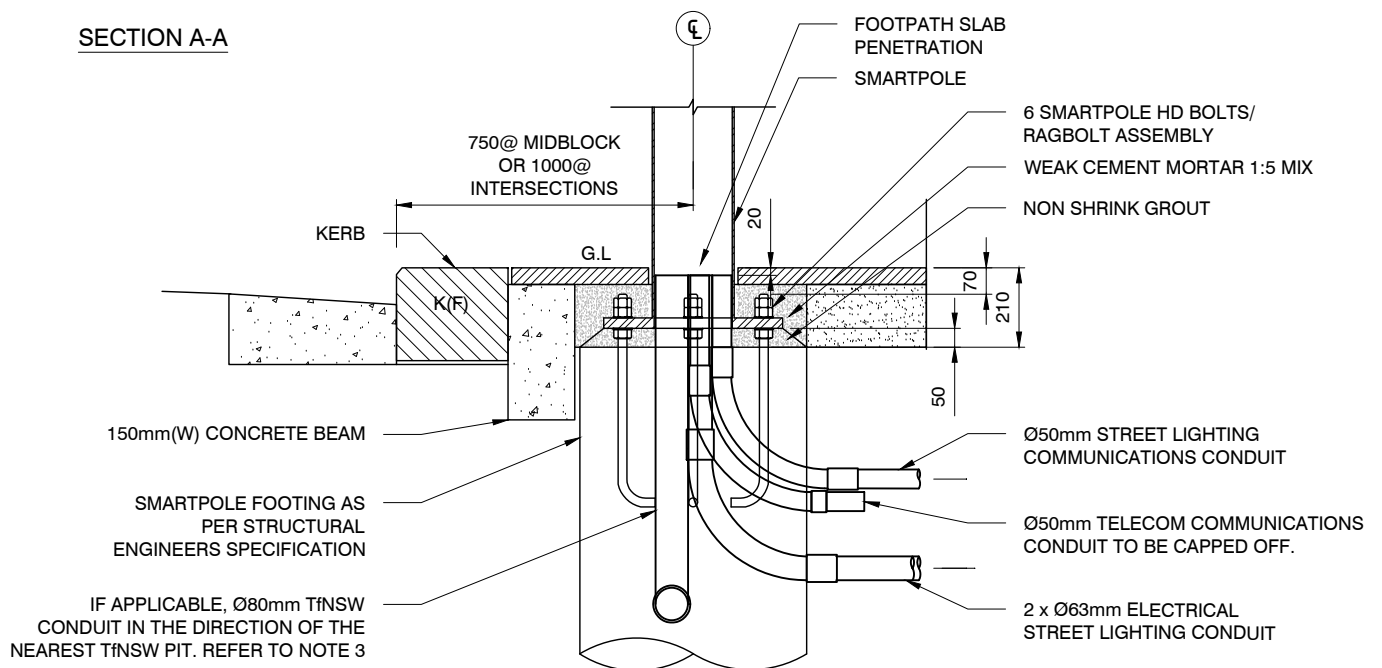
NOTES:

1. TO BE READ IN CONJUNCTION WITH DRAWING 5.1.3
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

CONDUIT ARRANGEMENTS AT POLE BASE

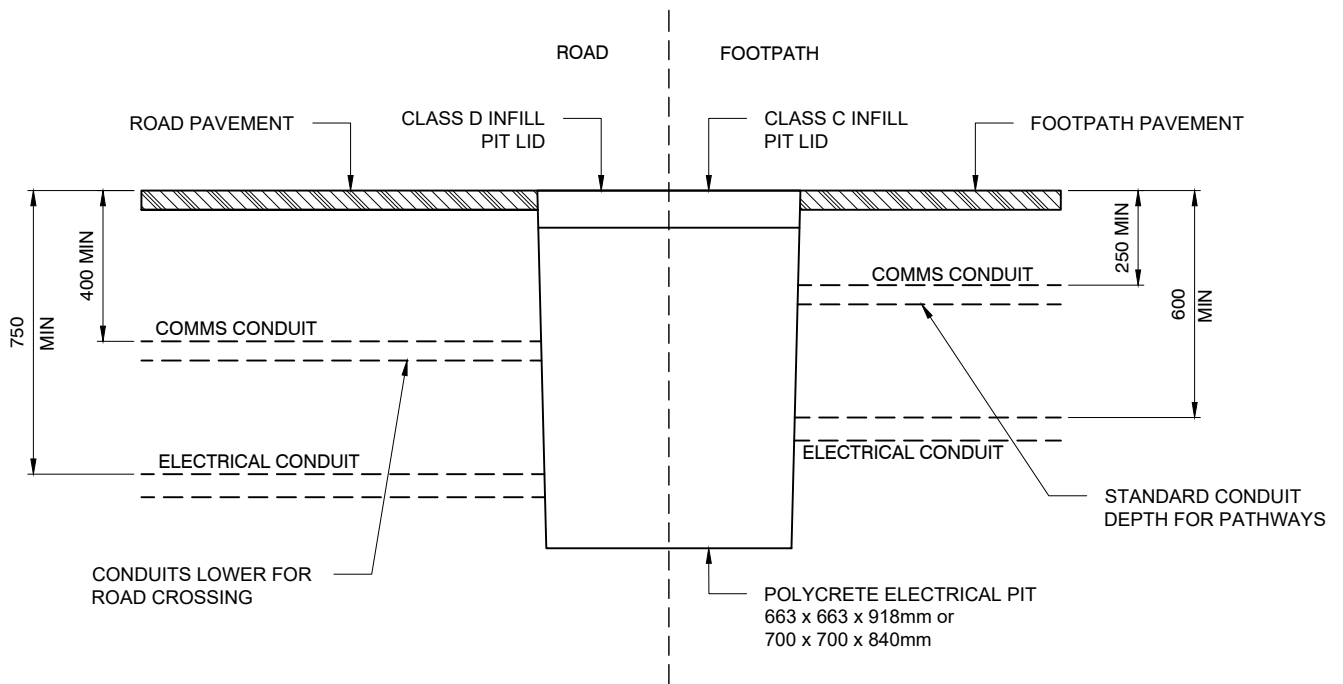


SECTION A-A



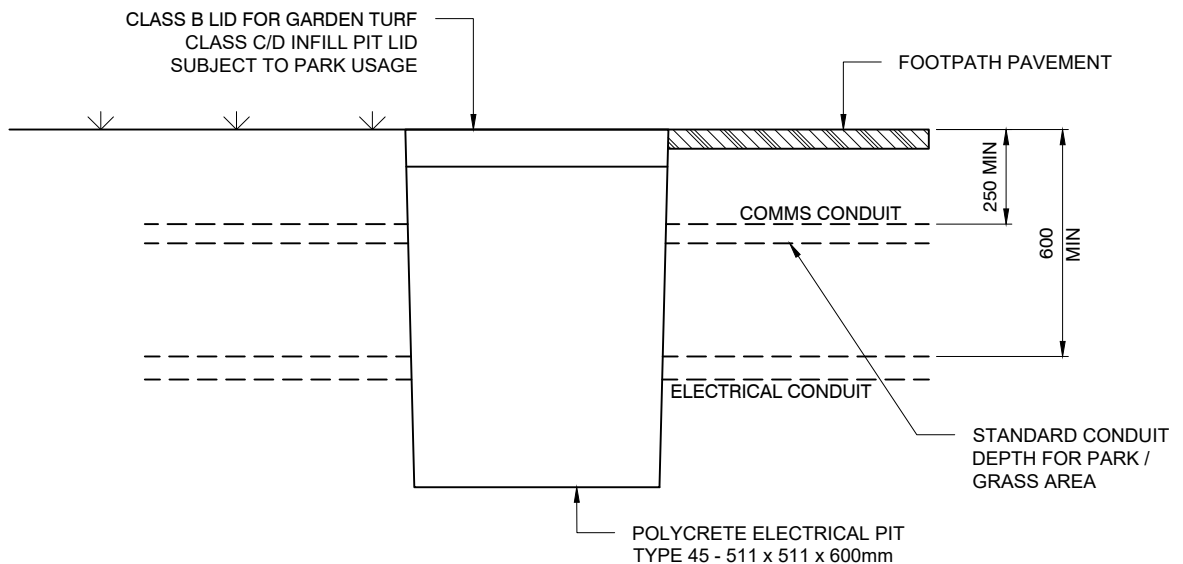
SECTION 1:20

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



ELECTRICAL PIT ARRANGEMENT FOR ROAD / FOOTPATH

SCALE 1:20

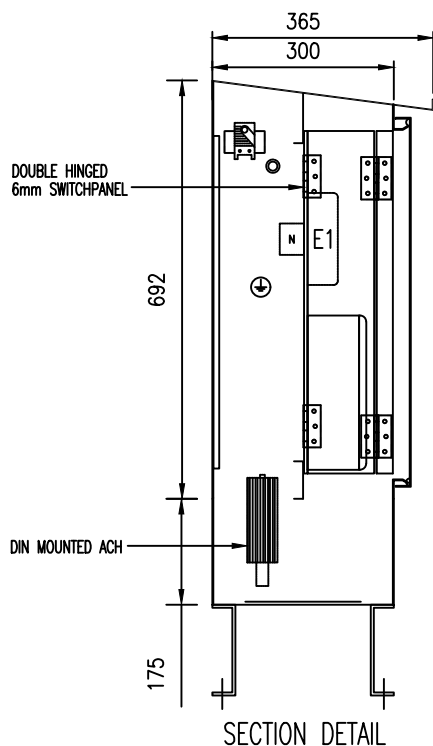
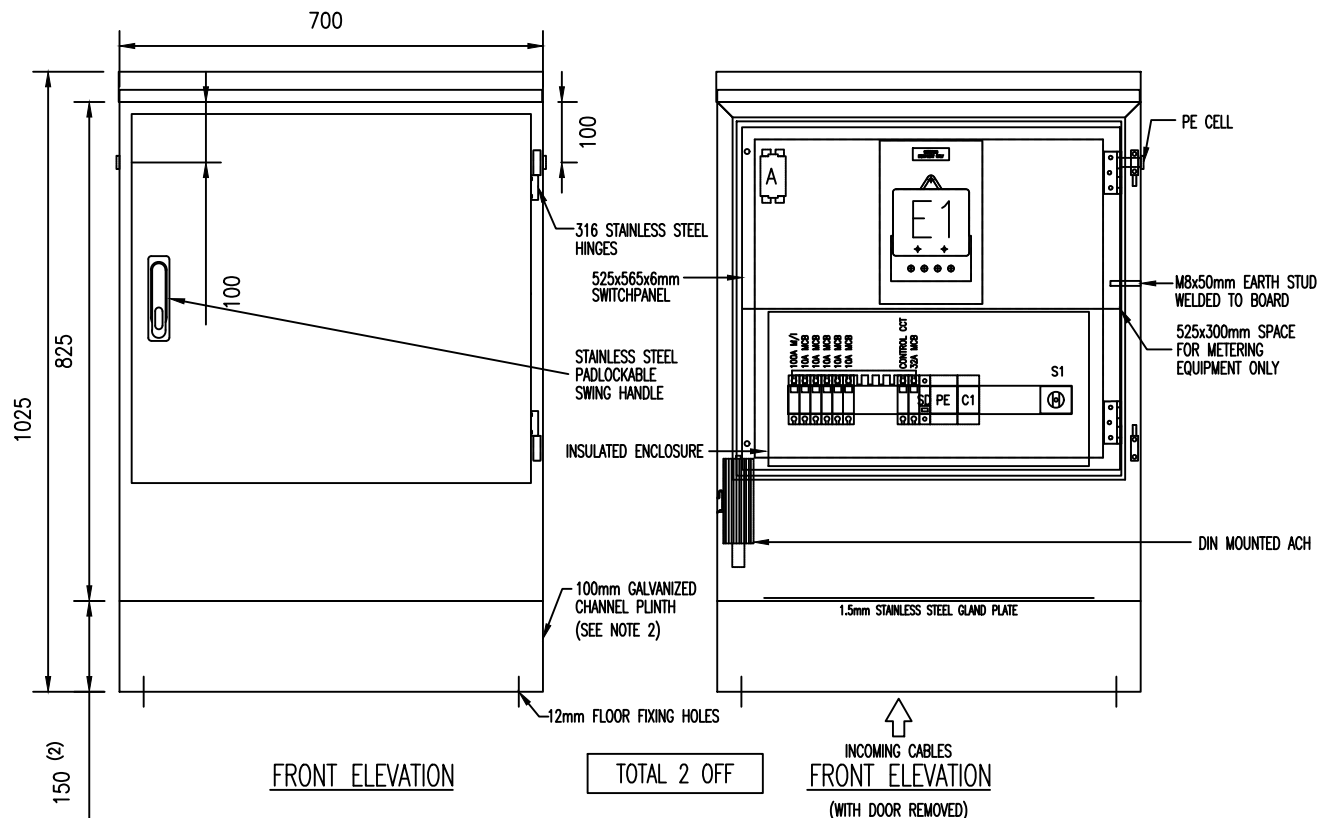


ELECTRICAL PIT ARRANGEMENT FOR PARKS

SCALE 1:20

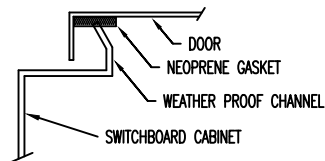
NOTES:

1. PIT LID IS TO HAVE A 75mm DIAMETER STAINLESS STEEL DISC ENGRAVED "CoS-Electrical" SET FLUSH WITH LID SURFACE USING EPOXY ADHESIVE. THIS DISC SHOULD BE SLIP RESISTANT.
2. REFER TO THE MANUFACTURER INSTALLATION INSTRUCTION FOR COMPLETE DETAILS
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



CONSTRUCTION NOTES:

CABINET:	1.5mm STAINLESS STEEL 316 GRADE
DOOR:	1.5mm STAINLESS STEEL 316 GRADE
PLINTH:	75mm x 38mm MILD STEEL CHANNEL
ESCUTCHEON & GEAR PAN:	1.5mm STAINLESS STEEL 316 GRADE
GLAND PLATES:	1.5mm STAINLESS STEEL 316 GRADE
FORM OF SEGREGATION:	FORM 1
DEGREE OF PROTECTION:	IP-66 - REFER TO WEATHER PROOFING DETAIL
FAULT RATING:	6KA
FINISH:	DE-SCALE & DE-GREASE
PREPARATION:	GRAIN FINISH
EXTERNAL COLOUR:	GRAIN FINISH
INTERNAL COLOUR:	GLOSS WHITE
BRACKETS:	HOT DIP GALVANISED
PLINTH:	
LABELS:	ENGRAVED PLASTIC LAMINATE
FIXING:	DOUBLE SIDED ADHESIVE & SCREW FIXED
COLOUR:	MAIN ISOLATOR - WHITE LETTERS ON BLACK WARNING, FIRE & LIFTS - WHITE LETTERS ON RED OTHERS - BLACK LETTERS ON WHITE
CONTROL WIRING:	MINIMUM 1.5mm Cu V90
WIRE MARKERS:	SMB HARVAL STANDARD FERRULES
TERMINATIONS:	BARE CABLE ENDS. WHERE TERMINALS REQUIRE LUGS, BOOTLACE FERRULES OR PRE-INSULATED RING LUGS.
COLOUR:	240V AC - PHASE COLOURED NEUTRAL - BLACK EARTH - GREEN/YELLOW AC CONTROL CIRCUIT ACTIVE- RED

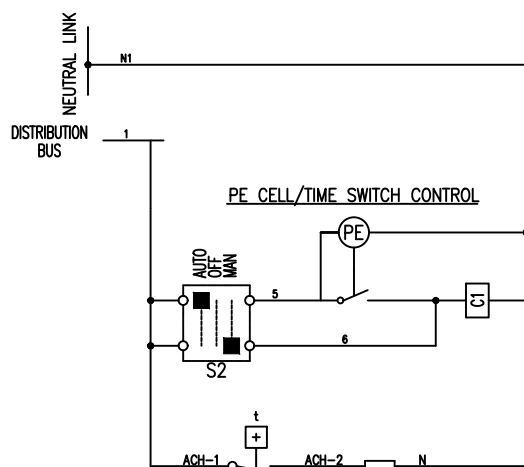


WEATHER PROOFING DETAIL

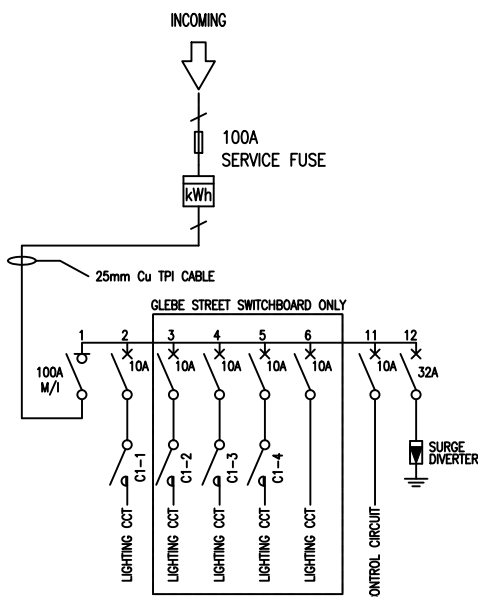
NOTES:

1. TO BE READ IN CONJUNCTION WITH DRAWING 5.1.8
2. 75 mm GALVANIZED CHANNEL PLINTH MAY BE USED WITH CONCRETE / ASPHALT PAVING
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

EQUIPMENT SCHEDULE				
ITEMS	ITEMS INFORMATION		QTY	TOTAL
	DESCRIPTION	PART NUMBER		
SERVICE FUSE	IPD SERIES 7 SERVICE FUSE	ACC-FUS-C1002BBWA	1	2
	FITTED WITH FUSE LINK	ACC-FUS-LRHLF-100	1	2
SINGLE PHASE METER	SUPPLIED AND FITTED BY OTHERS		1	2
SEALABLE NEUTRAL LINK	NETEC SEALABLE LINK 100A	AN100S-535-B	1	2
DOOR SWING HANDLE	STAINLESS STEEL PADLOCKABLE SWING HANDLE	GEN-HANDLE-SH-PL-316	1	2
INSULATED ENCLOSURE	CLIPSAL 24 MODULE SURFACE MOUNTING INSULATED ENCLOSURE	SN-4CF24FD	1	2
1P 12-WAY COMB BUSBAR	SCHNEIDER 1P 12-WAY 80A COMB BUSBAR	SN-BUSCOMB12P	1	2
1P 100A ISOLATOR	SCHNEIDER 1P 100A ISOLATOR	SN-A9S66191	1	2
1P 32A MCB	SCHNEIDER 1P 32A MCB 6kA C-CURVE	SN-A9F44132	1	2
1P 10A MCB	SCHNEIDER 1P 10A MCB 6kA C-CURVE (GLEBE ST SW =7 & JONES ST SW=2)	SN-A9F44110	-	9
LIGHT SENSITIVE SWITCH	SCHNEIDER IC200 LIGHT SENSITIVE SWITCH COMPLETE WITH PRE-WIRED PE CELL	SN-152B4	1	2
4P CONTACTOR [C1]	SCHNEIDER 4P 25A N/O CONTACTOR 240V AC COIL	SN-A9C20834	1	2
SELECTOR SWITCH [S1]	TELUX SURFACE MOUNT CHANGEOVER SWITCH ENGRAVED AUTO-OFF-MAN	BA-M10HSMAU1	1	2
SURGE DIVERTER	LDU GKSDL SINGLE PHASE SINGLE MODE 50kA SURGE DIVERTER	LD-GKSDL1-50-275	1	2
ANTI-CONDENSATION HEATERS	IPD- 240VAC 100W HEATER	IP-RACM-100	1	2
THERMOSTAT	IPD - THERMOSTAT 1 N/C 10A 230VAC	IP-TRT-10A230-NC	1	2



LIGHTING CONTROL CIRCUIT SCHEMATIC DIAGRAM



SINGLE LINE DIAGRAM

CITY OF SYDNEY MAIN SWITCHBOARD.
LOCATION: #
SUPPLY FED FROM : TO BE ADVISED
SUBMAIN : 2-1Cx25mm Cu XLPE
ASSET ID: TBA

Colour - White-Black
Size - 200x40mm
1 OFF

----- STREET OR ----- STREET

Manufactured by: MANUFACTURERE COMPANY NAME

Telephone: ----- e-mail: -----
Drawing Reference: ----- Date of Manufacture: -----
Proudly Engineered & Manufactured in Australia

Colour : Blue-White-Blue
Size : 180x50mm

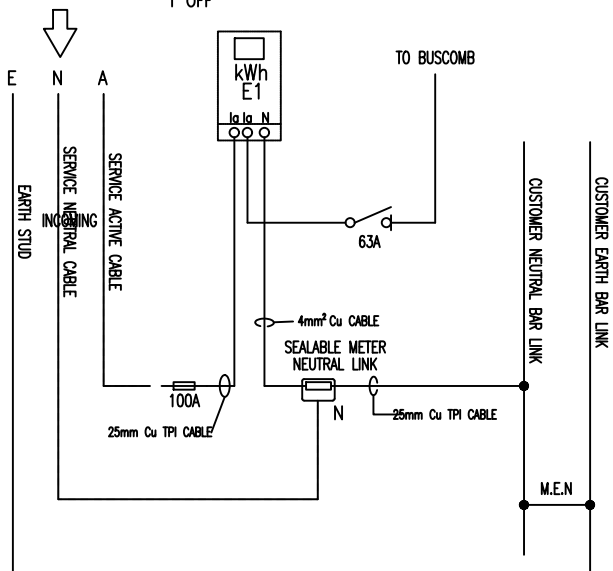
ELECTRICITY METER
LOCATED BEHIND

Colour : White-Black
Size : 120x30mm
1 OFF

1 OFF

S1 C1

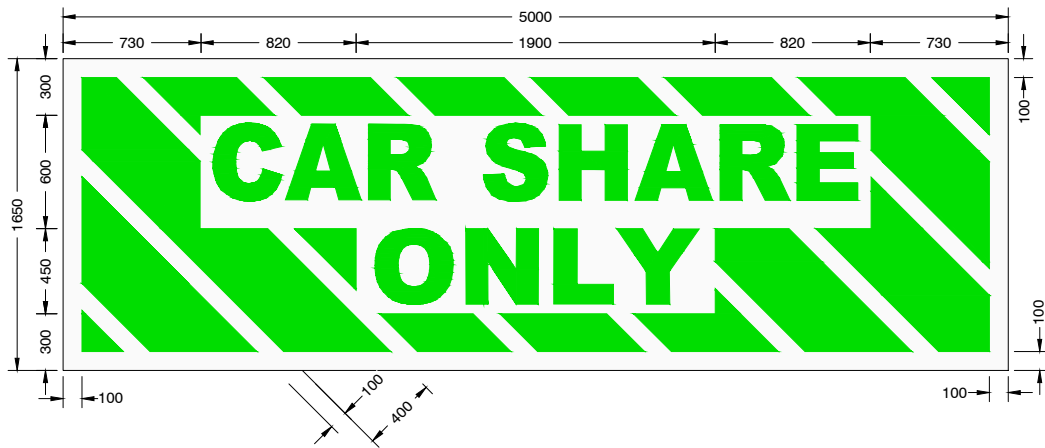
Colour : White-Black
Size : 18x18mm
6mm TEXT
1 OFF EACH



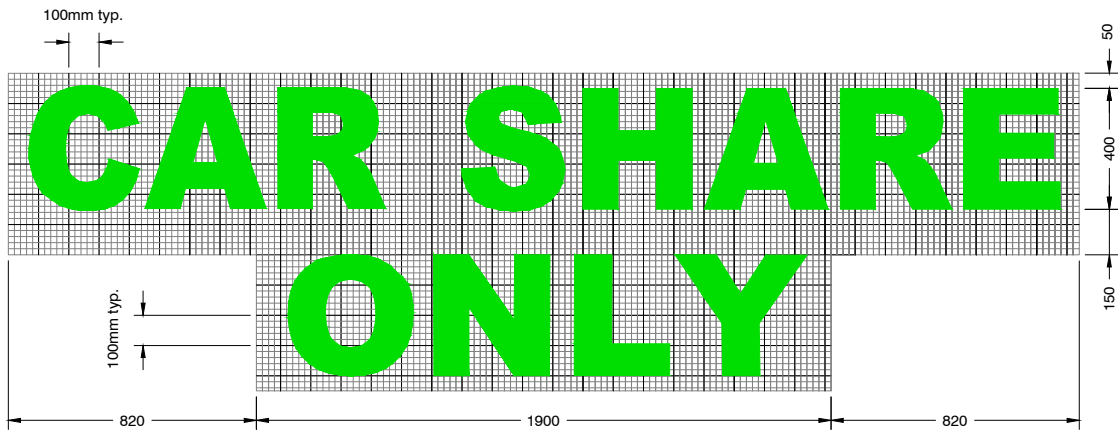
E1 METER WIRING SCHEMATIC

NOTES:

1. TO BE READ IN CONJUNCTION WITH DRAWING 5.1.7
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



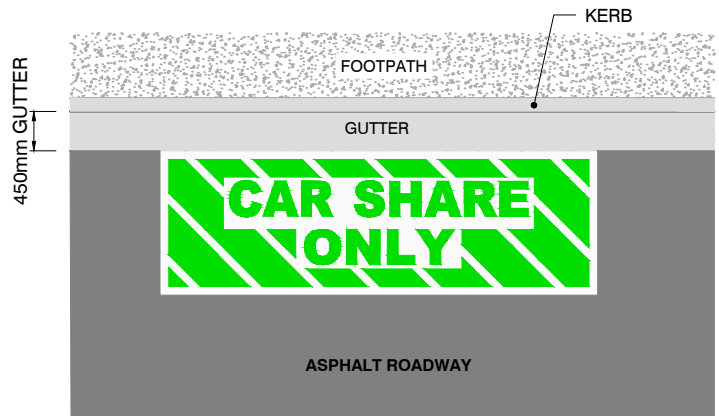
LINEMARKING PLAN
SCALE 1:40



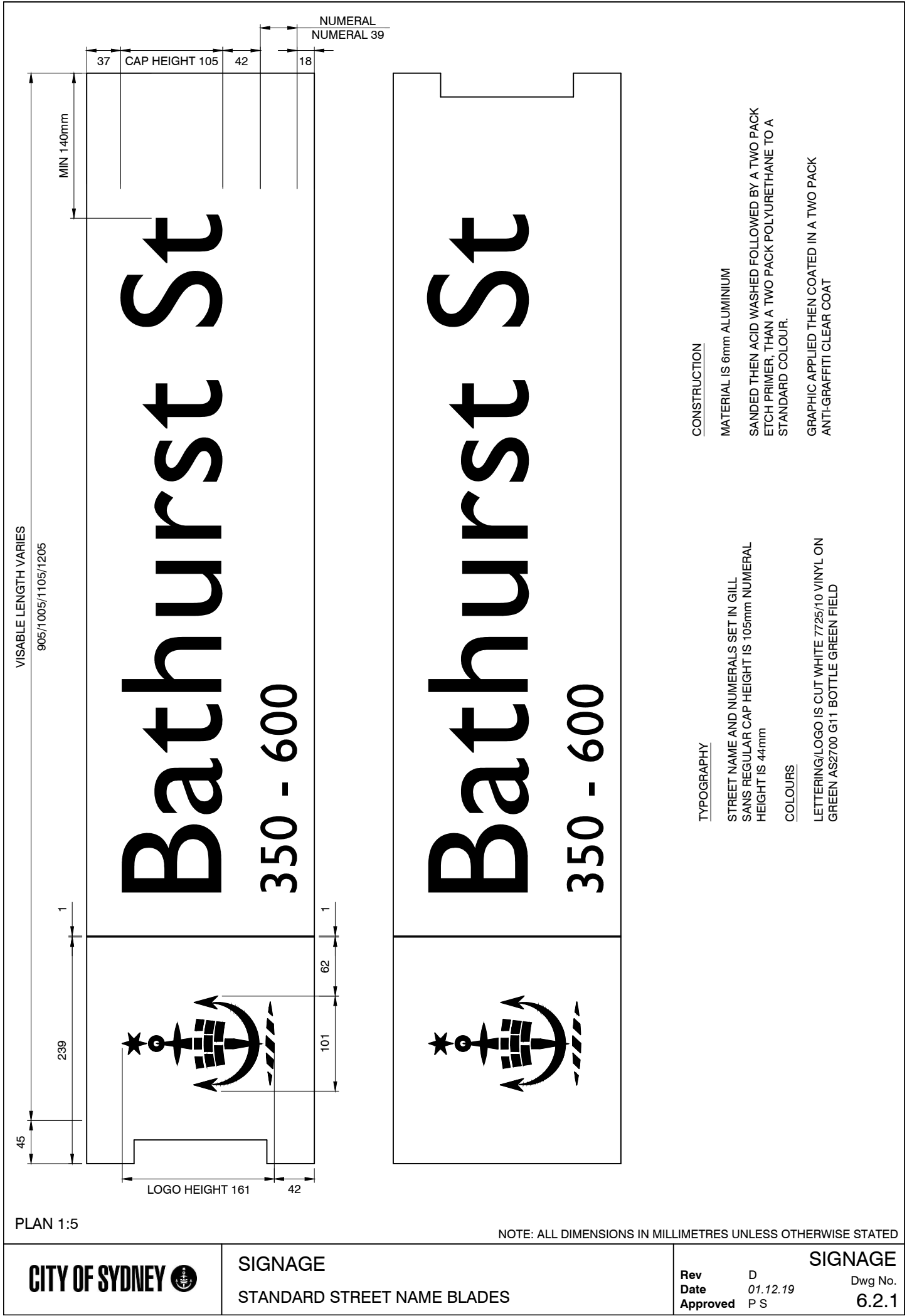
TEXT ALIGNMENT PLAN
SCALE 1:25

NOTES:

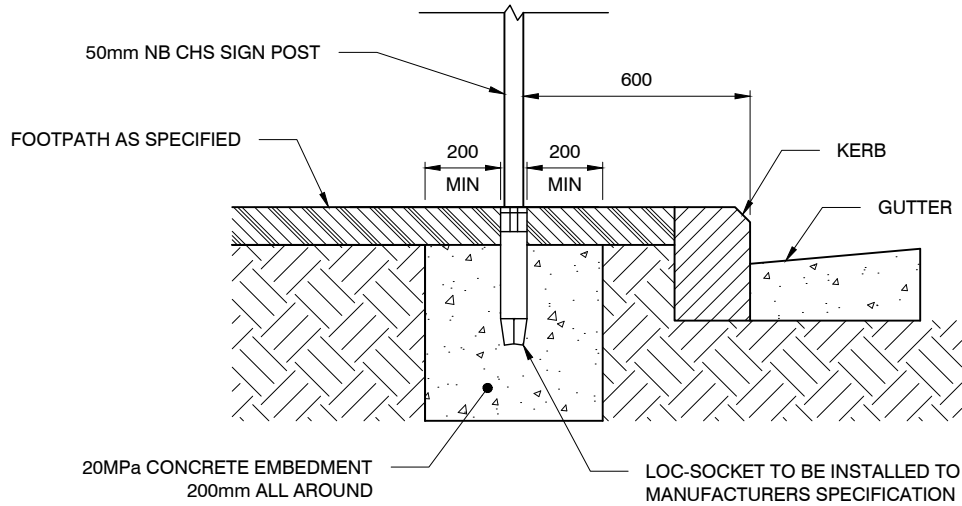
1. TEXT HEIGHTS AND WIDTHS AS SHOWN.
2. TEXT SHALL BE CENTRALLY LOCATED.
3. TEXT SHALL BE IN GREEN LETTERING ON A WHITE BACKGROUND WITH A 100mm WIDE WHITE BORDER.
4. CHEVRON MARKING SHALL BE 400mm WIDE GREEN STRIPS WITH 100mm WIDE WHITE LINES AS SHOWN.
5. CHEVRON LINES SHALL BE AT A 45° ANGLE TO THE KERB AS SHOWN.
6. MARKING SHALL BE ALIGNED FLUSH WITH GUTTER LIP.
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



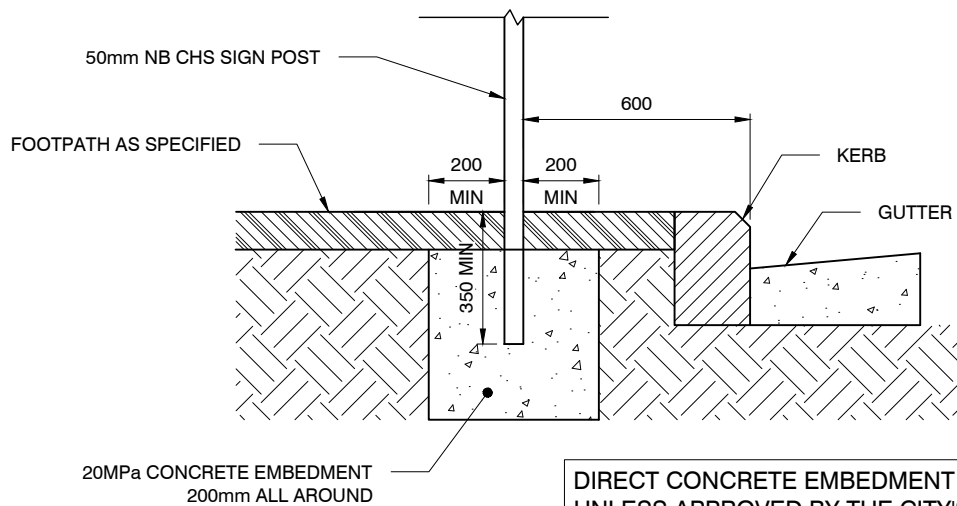
OVERALL PLAN
N.T.S



LOK-SOCKET INSTALLATION

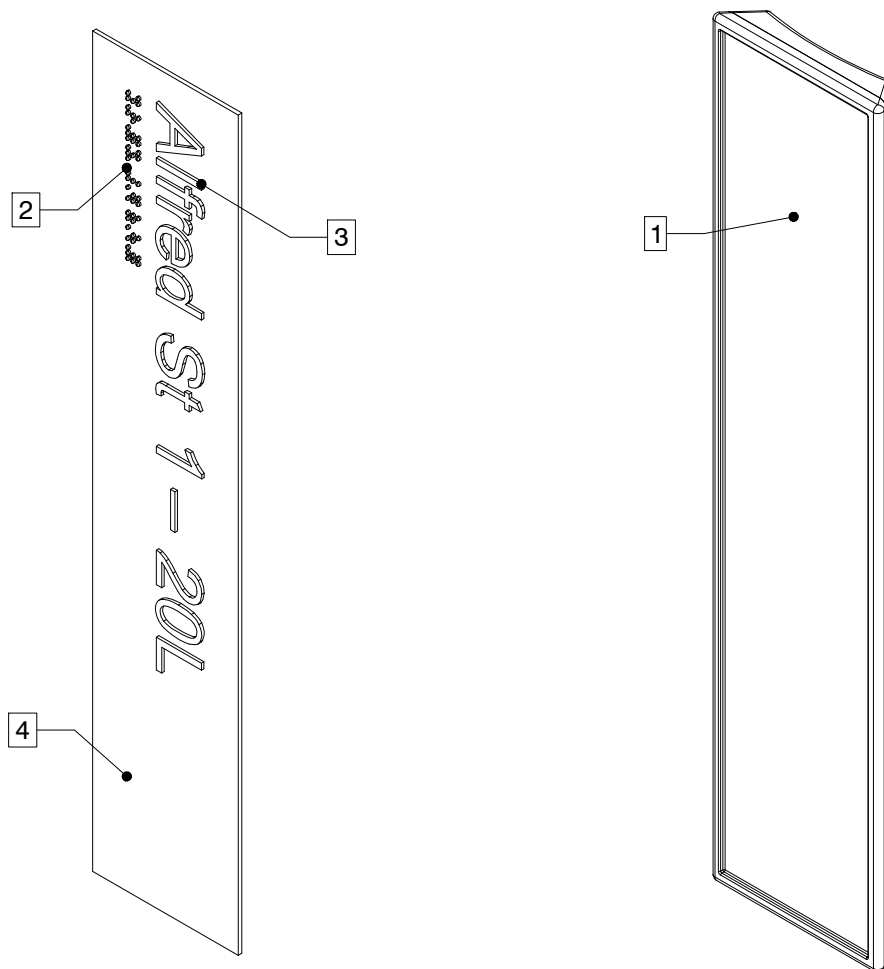


DIRECT CONCRETE EMBEDMENT (ALTERNATIVE OPTION ONLY IF APPROVED)



SECTION 1:20

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

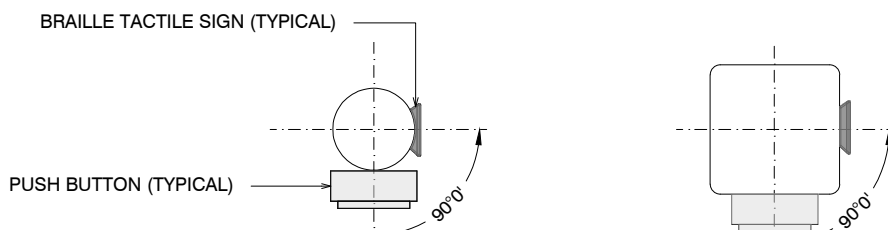
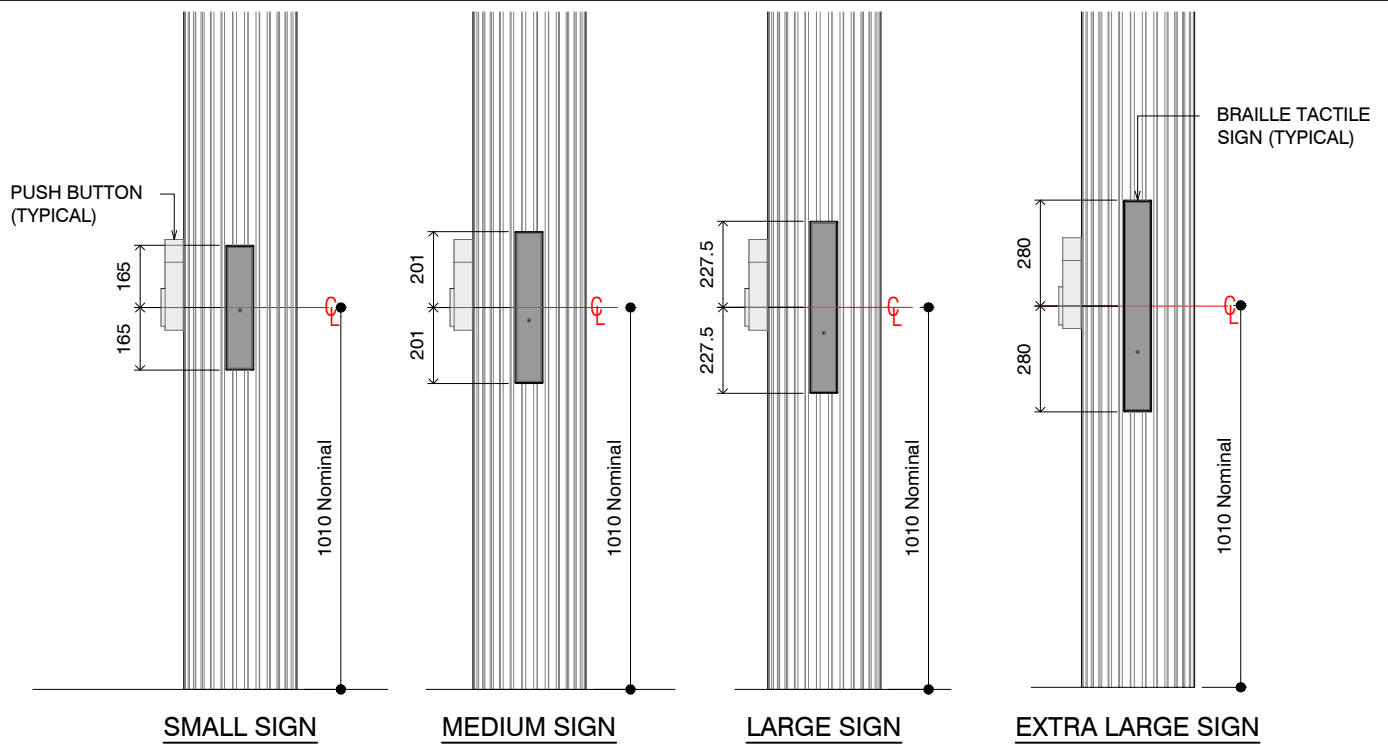


COMPONENTS:

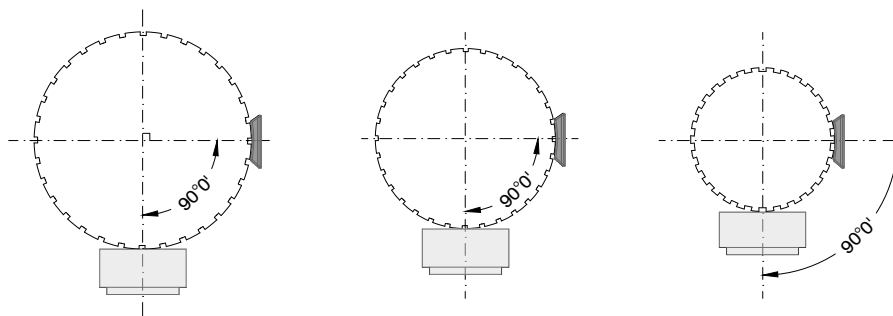
- 1 Casting Component:
- 2 Braille Component:
- 3 Tactile Letters Component:
- 4 Base Component:

NOTES:

1. TACTILE BRAILLE SIGNS SHALL BE INSTALLED ON EVERY POLE WHERE TRAFFIC SIGNAL PUSH BUTTON EXISTS OR BEING INSTALLED. REFER TO 'LEGIBLE SYDNEY DESIGN MANUAL' FOR DETAILED SPECIFICATIONS.
2. SIZE, TYPE, CONTENT, LOCATION, MATERIAL AND INSTALLATION DETAILS OF ANY SIGNS SHALL BE SUBMITTED AND APPROVED BY CITY'S ELECTRICAL AND STREET FURNITURE'S ASSET MANAGEMENT TEAM.
3. TYPE, DESIGN, CONTENT, MESSAGING AND SIZE OF THE TACTILE SIGN VARIES DEPENDING ON THE TYPE OF THE POLE.
4. THIS DRAWING SHALL BE READ IN CONJUNCTION WITH DRAWING 6.3.3.
5. TACTILE/BRAILLE SIGNAGE SHALL BE LOCATED IN A STANDARD POSITION.
6. TACTILE/BRAILLE SIGNAGE SHALL BE PLACED ON THE RIGHT-HAND SIDE OF THE AUDIO TACTILE PEDESTRIAN CALL BUTTON.
7. THE CENTRE OF THE SIGN SHALL BE LEVEL WITH THE CENTRE OF THE AUDIO-TACTILE PEDESTRIAN CALL BUTTON.
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



INSTALLATION ON VARIOUS TYPES OF TfNSW-APPROVED POLES

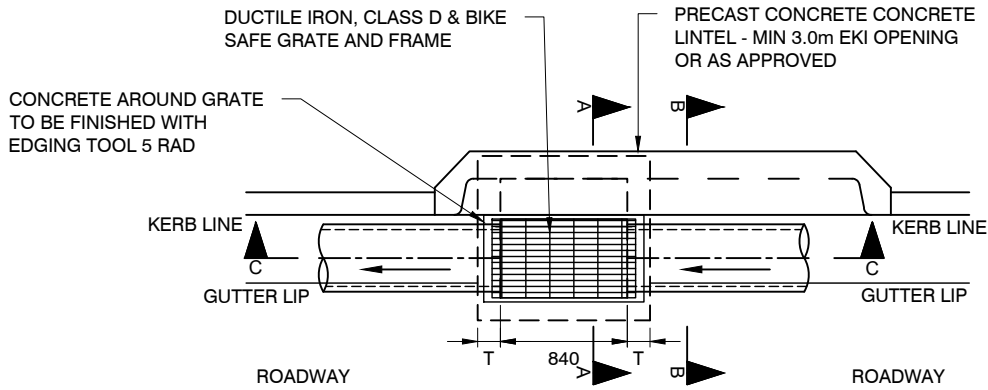


INSTALLATION ON VARIOUS TYPES OF SMARTPOLES

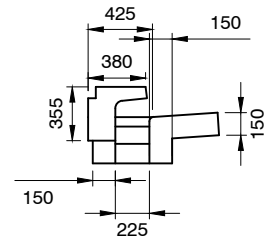
NOTES:

1. TACTILE BRAILLE SIGNS SHALL BE INSTALLED ON EVERY POLE WHERE TRAFFIC SIGNAL PUSH BUTTON EXISTS OR BEING INSTALLED.
2. SIZE, TYPE, CONTENT, LOCATION, MATERIAL AND INSTALLATION DETAILS OF ANY SIGNS SHALL BE SUBMITTED AND APPROVED BY CITY'S ELECTRICAL AND STREET FURNITURE'S ASSET MANAGEMENT TEAM.
3. TYPE AND SIZE OF THE TACTILE SIGN VARIES DEPENDING ON THE TYPE OF THE POLE.
4. THIS DRAWING SHALL BE READ IN CONJUNCTION WITH DRAWING 6.3.3.
5. TACTILE/BRAILLE SIGNAGE SHALL BE LOCATED IN A STANDARD POSITION.
6. TACTILE/BRAILLE SIGNAGE SHALL BE PLACED ON THE RIGHT-HAND SIDE OF THE POLE AS THE READER FACES THE KERB.
7. THE CENTRE OF THE SIGN SHALL BE LEVEL WITH THE CENTRE OF THE AUDIO-TACTILE PEDESTRIAN CALL BUTTON.
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

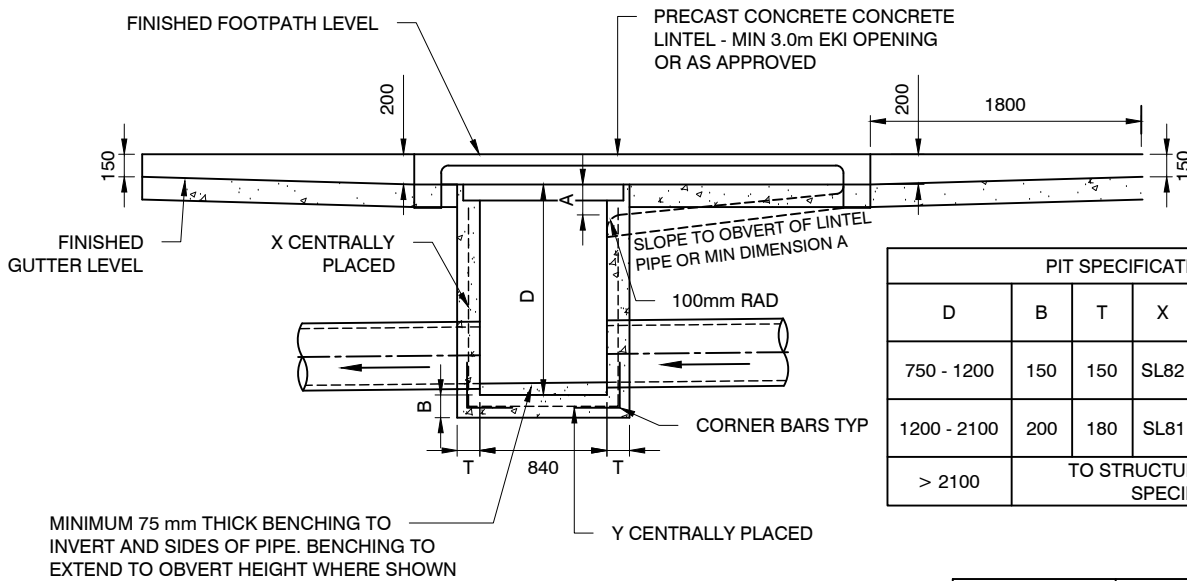
PLAN



SECTION B-B



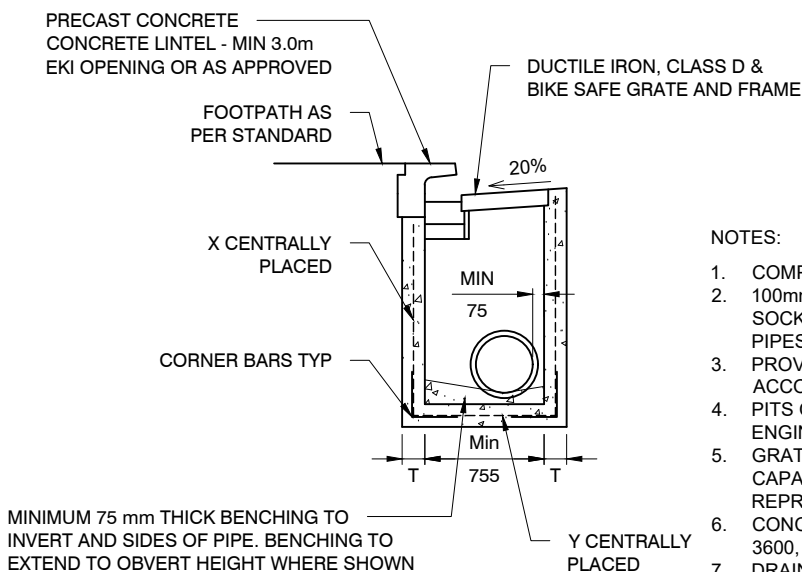
SECTION C-C



PIT SPECIFICATIONS					
D	B	T	X	Y	CORNER BARS
750 - 1200	150	150	SL82	SL81	N12-300 L 500
1200 - 2100	200	180	SL81	SL81	N12-200 L 600
> 2100	TO STRUCTURAL ENGINEERS SPECIFICATION				

INLET LENGTH - EKI (m)	MINIMUM DIMENSION A (mm)
1.8	250
2.4	300
3.0	400
3.6	450
4.2	500

SECTION A-A

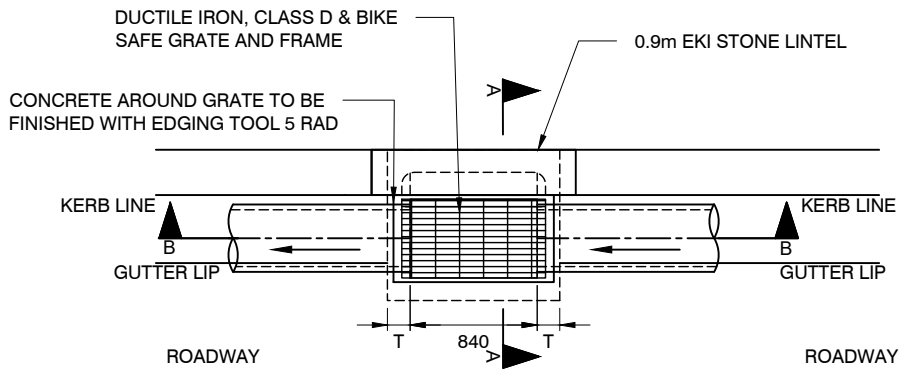


NOTES:

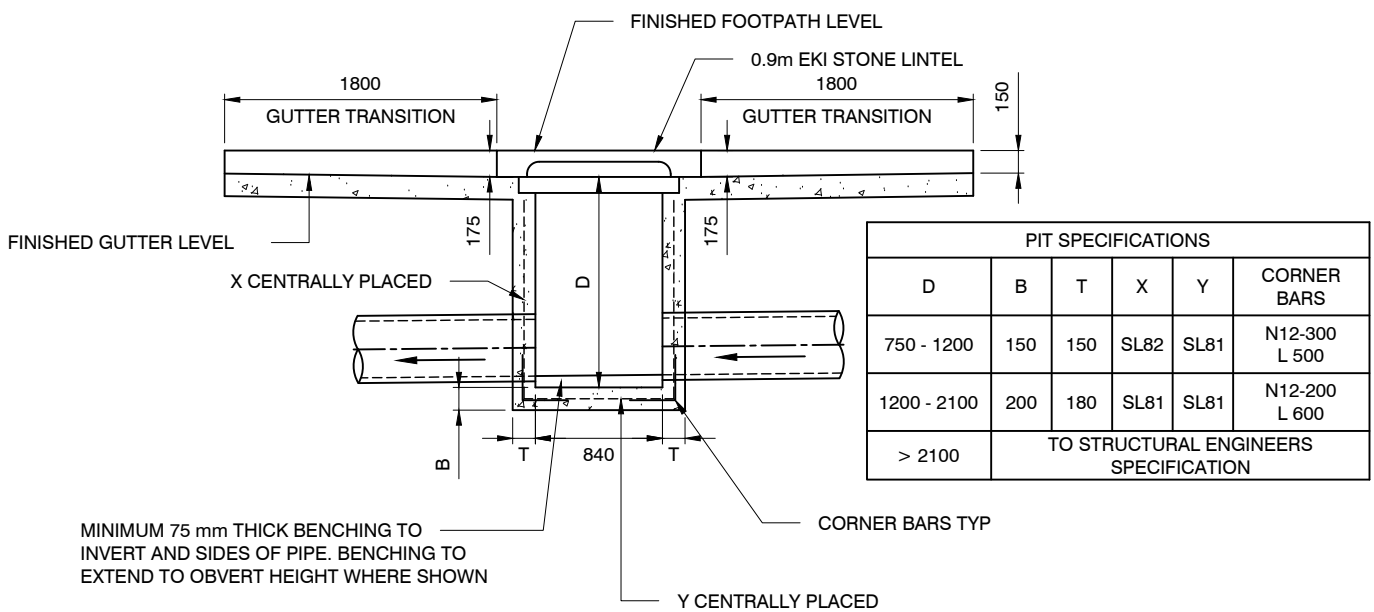
1. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
2. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
3. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
4. PITS OVER 2.1m IN DEPTH TO BE DESIGNED BY STRUCTURAL ENGINEER.
5. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
6. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
7. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:50

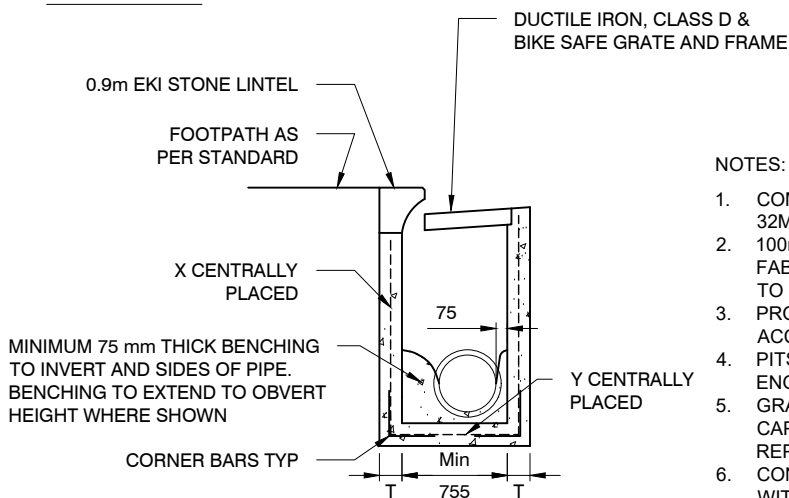
PLAN



SECTION B-B



SECTION A-A

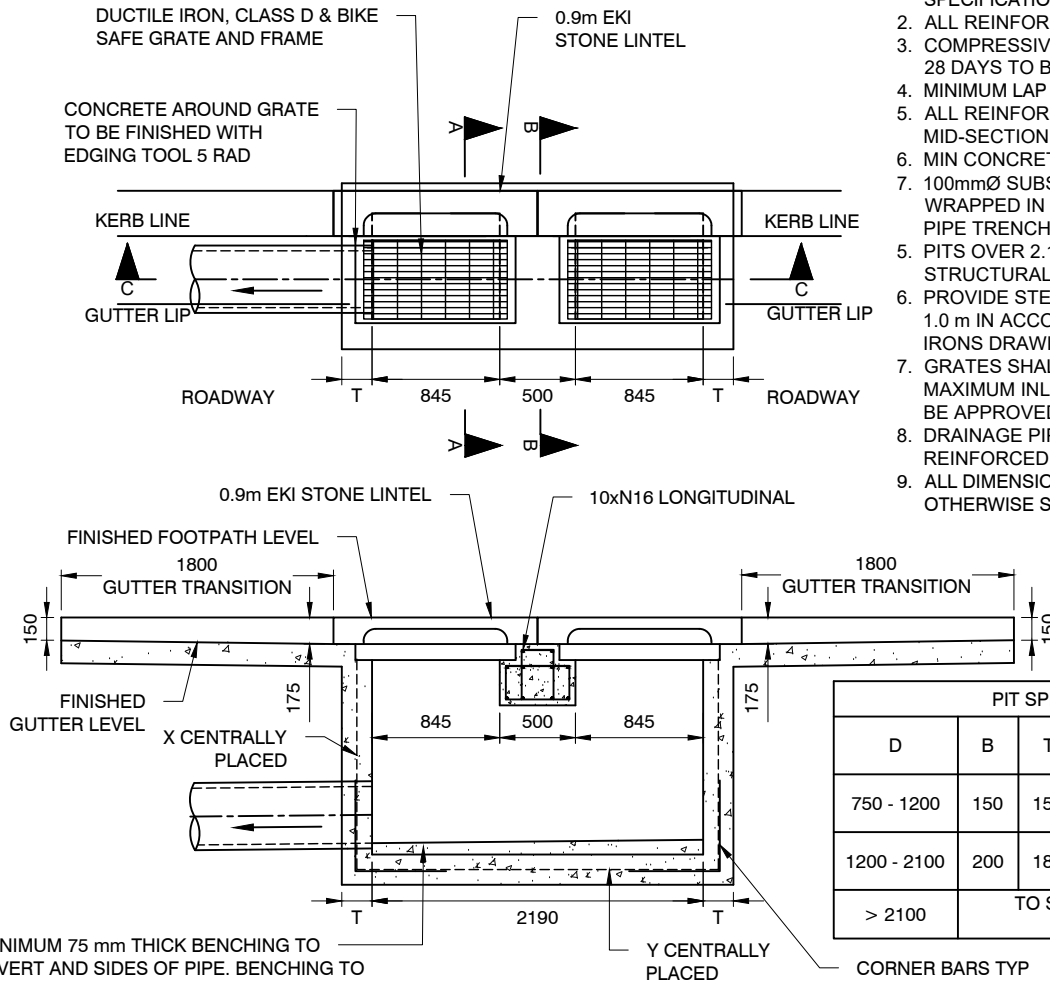


NOTES:

1. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
2. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
3. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
4. PITS OVER 2.1m IN DEPTH TO BE DESIGNED BY STRUCTURAL ENGINEER.
5. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
6. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
7. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:50

PLAN



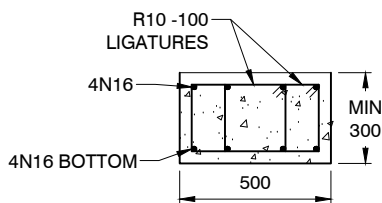
NOTES:

1. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
2. ALL REINFORCEMENT TO BE GRADE 500
3. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
4. MINIMUM LAP LENGTH IS TO BE 40 x BAR Ø UNO.
5. ALL REINFORCEMENT SHALL BE PLACED IN MID-SECTION UNO.
6. MIN CONCRETE BEAM COVER SHALL BE 40mm
7. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
5. PITS OVER 2.1m IN DEPTH TO BE DESIGNED BY STRUCTURAL ENGINEER.
6. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
7. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
8. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
9. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

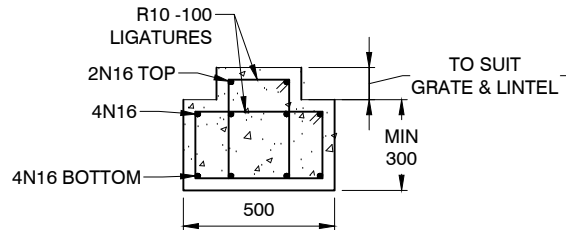
PIT SPECIFICATIONS

D	B	T	X	Y	CORNER BARS
750 - 1200	150	150	SL82	SL81	N12-300 L 500
1200 - 2100	200	180	SL81	SL81	N12-200 L 600
> 2100	TO STRUCTURAL ENGINEERS SPECIFICATION				

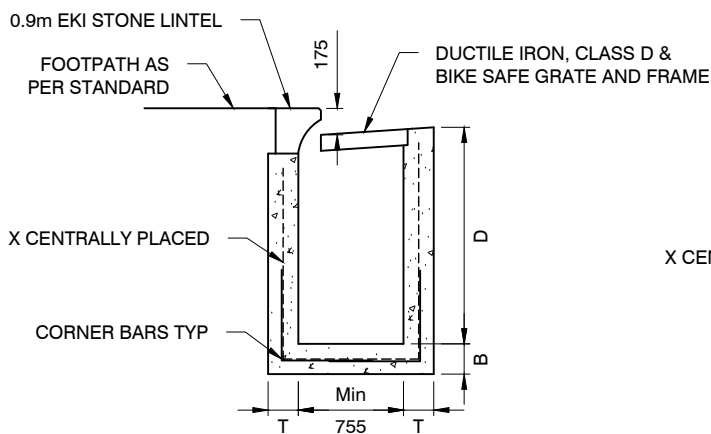
SECTION C-C



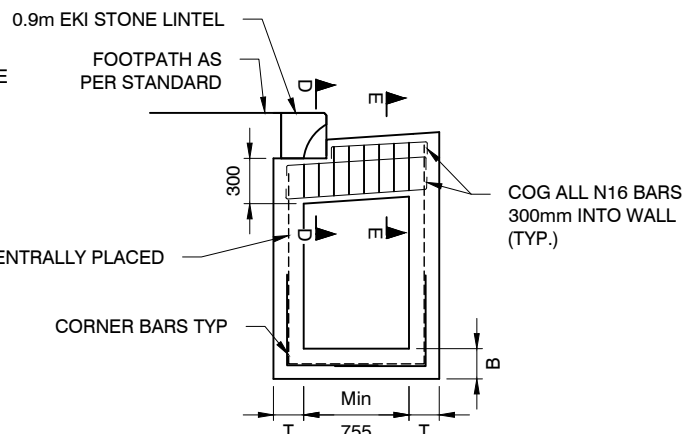
SECTION D-D



SECTION E-E



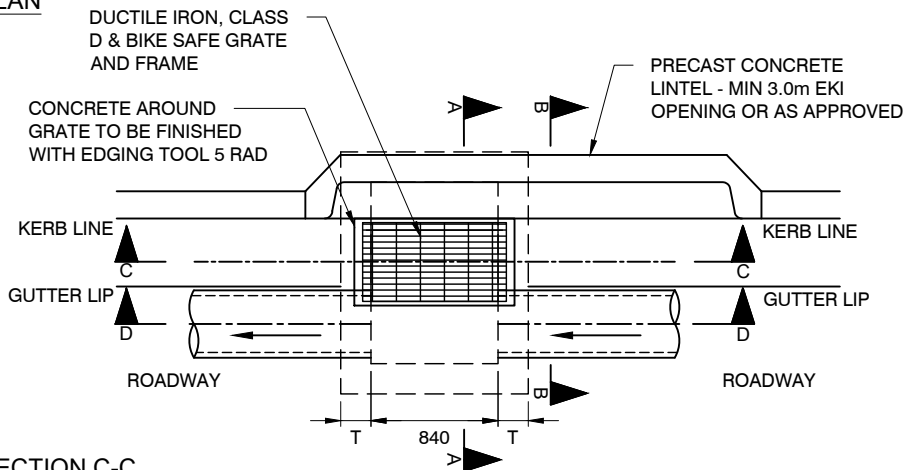
SECTION A-A



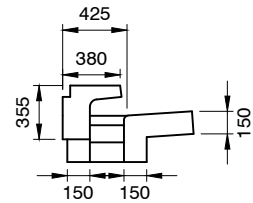
SECTION B-B

SCALE 1:50

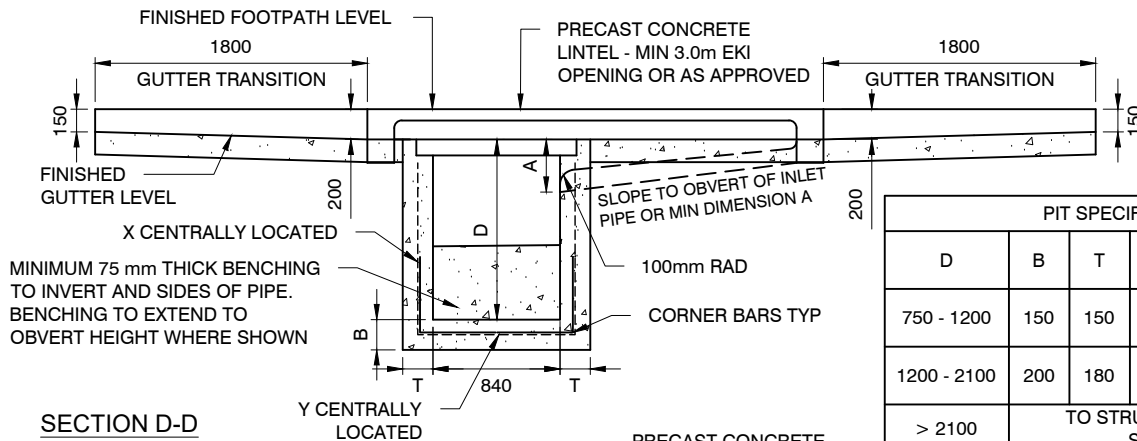
PLAN



SECTION B-B

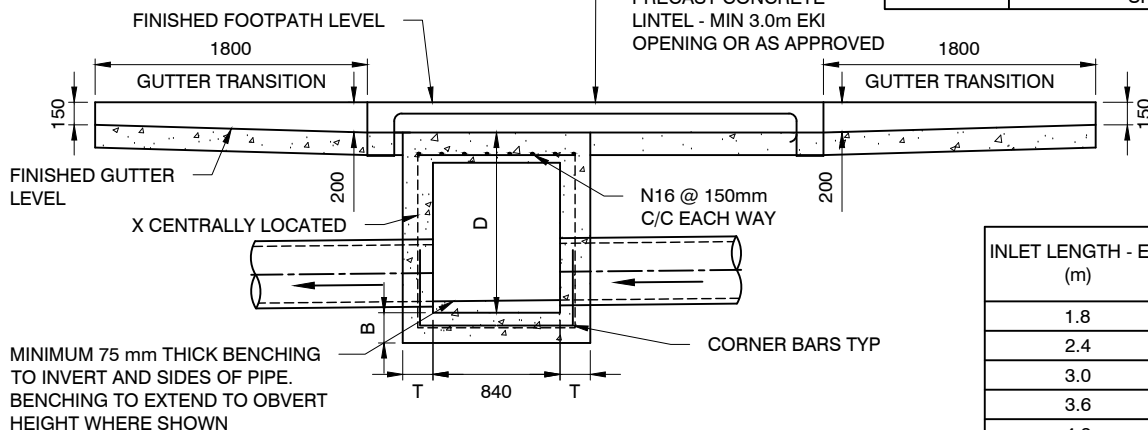


SECTION C-C



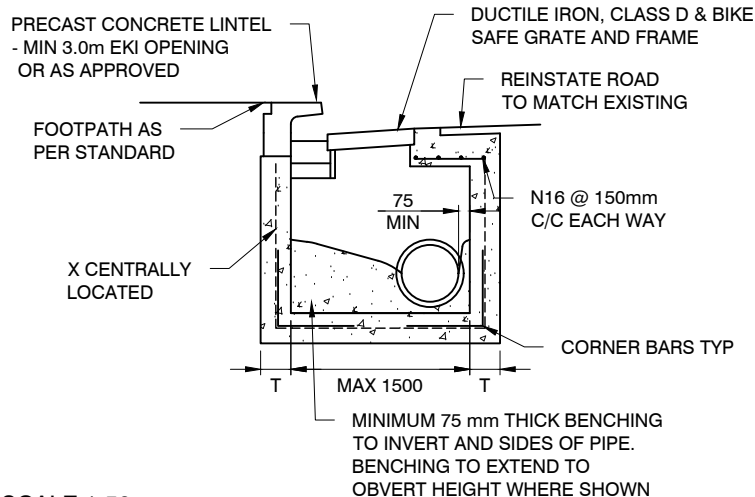
PIT SPECIFICATIONS					
D	B	T	X	Y	CORNER BARS
750 - 1200	150	150	SL82	SL81	N12-300 L 500
1200 - 2100	200	180	SL81	SL81	N12-200 L 600
> 2100	TO STRUCTURAL ENGINEERS SPECIFICATION				

SECTION D-D



INLET LENGTH - EKI (m)	MINIMUM DIMENSION A (mm)
1.8	250
2.4	300
3.0	400
3.6	450
4.2	500

SECTION A-A

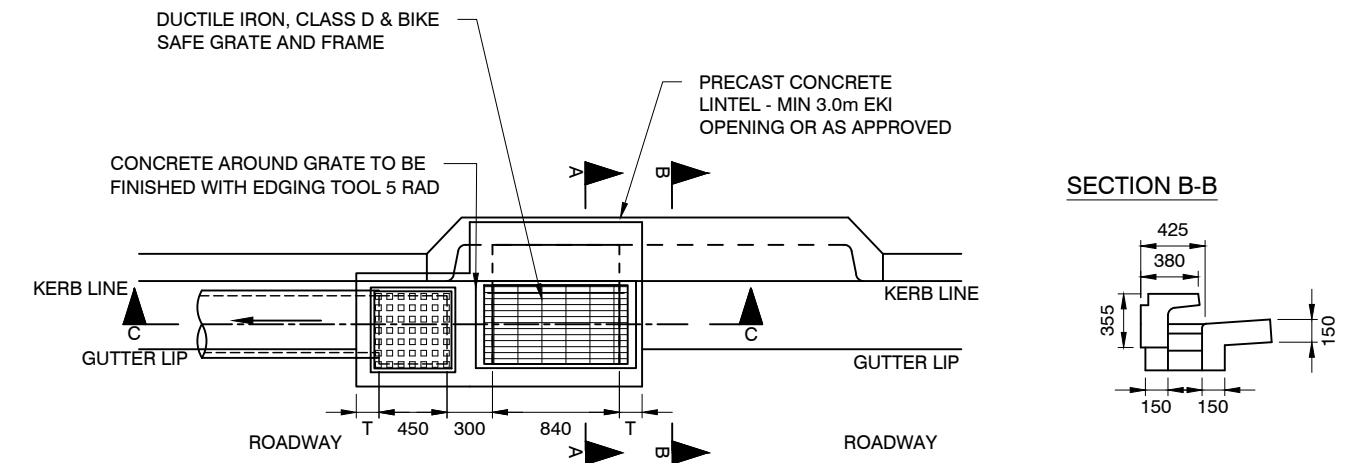


NOTES:

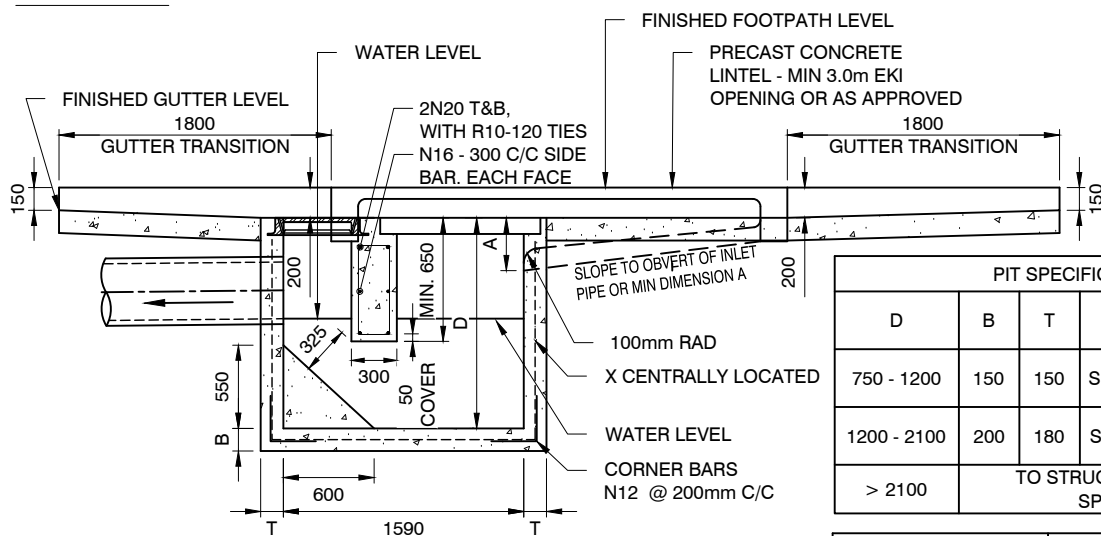
1. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
2. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
3. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
4. PITS OVER 2.1m IN DEPTH TO BE DESIGNED BY STRUCTURAL ENGINEER.
5. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
6. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
7. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:50

PLAN



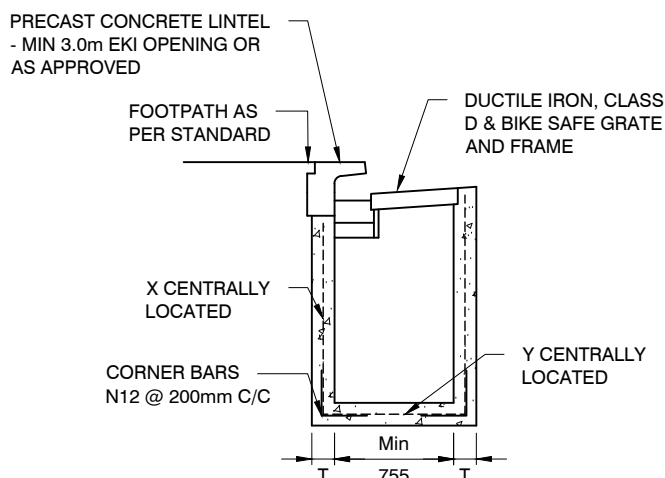
SECTION C-C



PIT SPECIFICATIONS					
D	B	T	X	Y	CORNER BARS
750 - 1200	150	150	SL82	SL81	N12-300 L 500
1200 - 2100	200	180	SL81	SL81	N12-200 L 600
> 2100	TO STRUCTURAL ENGINEERS SPECIFICATION				

INLET LENGTH - EKI (m)	MINIMUM DIMENSION A (mm)
1.8	250
2.4	300
3.0	400
3.6	450
4.2	500

SECTION A-A

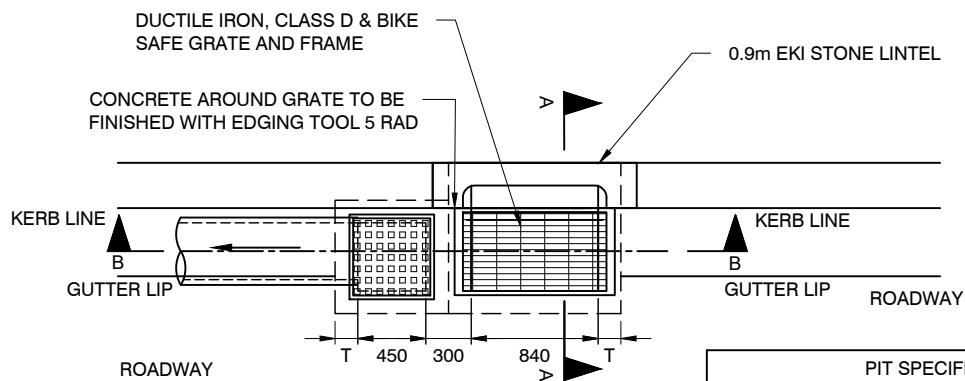


SCALE 1:50

NOTES:

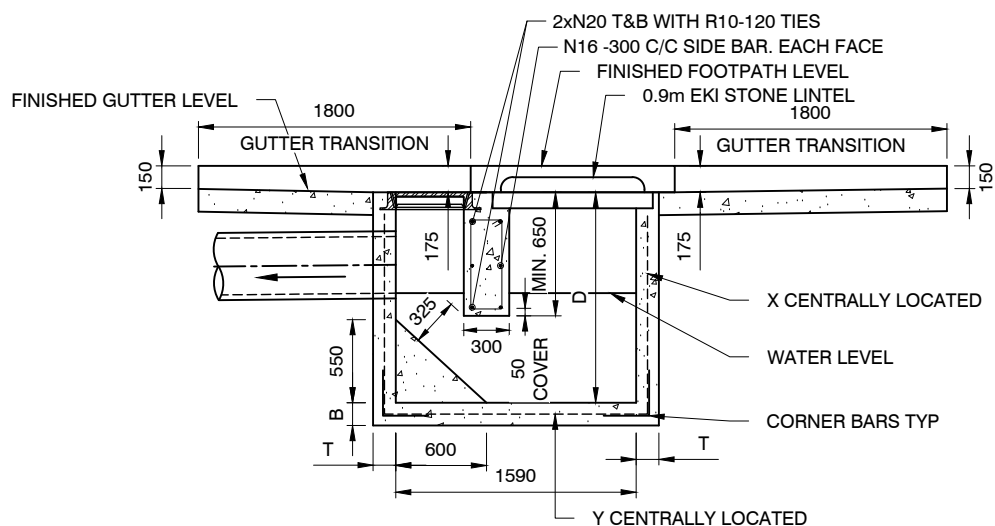
1. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
2. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
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5. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
6. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
7. MINIMUM LAP LENGTH IS TO BE 40 x BAR Ø UNLESS NOTED OTHERWISE.
8. DRAINAGE PIPE TO BE MINIMUM Ø375 CLASS 4 REINFORCED CONCRETE PIPE.
9. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

PLAN

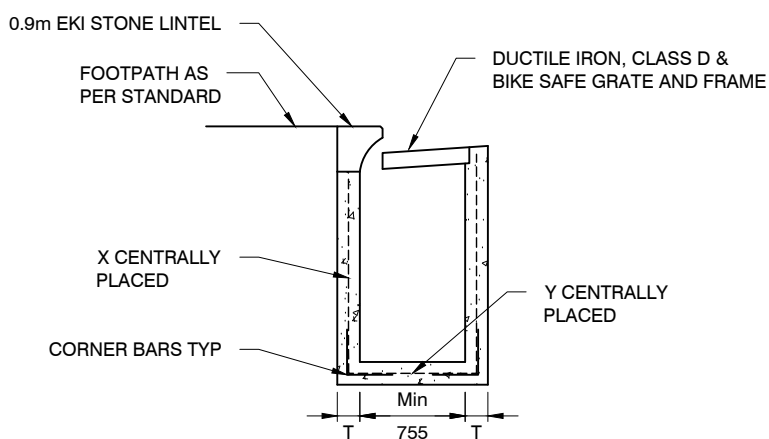


PIT SPECIFICATIONS					
D	B	T	X	Y	CORNER BARS
750 - 1200	150	150	SL82	SL81	N12-300 L 500
1200 - 2100	200	180	SL81	SL81	N12-200 L 600
> 2100	TO STRUCTURAL ENGINEERS SPECIFICATION				

SECTION B-B



SECTION A-A

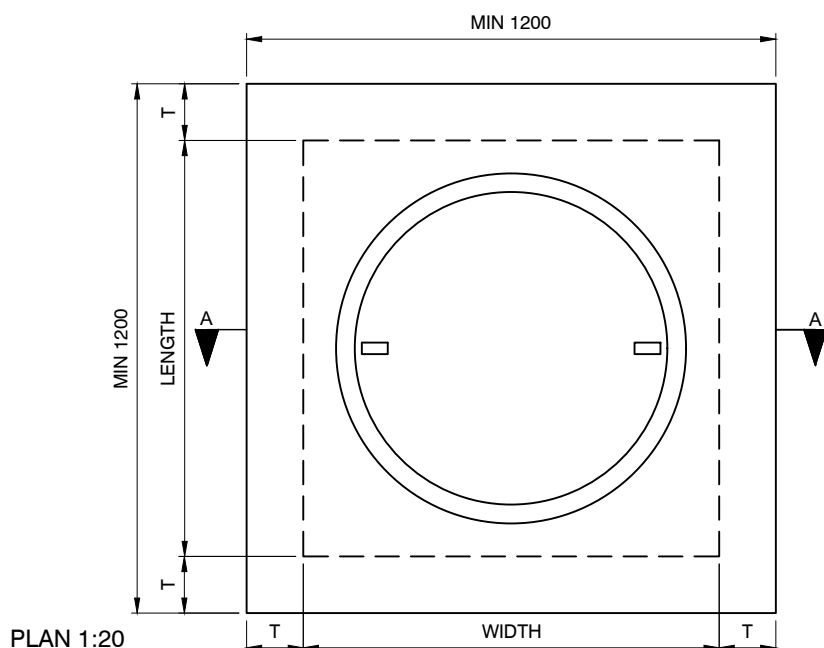


SCALE 1:50

NOTES:

1. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
2. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
3. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
4. PITS OVER 2.1m IN DEPTH TO BE DESIGNED BY STRUCTURAL ENGINEER.
5. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
6. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
7. DRAINAGE PIPE TO MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

MANHOLE COVER AND FRAME



PIT LID IS A CLASS D DUCTILE IRON HINGED LID

PIT SPECIFICATIONS					
D	B	T	X	Y	CORNER BARS
750 - 1200	150	150	SL82	SL81	N12-300 L 500
1200 - 2100	200	180	SL81	SL81	N12-200 L 600
> 2100	TO STRUCTURAL ENGINEERS SPECIFICATION				

SECTION A-A

VARIABLE HEIGHT TO ALLOW FOR ADJUSTMENT OF MANHOLE COVER TO SUIT DESIGN SURFACE LEVELS AND GRADING. LID MAY BE RECESSED INTO THE CONVERTER SLAB AS REQUIRED. ADJUST NUMBER OF RISERS AS REQUIRED.

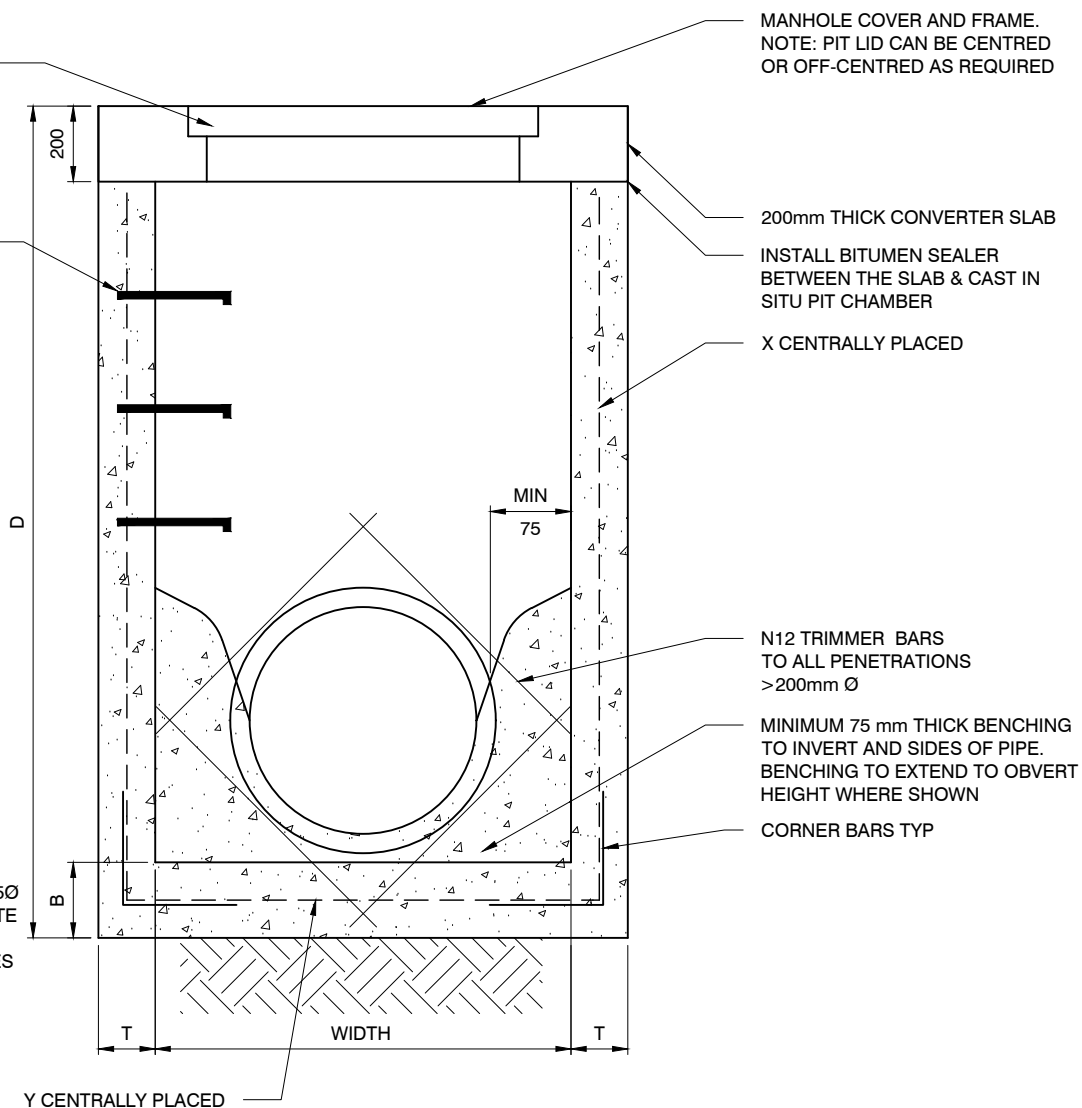
STEP IRONS REQUIRED @300mm SPACINGS IF DEPTH OF PIT IS GREATER THAN 1.0m AS PER STANDARD DETAIL

MINIMUM DIMENSIONS OF PIT (mm)		
Ø OF OUTLET ON STRAIGHT	WIDTH	LENGTH
UP TO 750	900	900
825 TO 900	1200	1200
1050	1200	1200
1200	1350	1200
1350	1500	1200
1500	1650	1200
1650	1800	1200
1800	1950	1200

NOTES:

- ALL CONCRETE IS TO HAVE MINIMUM STRENGTH OF 32MPa.
- DRAINAGE PIPE TO MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
- ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

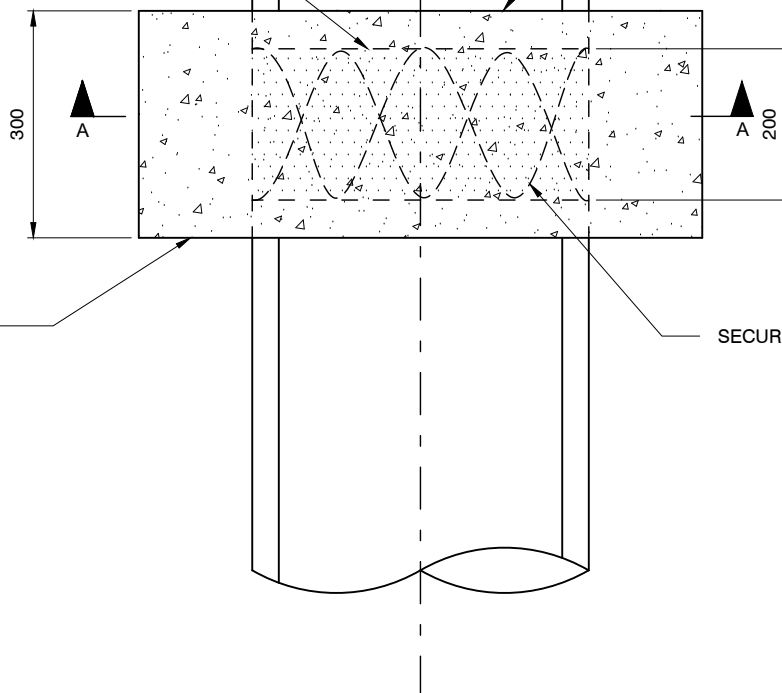
SECTION 1:20



PLAN

WRAP FLASHTAC TAPE WITH 100mm
OVERLAY ON EACH SIDE OF JOINT

CLEAN PIPE SURFACES PRIOR TO
CONSTRUCTING THE BANDAGE JOINT



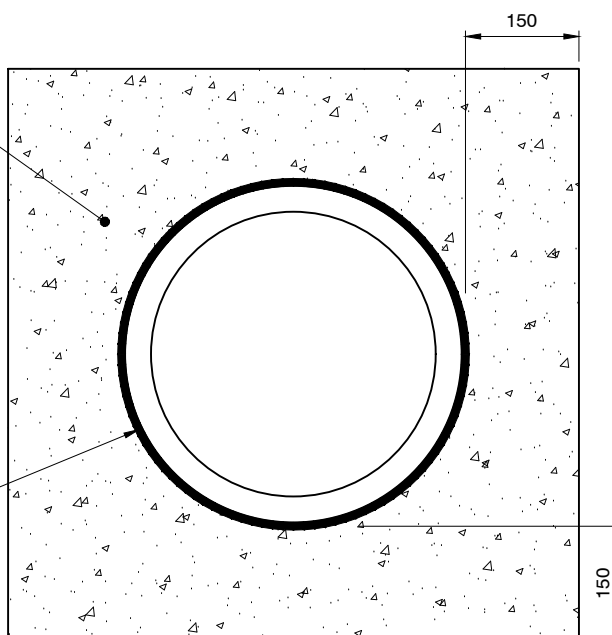
ENCASE WITH MASS CONCRETE
150mm THICK WITH 50mm OVERLAY
ON EACH SIDE OF TAPE

SECURE TAPE WITH WIRE

SECTION A-A

MASS CONCRETE ENCASEMENT

FLASHTAC TAPE



SCALE 1:10

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

CONCRETE PIPE TRENCH BACKFILL DETAIL

≤ 900mm DIA.

EXISTING SURFACE LEVEL

FOOTPATH/ROAD TO SUIT
(REFER TO PAVEMENT DETAILS)

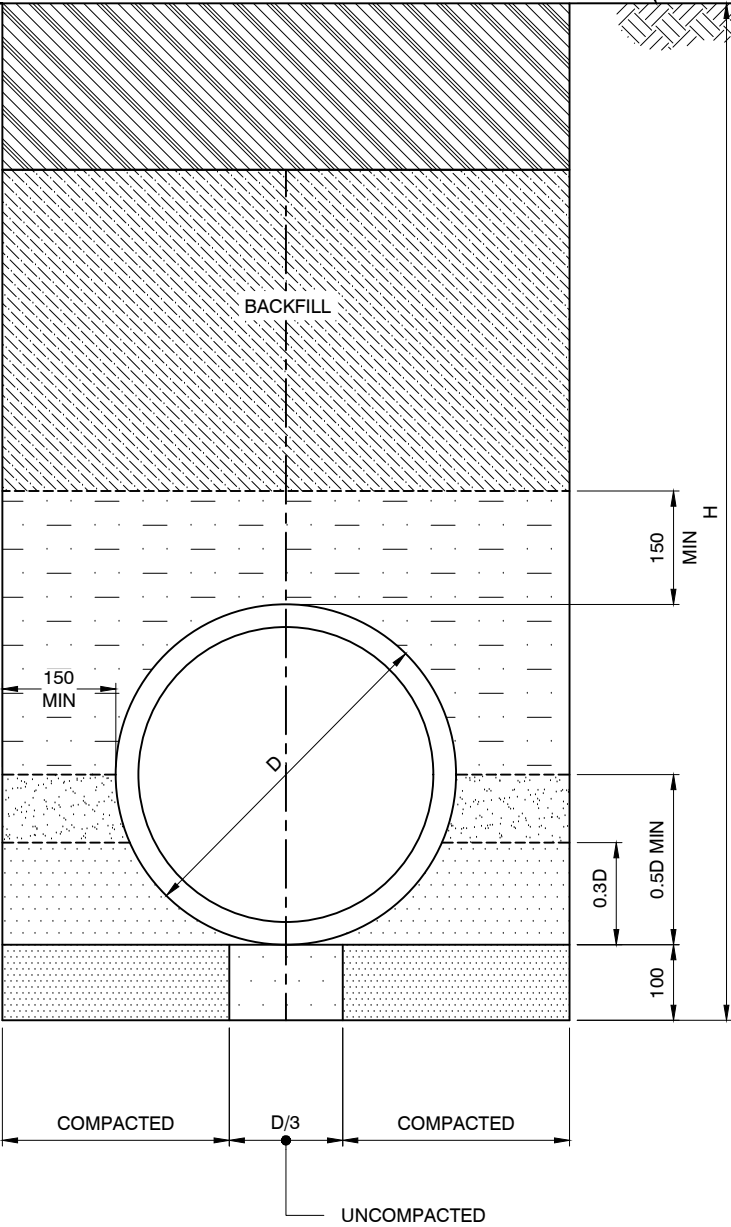
BACKFILL ZONE

OVERLAY ZONE

SIDE ZONES

HAUNCH ZONES

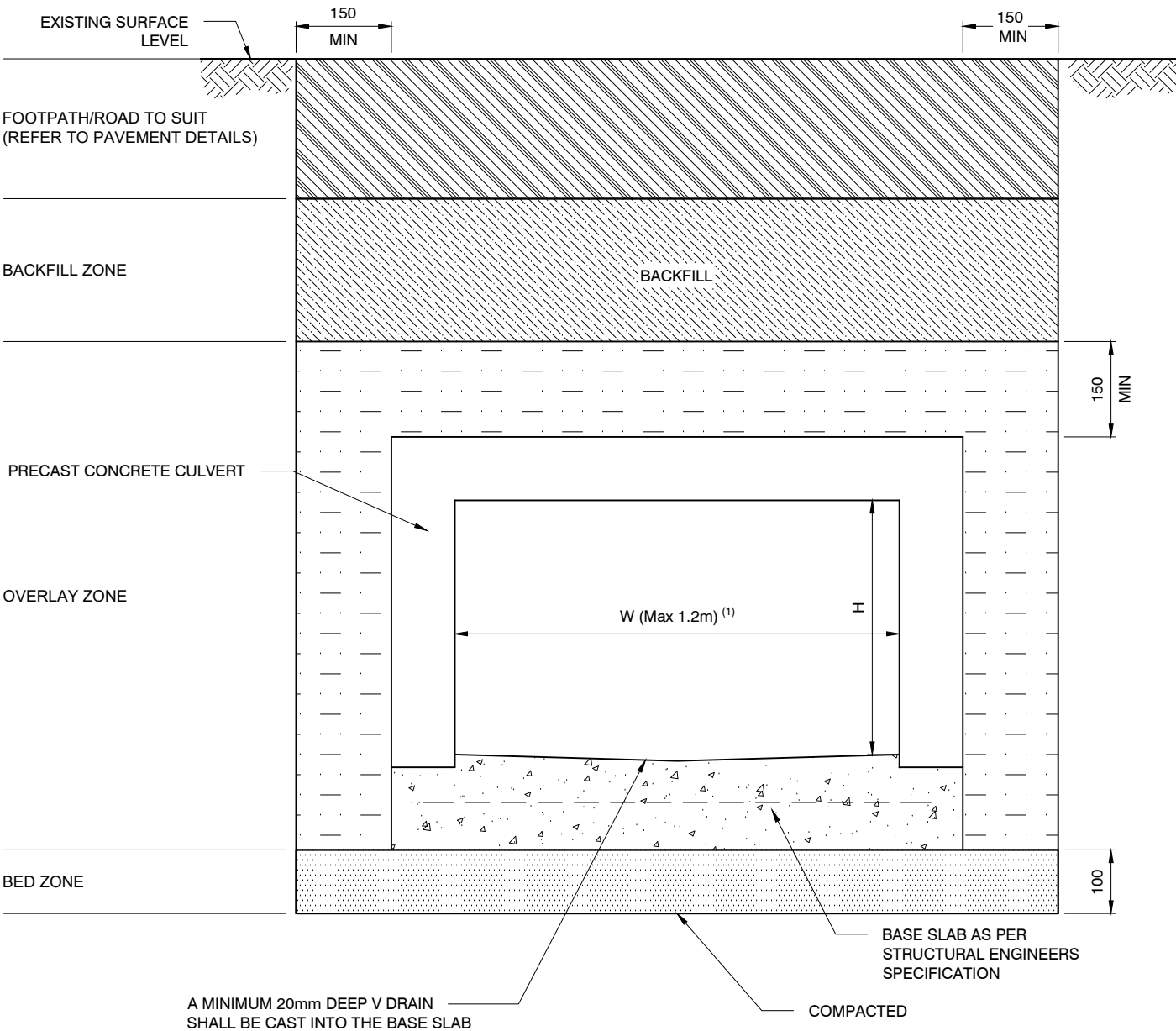
BED ZONE



SECTION 1:10

- NOTES:
1. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
 2. TRENCH BACKFILL DETAIL FOR DRAINAGE PIPE GREATER THAN 900 mm DIA SHALL BE REVIEWED AND APPROVED BY COUNCIL'S REPRESENTATIVE.
 3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

CONCRETE CULVERT TRENCH BACKFILL DETAIL



SECTION 1:10

NOTES:

1. CULVERTS WITH WIDTHS GREATER THAN 1.2 m SHALL BE CONSTRUCTED AS PER THE STRUCTURAL ENGINEER'S SPECIFICATIONS.
2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

MANHOLE COVER AND FRAME



1. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
2. ALL REINFORCEMENT TO BE GRADE 500
3. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
4. ALL REINFORCEMENT SHALL BE PLACED IN MID-SECTION UNO. OTHERWISE MIN CONCRETE COVER SHALL BE 40mm
5. FOR ANY PENETRATION THROUGH WALLS AND SLABS GREATER THAN 200 SPACING, PROVIDE N12 TRIMMER BARS AND ADDITIONAL N12 REPLACEMENT BARS ON EACH SIDE.
6. LAP LENGTH IS TO BE MINIMUM 40 x BAR Ø UNLESS NOTED OTHERWISE.
7. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
8. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
9. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
10. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
11. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SECTION A-A

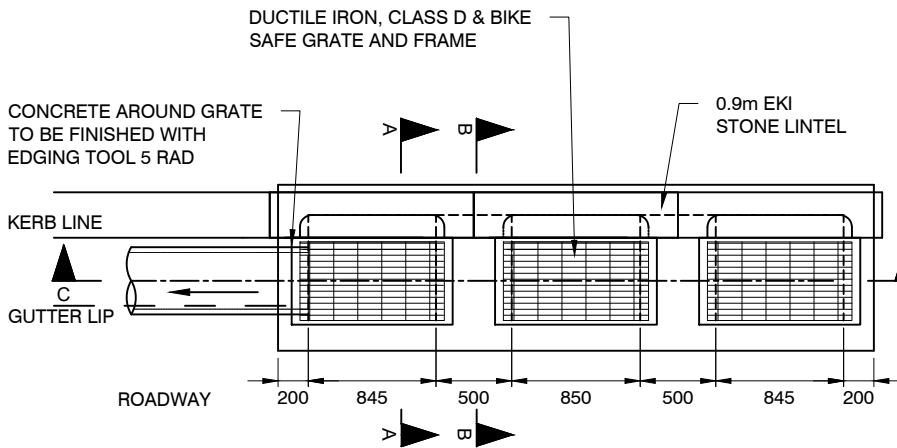


D	B	T	REINFORCEMENT
750 - 1300	200	150	SL82
1300 - 2400	250	200	SL102
>2400	AS PER STRUCTURAL ENGINEERS SPECIFICATION		

MINIMUM DIMENSIONS OF PIT (mm)		
Ø OF OUTLET ON STRAIGHT	WIDTH	LENGTH
UP TO 750	900	900
825 TO 900	1200	1200
1050	1200	1200
1200	1350	1200
1350	1500	1200
1500	1650	1200
1650	1800	1200
1800	1950	1200

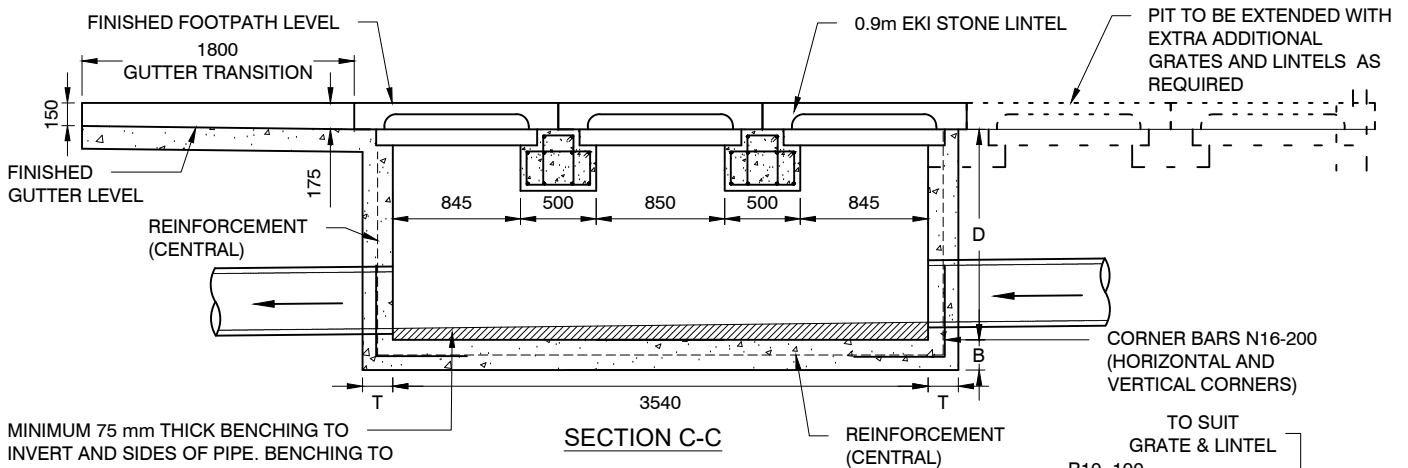
NOTES:

1. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
2. ALL REINFORCEMENT TO BE GRADE 500
3. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
4. MINIMUM LAP LENGTH IS TO BE 40 x BAR Ø UNLESS NOTED OTHERWISE.
5. ALL REINFORCEMENT SHALL BE PLACED IN MID-SECTION UNO.
6. MIN CONCRETE BEAM COVER SHALL BE 40mm
7. FOR ANY PENETRATION THROUGH WALLS AND SLABS GREATER THAN 200 SPACING, PROVIDE N12 TRIMMER BARS AND ADDITIONAL N12 REPLACEMENT BARS ON EACH SIDE.
8. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
9. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
10. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
11. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
12. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

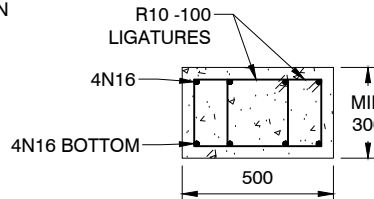


PLAN

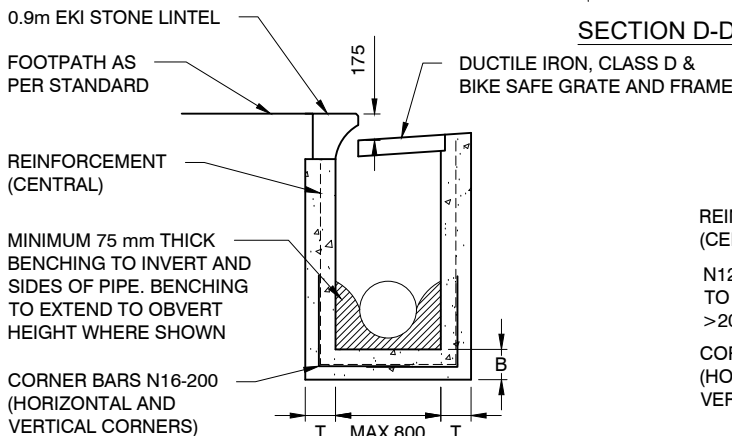
D	B	T	REINFORCEMENT
750 - 1300	200	150	SL82
1300 - 2400	230	200	SL102
>2400	AS PER STRUCTURAL ENGINEERS SPECIFICATION		



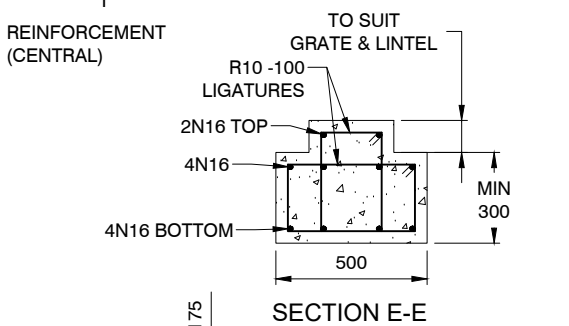
SECTION C-C



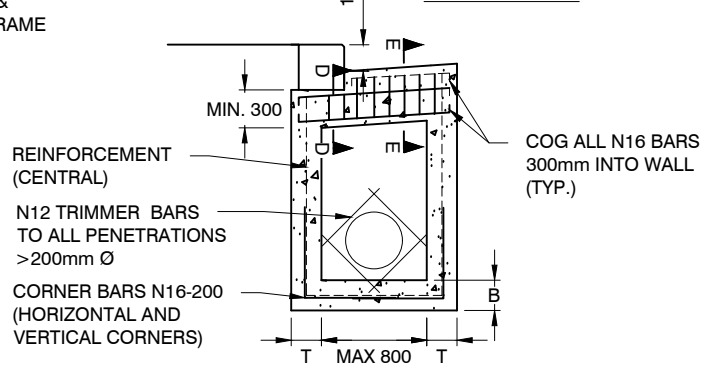
SECTION D-D



SECTION A-A



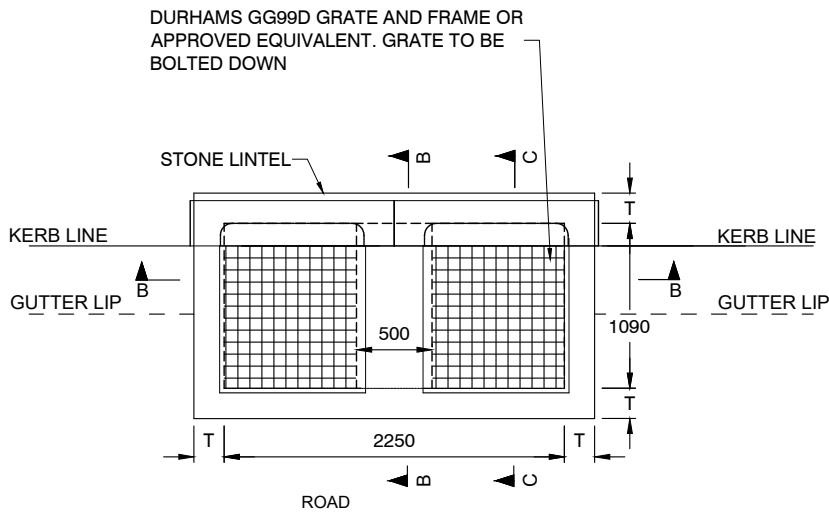
SECTION E-E



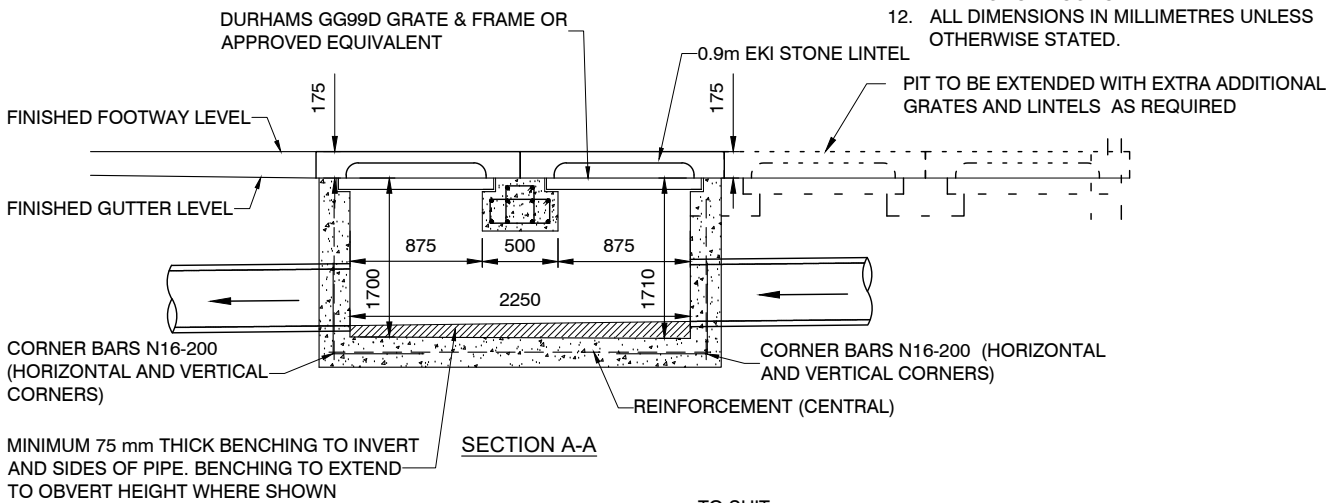
SECTION B-B

NOTES:

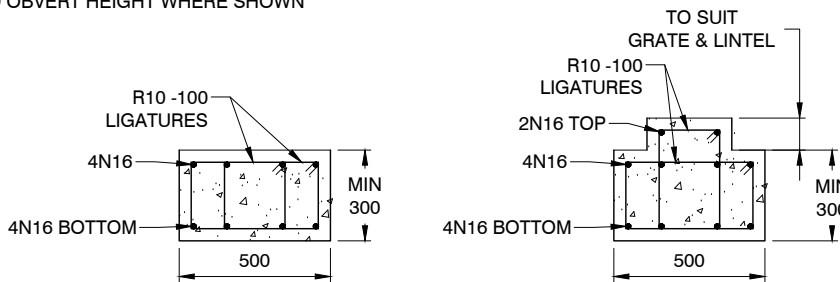
1. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
2. ALL REINFORCEMENT TO BE GRADE 500
3. ALL LIGATURES TO BE MIN R10.
4. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
5. ALL REINFORCEMENT SHALL BE PLACED IN MID-SECTION UNO. OTHERWISE MIN CONCRETE COVER SHALL BE 40mm - B1 COVER
6. FOR ANY PENETRATION THROUGH WALLS AND SLABS GREATER THAN 200 SPACING, PROVIDE N12 TRIMMER BARS AND ADDITIONAL N12 REPLACEMENT BARS ON EACH SIDE.
7. LAP LENGTH IS TO BE MINIMUM $40 \times \text{BAR } \varnothing$ UNLESS NOTED OTHERWISE.
8. 100mm \varnothing SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
9. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
10. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
11. DRAINAGE PIPE TO BE MINIMUM 375 \varnothing CLASS 4 REINFORCED CONCRETE PIPE.
12. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



PLAN 1:50



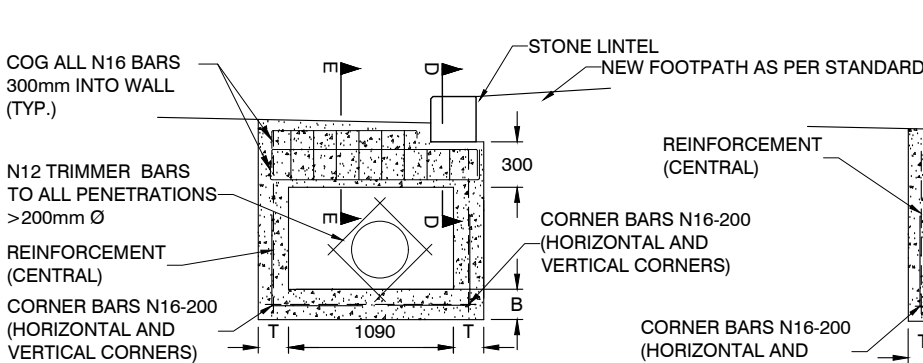
SECTION A-A



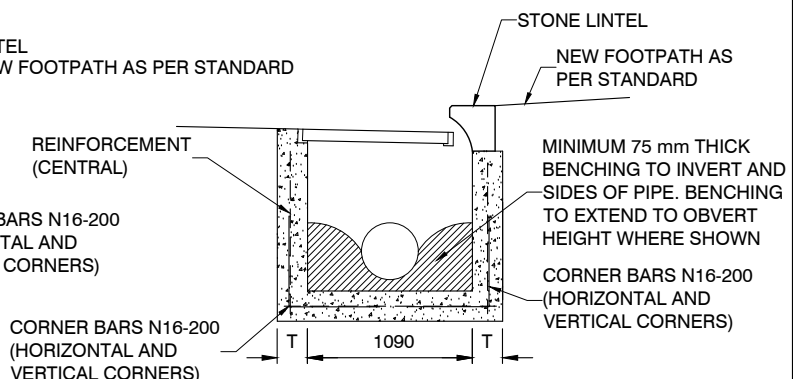
SECTION D-D

SECTION E-E

H	B	T	REINFORCEMENT
750 - 1300	200	150	SL82
1300 - 2400	250	200	SL102
>2400	AS PER STRUCTURAL ENGINEERS SPECIFICATION		

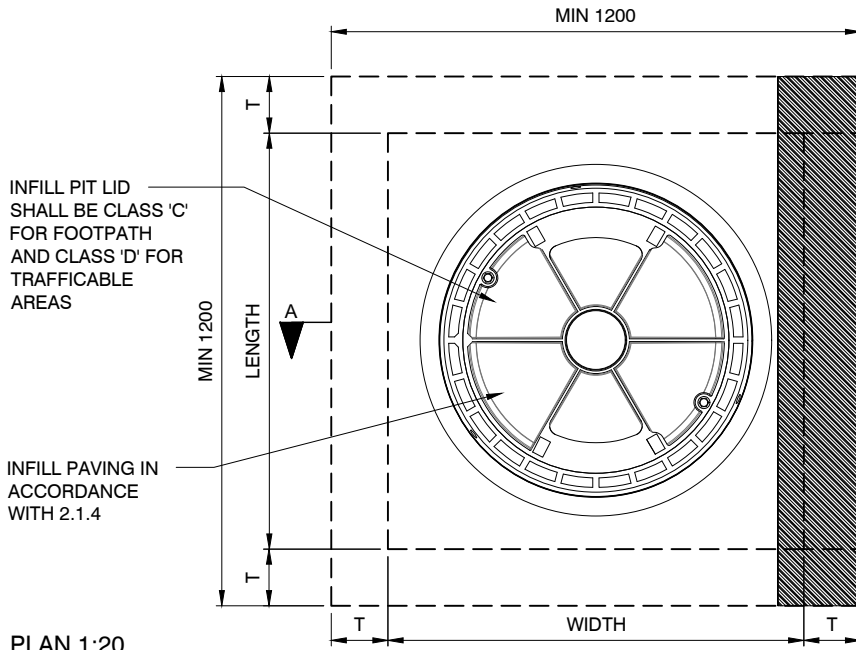


SECTION B-B



SECTION C-C

MANHOLE COVER AND FRAME

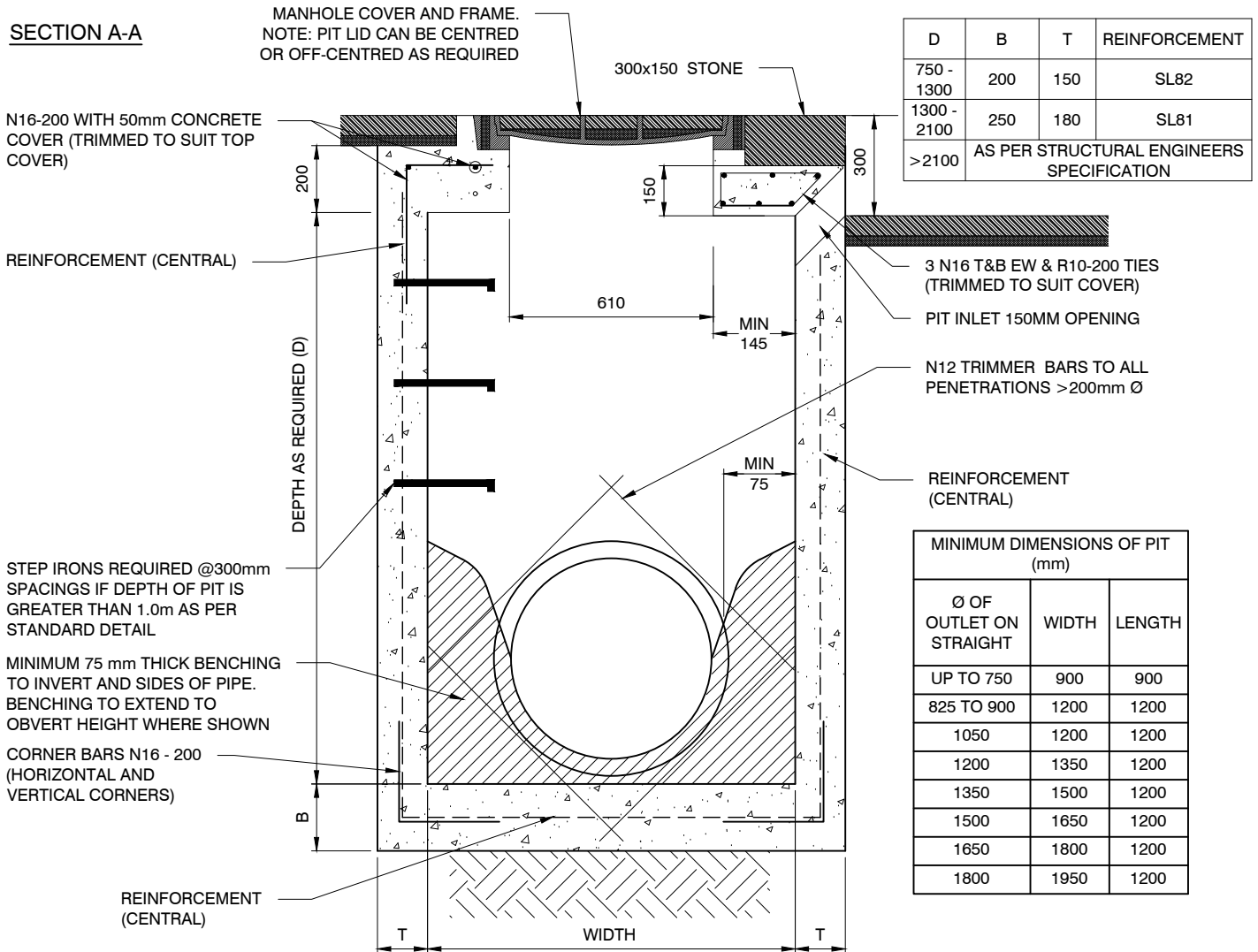


PLAN 1:20

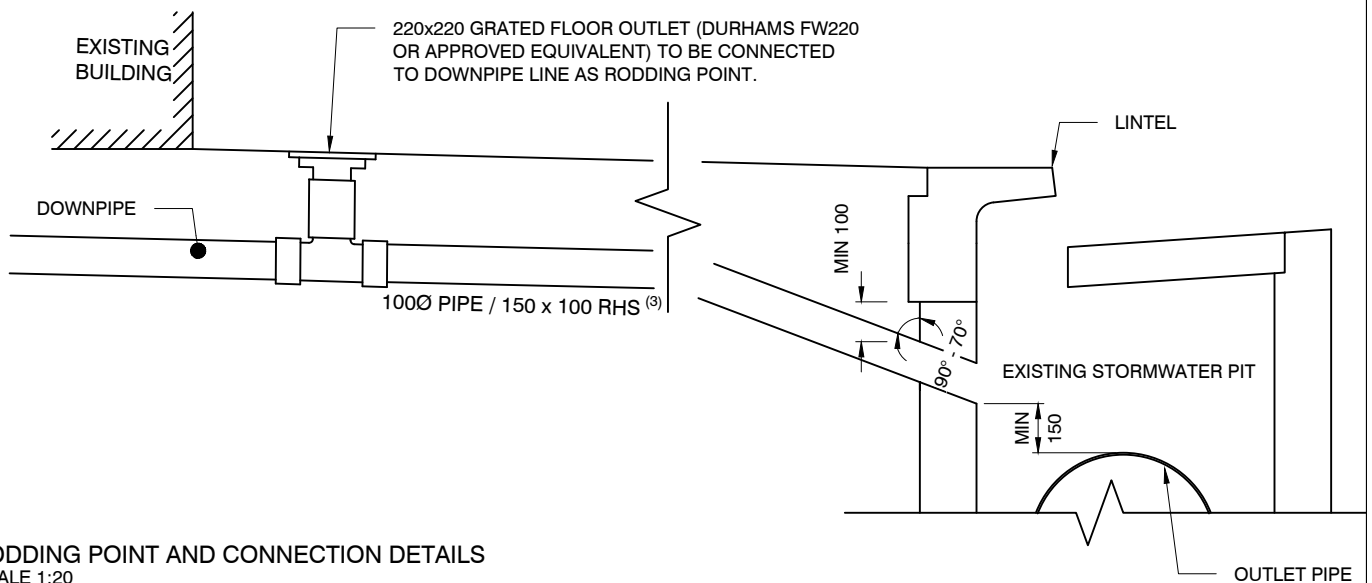
NOTES:

1. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
2. ALL REINFORCEMENT TO BE GRADE 500
3. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
4. ALL REINFORCEMENT SHALL BE PLACED IN MID-SECTION UNO. OTHERWISE MIN CONCRETE COVER SHALL BE 40mm
5. FOR ANY PENETRATION THROUGH WALLS AND SLABS GREATER THAN 200 SPACING, PROVIDE N12 TRIMMER BARS AND ADDITIONAL N12 REPLACEMENT BARS ON EACH SIDE.
6. LAP LENGTH IS TO BE MINIMUM 40 x BAR Ø UNLESS NOTED OTHERWISE.
7. 100mmØ SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
8. PROVIDE STEP IRONS FOR PITS DEEPER THAN 1.0 m IN ACCORDANCE WITH STANDARD STEP IRONS DRAWING.
9. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
10. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE.
11. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

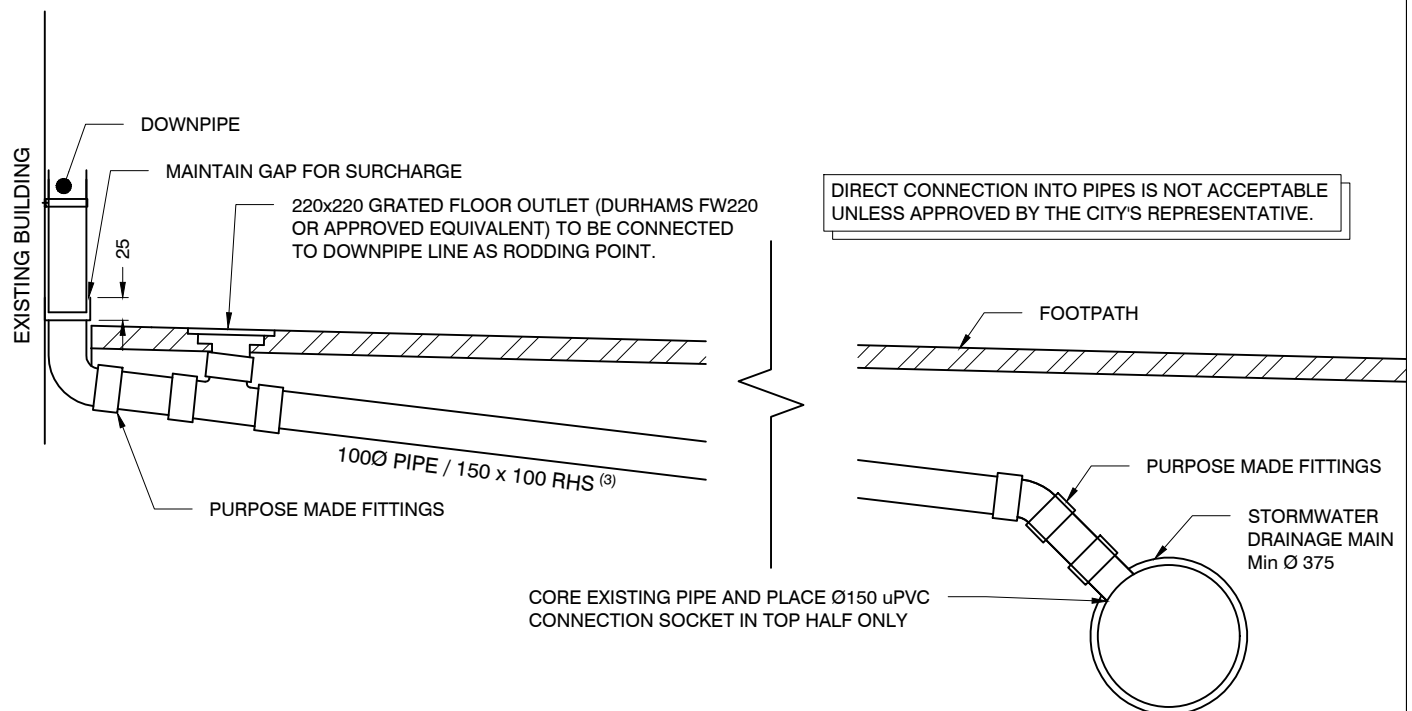
SECTION A-A



SECTION 1:20



RODDING POINT AND CONNECTION DETAILS
SCALE 1:20

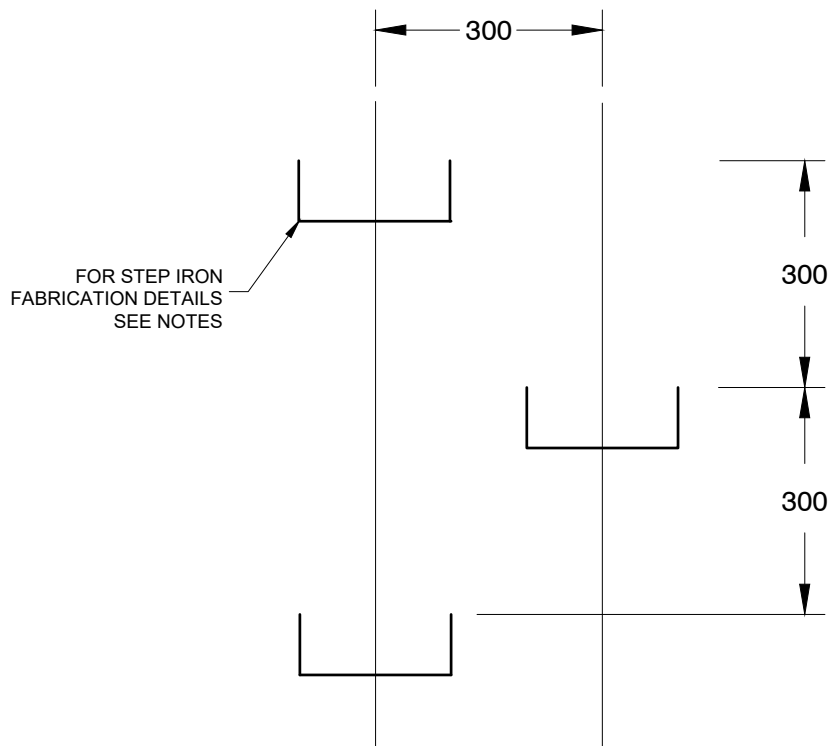


ALTERNATIVE RODDING POINT DETAILS
SCALE 1:20

ALTERNATIVE CONNECTION DETAILS (ONLY IF APPROVED)
SCALE 1:20

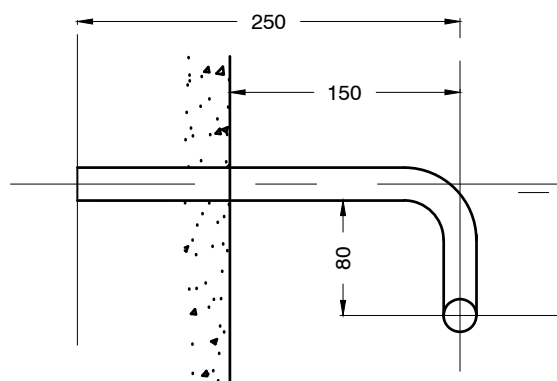
NOTES:

1. ALL CONNECTIONS SHALL BE CORE DRILLED AND SEALED WITH A NON-SHRINK GROUT.
2. PIPE SHALL COMPLY WITH TECHNICAL SPECIFICATIONS FOR DIRECT CONNECTIONS.
3. FOR DISCHARGE VIA A KERB OUTLET, WHERE FOOTPATH PAVERS ARE THICKER THAN 40 MM, 150 X 50 mm GALVANIZED RHS WITH 5 MM THICKNESS CAN BE USED. REFER TO STANDARD DRAWING 1.1.14.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



STEP IRON ARRANGEMENT

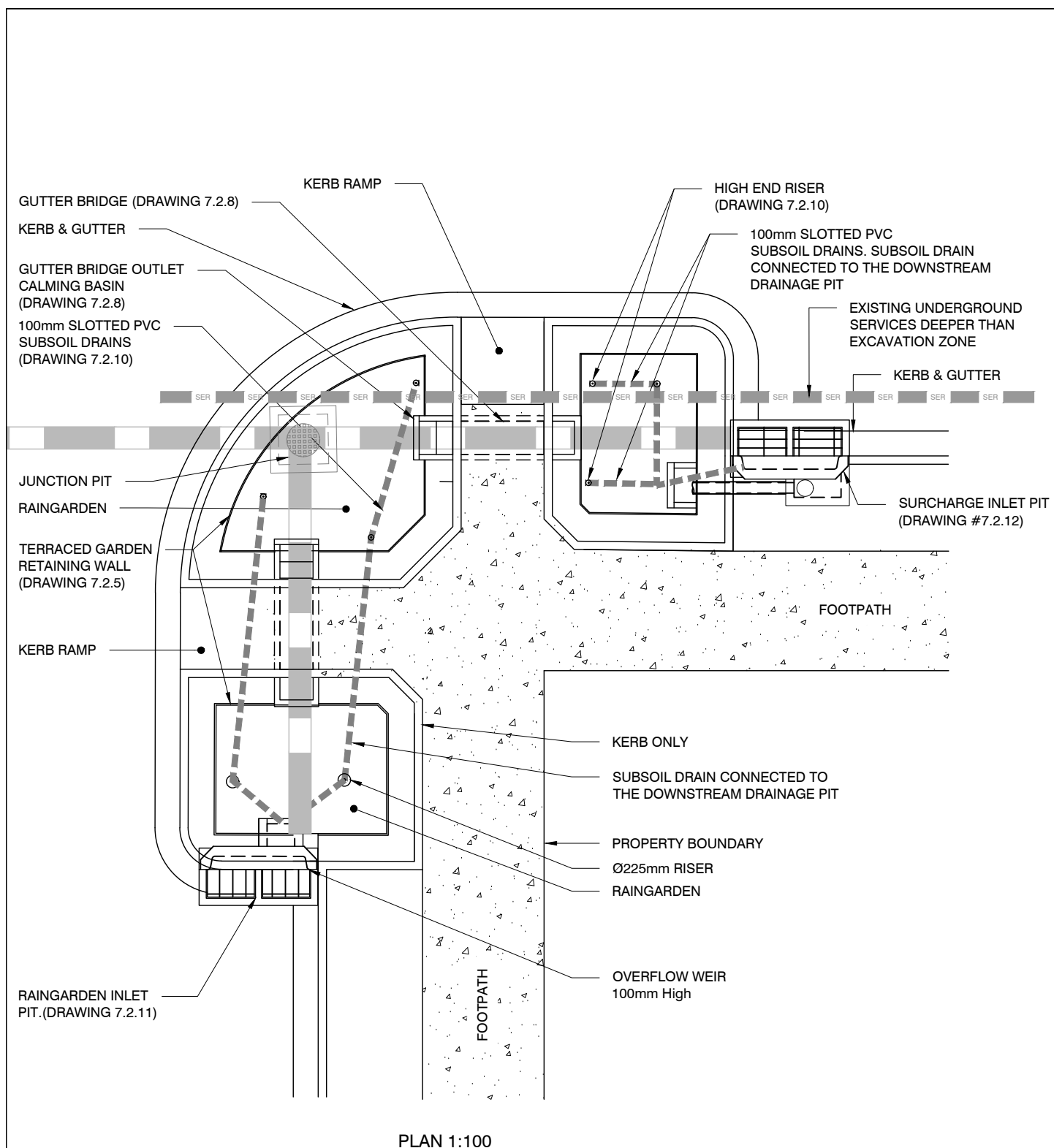
ELEVATION 1:10



SECTION 1:5

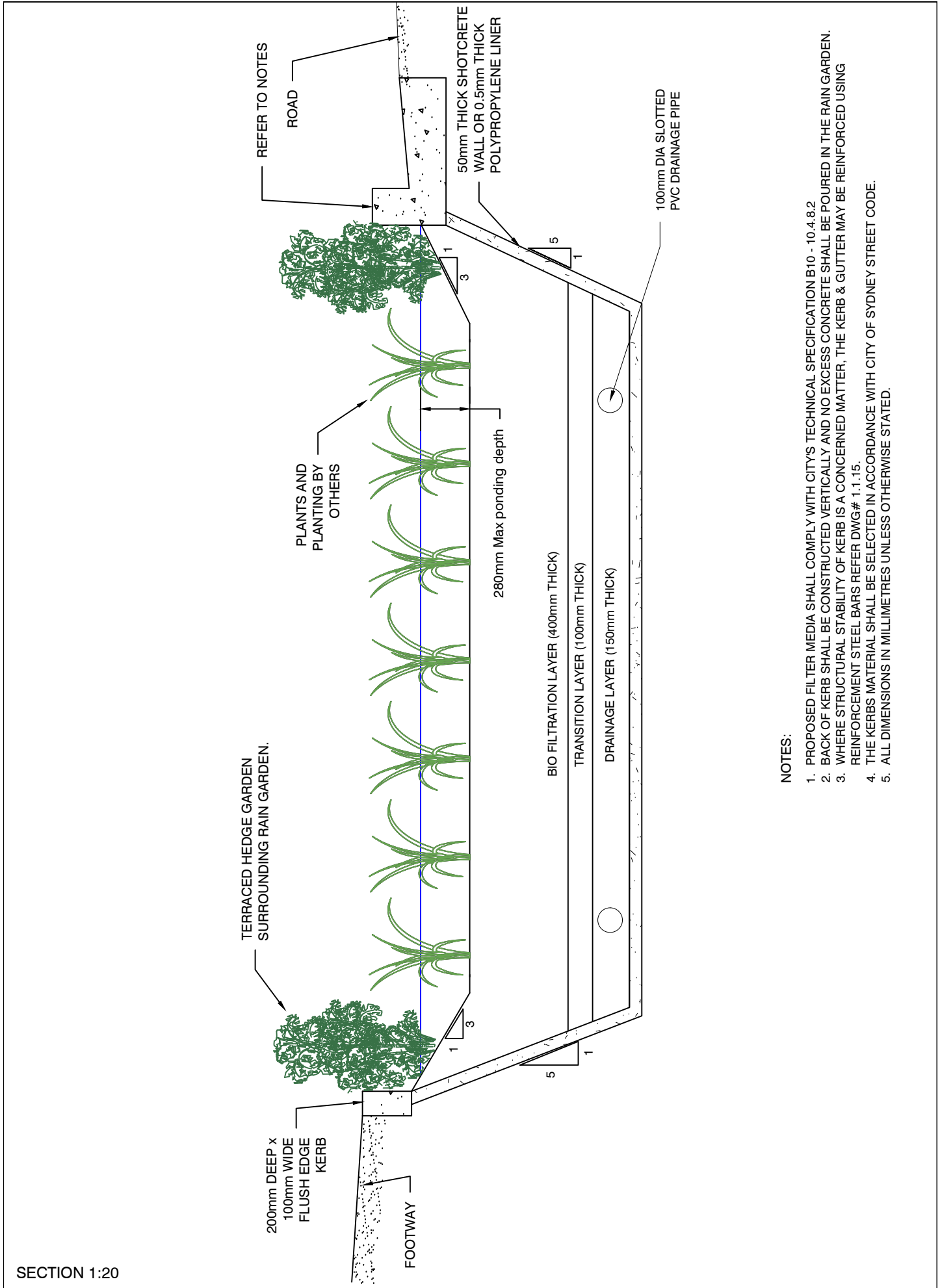
NOTES:

1. STEP IRONS MUST BE FABRICATED FROM 20mm Ø M.S.
2. ALL BENDS MUST BE FORMED AROUND 12mm diameter PIN.
3. STEP IRONS MUST BE HOT-DIPPED GALVANISED.
4. STEP IRONS MUST BE LOCATED:
 - (i) DIRECTLY BELOW THE OPENING OF THE COVER.
 - (ii) DESIRABLY ON A WALL WITHOUT PIPE OPENINGS.
 - (iii) DESIRABLY ON ONE OF THE LONG SIDES OF THE PIT.
8. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



NOTES:

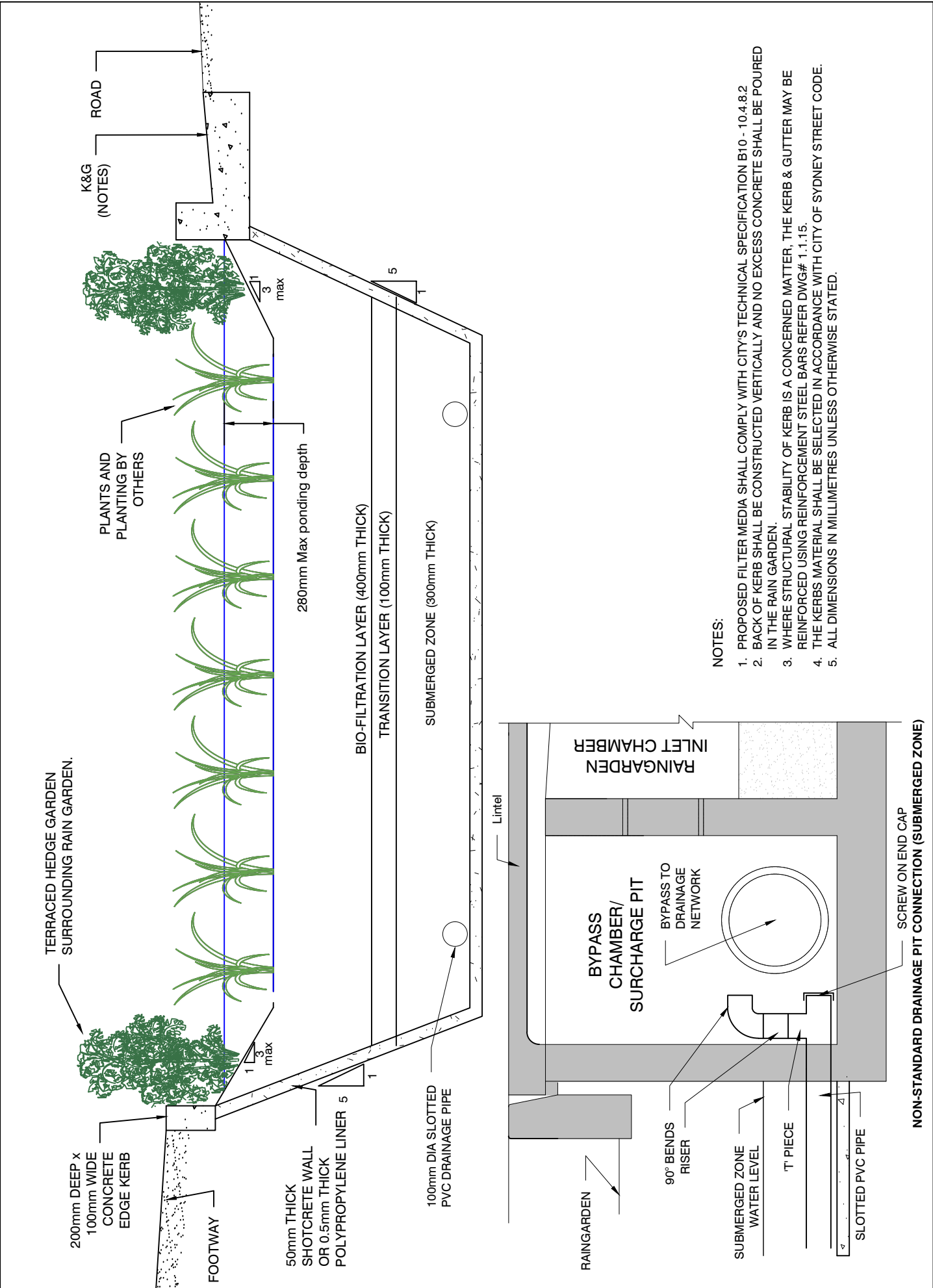
1. THE RAINGARDENS ARE PREFERRED TO BE TERRACED RAINGARDEN TO MAXIMISE THE PONDING VOLUME. REFER DRAWING 7.2.5.
2. THE RAINGARDEN & SURROUNDINGS AREAS SHALL BE DESIGNED IN ACCORDANCE WITH SYDNEY STREET TECHNICAL SPECIFICATION PART A4.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



NOTES:

- 1. PROPOSED FILTER MEDIA SHALL COMPLY WITH CITY'S TECHNICAL SPECIFICATION B10 - 10.4.8.2
- 2. BACK OF KERB SHALL BE CONSTRUCTED VERTICALLY AND NO EXCESS CONCRETE SHALL BE POURED IN THE RAIN GARDEN.
- 3. WHERE STRUCTURAL STABILITY OF KERB IS A CONCERNED MATTER, THE KERB & GUTTER MAY BE REINFORCED USING REINFORCEMENT STEEL BARS REFER DWG# 1.1.15.
- 4. THE KERBS MATERIAL SHALL BE SELECTED IN ACCORDANCE WITH CITY OF SYDNEY STREET CODE.
- 5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

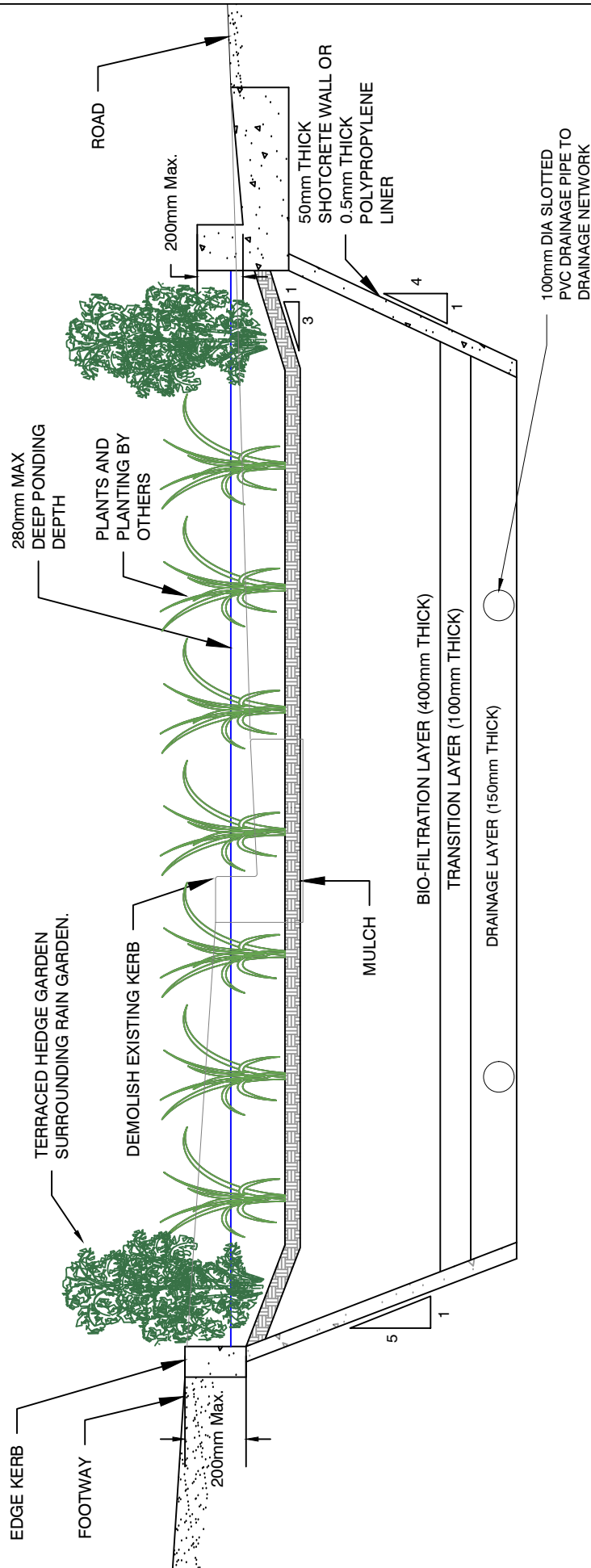
SECTION 1:20



NOTES:

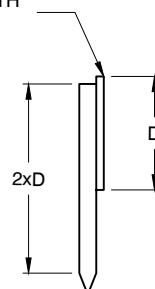
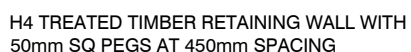
- 1. PROPOSED FILTER MEDIA SHALL COMPLY WITH CITY'S TECHNICAL SPECIFICATION B10 - 10.4.8.2
- 2. BACK OF KERB SHALL BE CONSTRUCTED VERTICALLY AND NO EXCESS CONCRETE SHALL BE POURED IN THE RAIN GARDEN.
- 3. WHERE STRUCTURAL STABILITY OF KERB IS A CONCERNED MATTER, THE KERB & GUTTER MAY BE REINFORCED USING REINFORCEMENT STEEL BARS REFER DWG# 1.1.15.
- 4. THE KERBS MATERIAL SHALL BE SELECTED IN ACCORDANCE WITH CITY OF SYDNEY STREET CODE.
- 5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

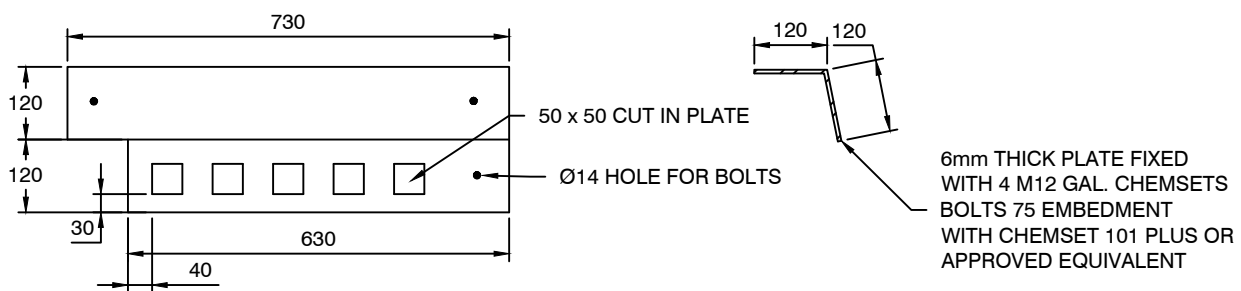
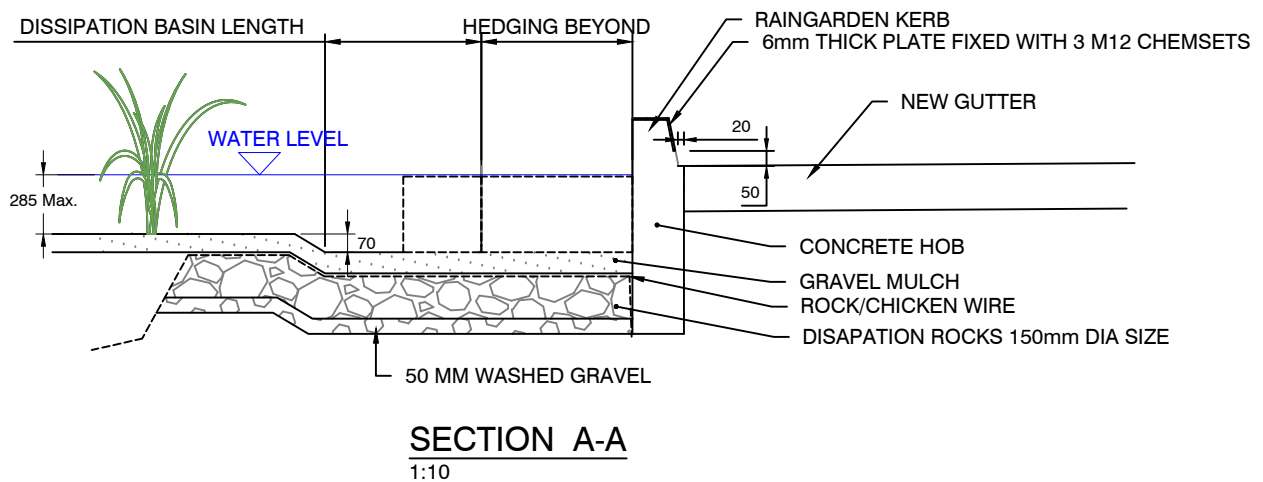
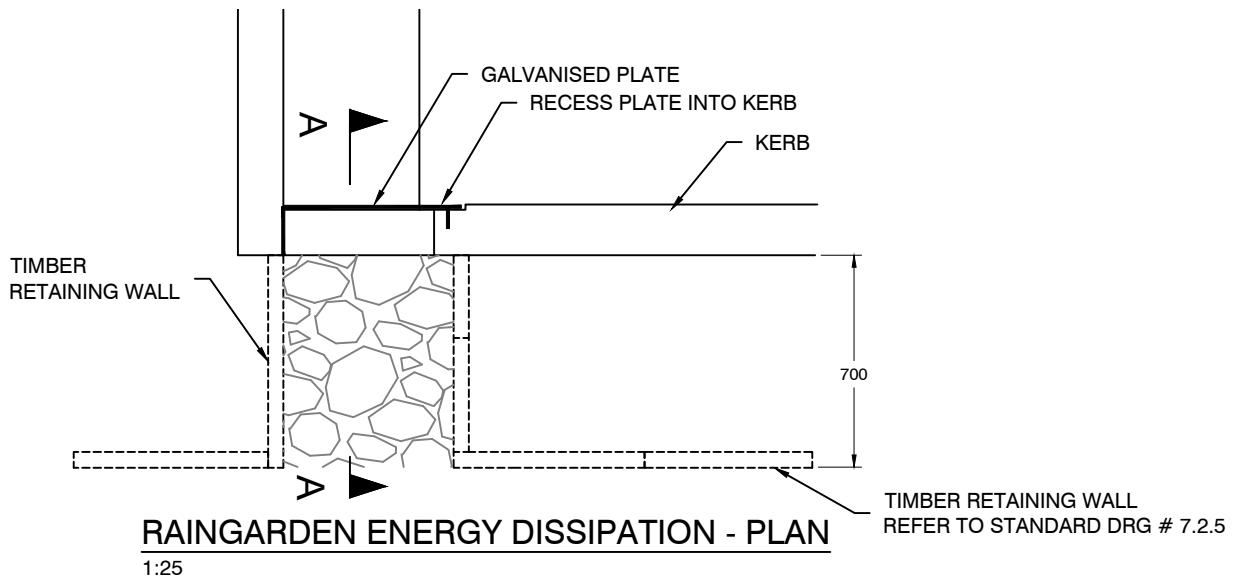
SECTION 1:20



NOTES:

1. PROPOSED FILTER MEDIA SHALL COMPLY WITH CITY'S TECHNICAL SPECIFICATION B10 - 10.4.8.2
2. BACK OF KERB SHALL BE CONSTRUCTED VERTICALLY AND NO EXCESS CONCRETE SHALL BE POURED IN THE RAIN GARDEN.
3. WHERE STRUCTURAL STABILITY OF KERB IS A CONCERNED MATTER, THE KERB & GUTTER MAY BE REINFORCED USING REINFORCEMENT STEEL BARS REFER DWG# 1.1.15.
4. THE KERBS MATERIAL SHALL BE SELECTED IN ACCORDANCE WITH CITY OF SYDNEY STREET CODE.
5. DRAINAGE LAYER MAY BE DELETED IF THERE IS NO DRAINAGE IN THE VICINITY SUBJECTED TO CITY'S APPROVAL.
6. THE SLOTTED PIPE SHALL BE CONNECTED TO BY PASS CHAMBER OF BYPASS PIT/SURCHARGE PIT
7. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

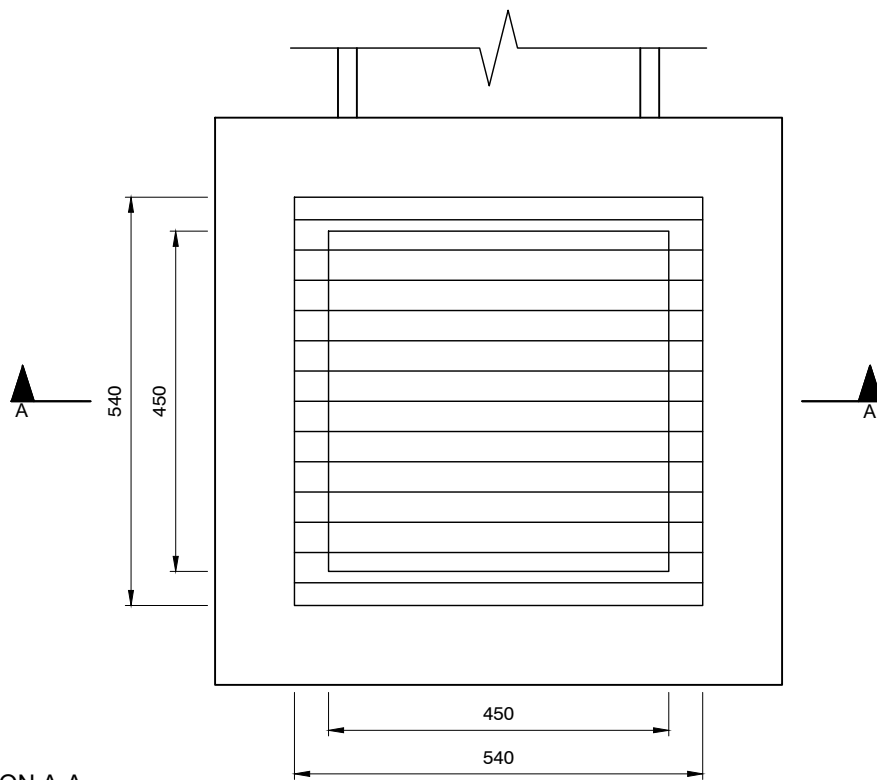




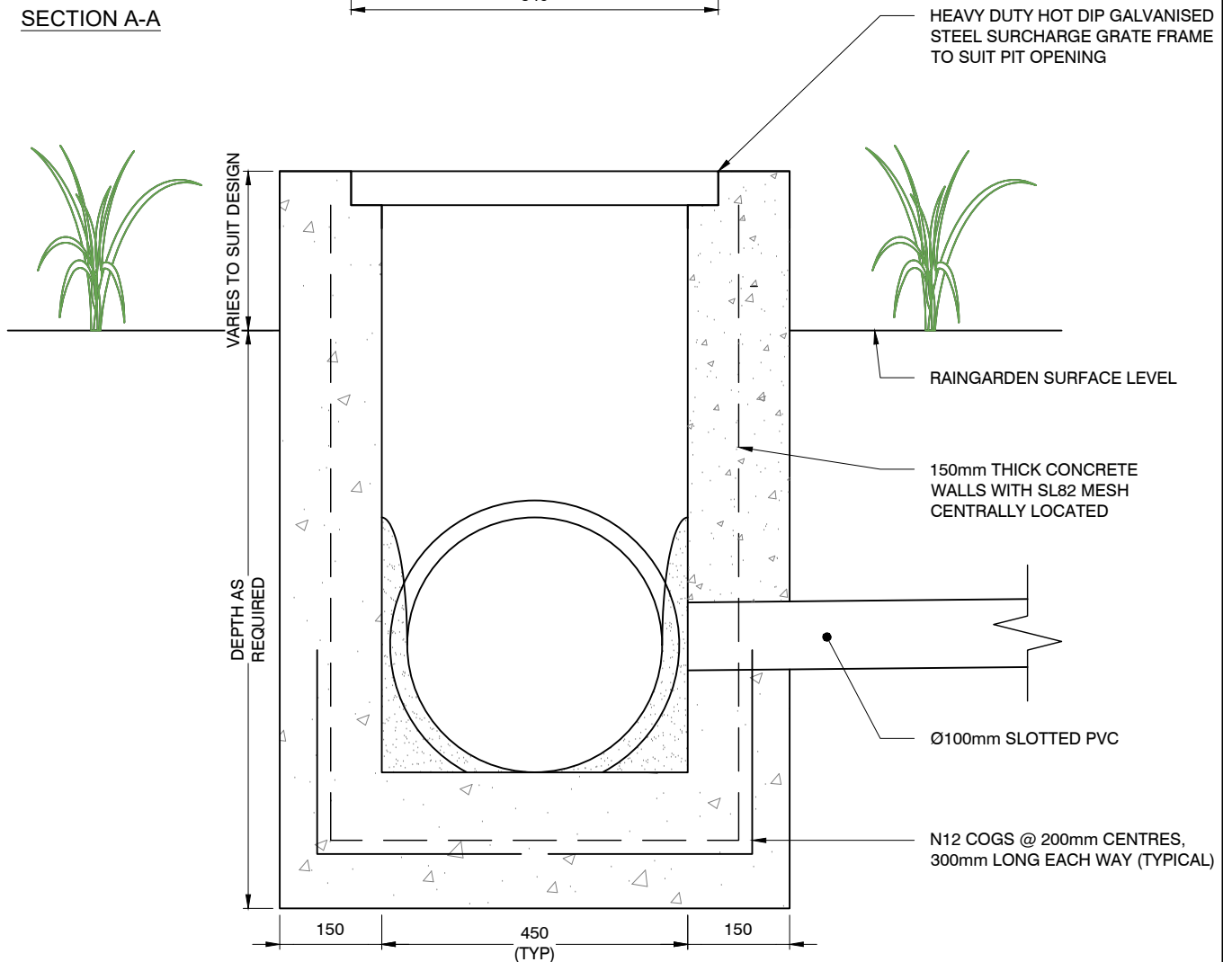
NOTES:

1. DISSIPATION BASIN LENGTH SHALL BE MINIMUM 400mm UNLESS NOTES OTHERWISE.
2. T - TERRACE GARDEN IS THE PREFERRED OPTION FOR MOST OF RAINGARDENS EXCEPT ROCK SWALES: IN WHICH CASE ENTRY STRUCTURE SHALL BE DESIGNED TO SUIT THE ROCK SWALE
3. THE PLATE SHALL BE RECESSED INTO THE KERB
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

PLAN



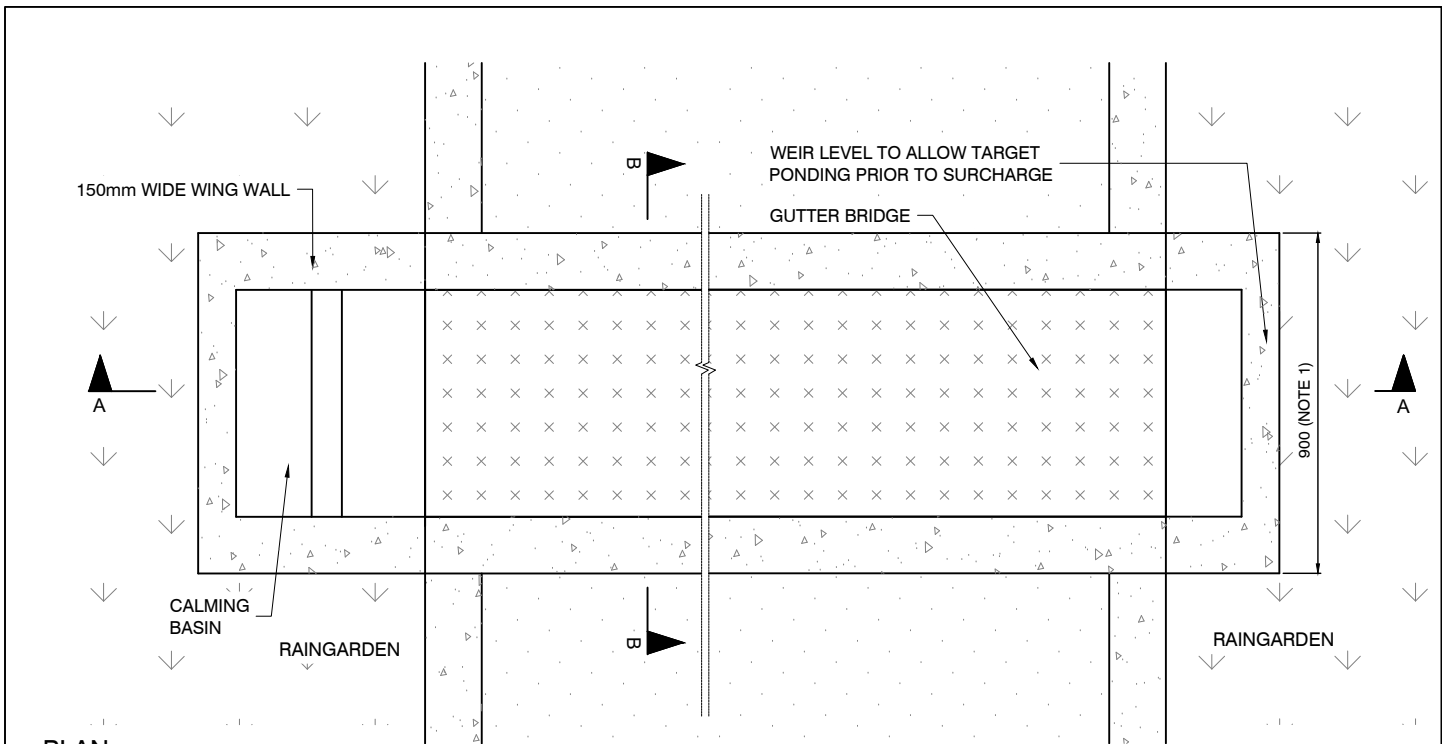
SECTION A-A



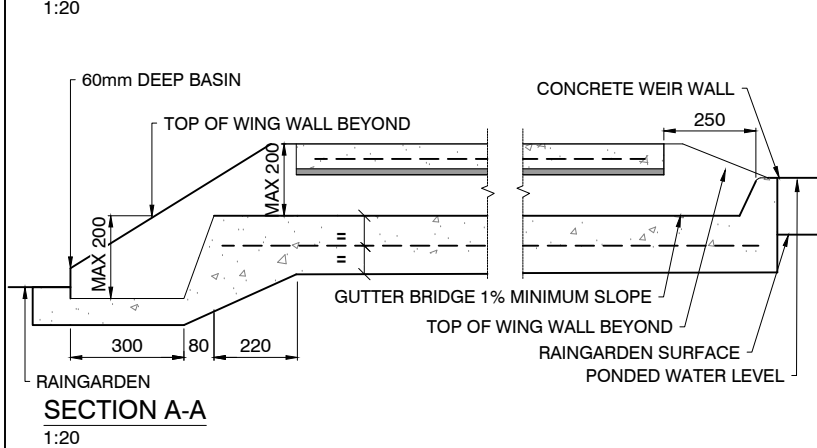
SCALE 1:10

NOTES:

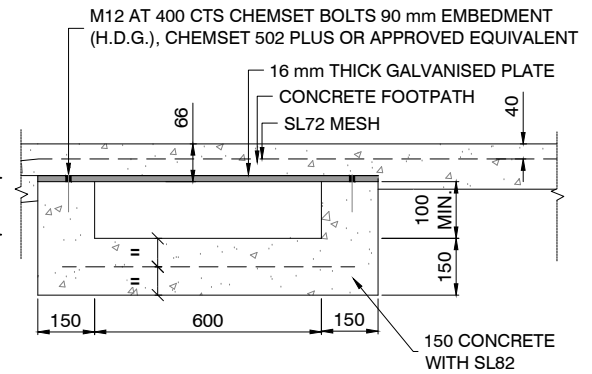
1. ALL CONCRETE IS TO HAVE A MINIMUM STRENGTH OF 32 MPa.
2. PIT STRUCTURE TO BE 150mm THICK UNLESS NOTED OTHERWISE.
3. DRAINAGE PIPE TO BE MINIMUM 375Ø CLASS 4 REINFORCED CONCRETE PIPE
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN
1:20

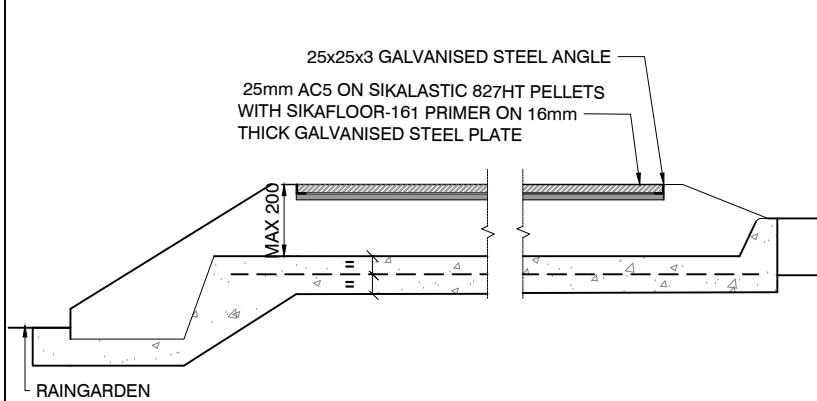


SECTION A-A
1:20

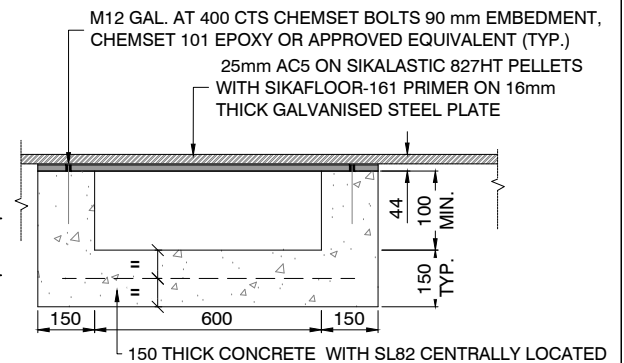


SECTION B-B
1:20

CONCRETE GUTTER BRIDGE



SECTION A-A
1:20



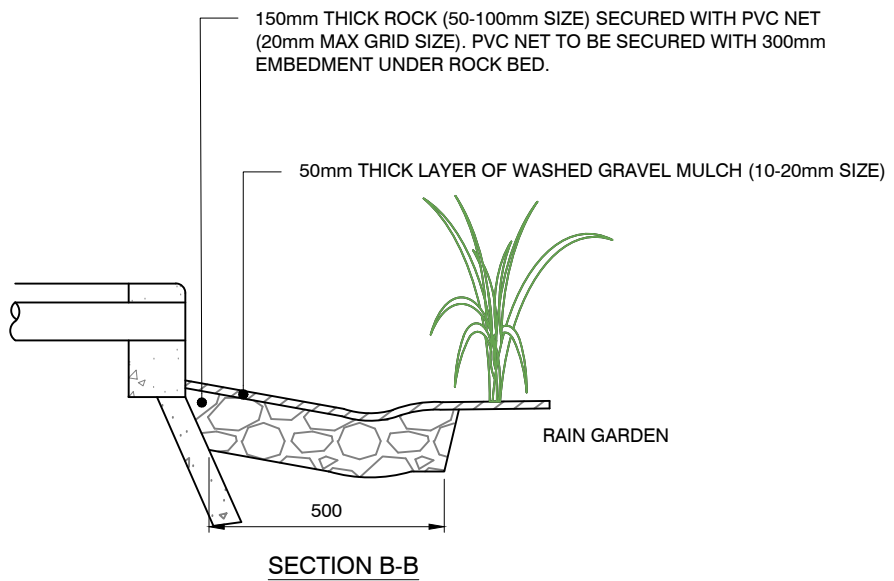
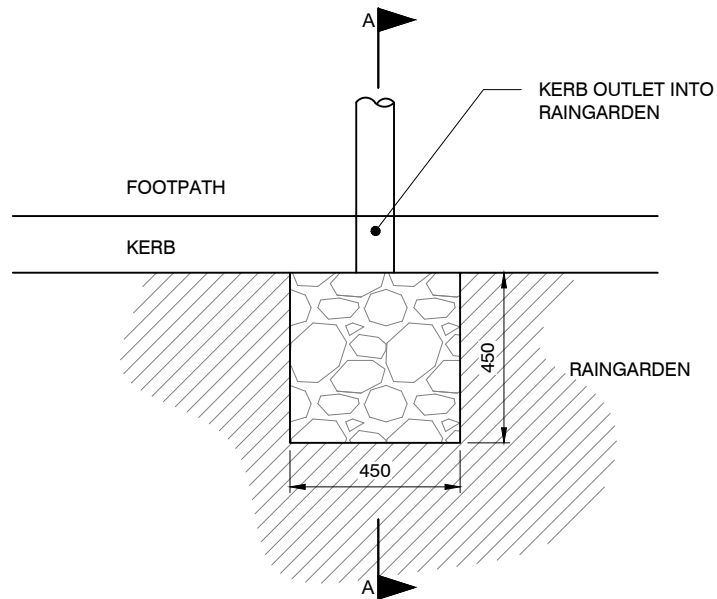
SECTION B-B
1:20

ASPHALT GUTTER BRIDGE

NOTES:

1. GUTTER BRIDGE SHALL BE DESIGNED TO SUIT MINIMUM 5 YEARS ARI STORM. DESIGNER SHALL SUBMIT MAINTENANCE REGIME WITH ANY RAINGARDEN INCORPORATED IN DESIGN.
2. USE OF BONDEK IS NOT ALLOWED FOR GUTTER BRIDGES.
3. SIZE OF GUTTER BRIDGE SHALL BE DESIGNED TO SUIT THE ANTICIPATED FLOW RATES
4. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
5. CONCRETE STRUCTURES & REINFORCEMENT TO COMPLY WITH AS 3600, AS 4671 & CoS TECHNICAL SPECIFICATIONS.
6. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

SMALL DISSIPATION ROCKS

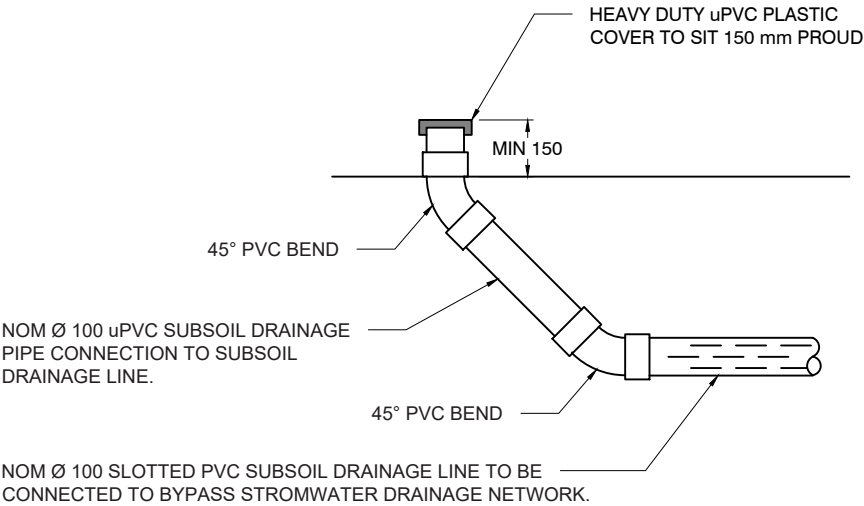


SCALE 1:20

NOTE: ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

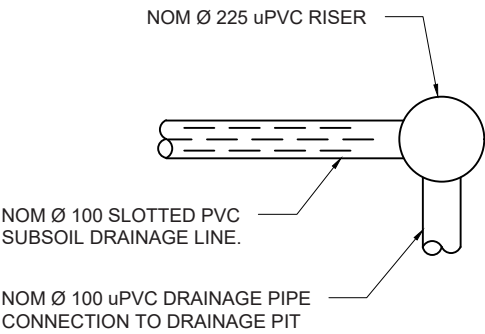
Ø 100 HIGH END RISER

SECTION

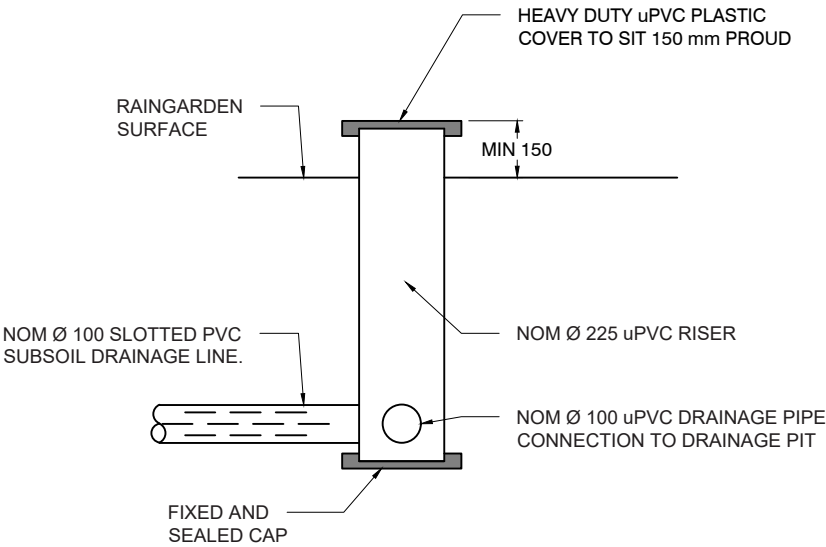


Ø 225 HIGH END RISER JUNCTION

PLAN

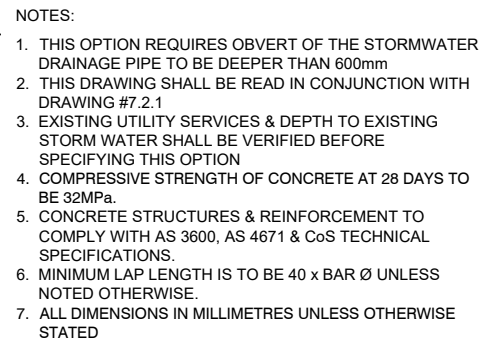
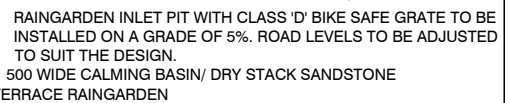


SECTION

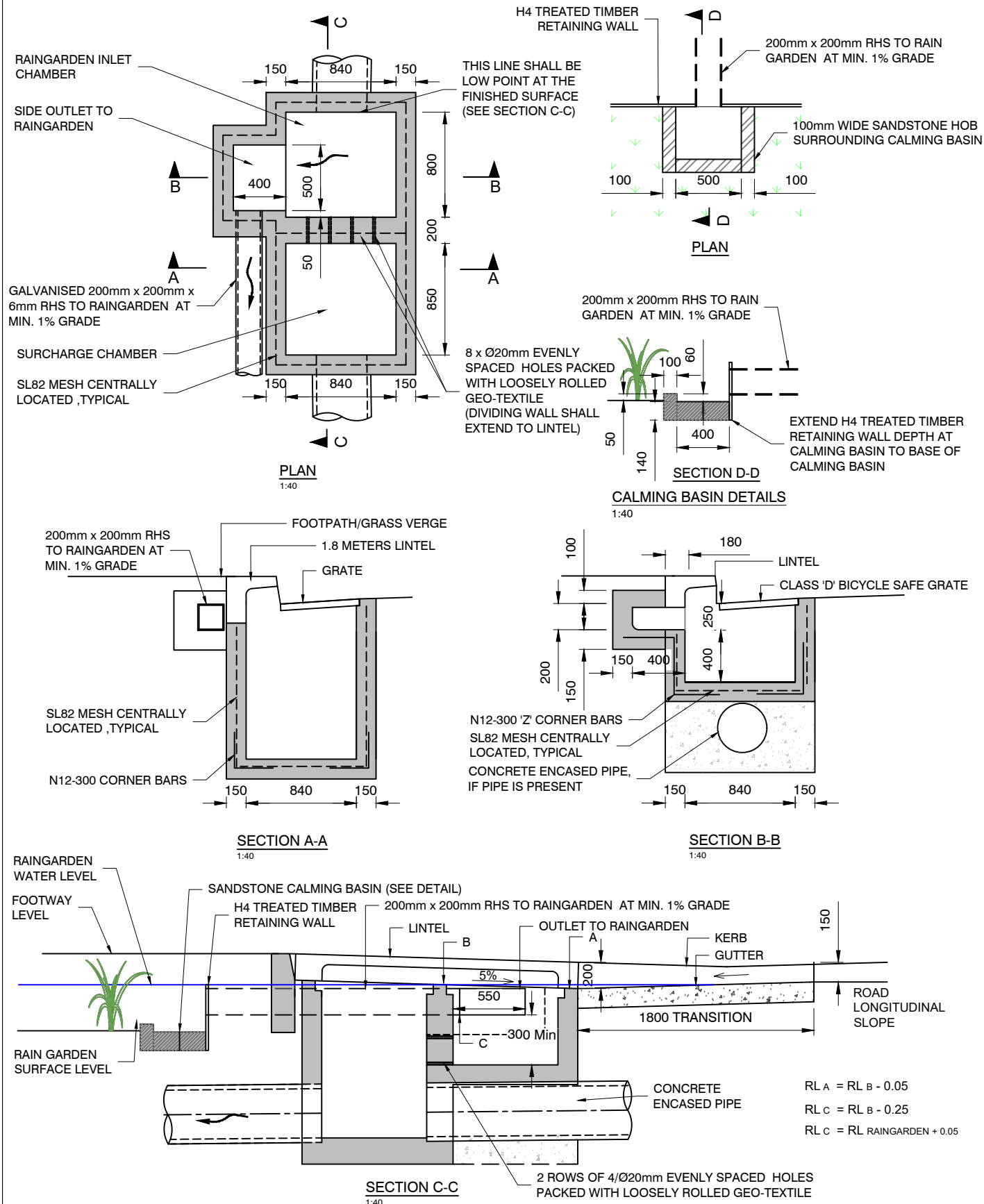


- NOTES:
1. THE SUBSOIL DRAIN SHALL BE CONNECTED TO THE
 - BYPASS CHAMBER OF THE INLET PIT, OR;
 - BYPASS DRAINAGE PIT, OR;
 - RAINGARDEN SURCHARGE PIT.
 2. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

SCALE 1:20

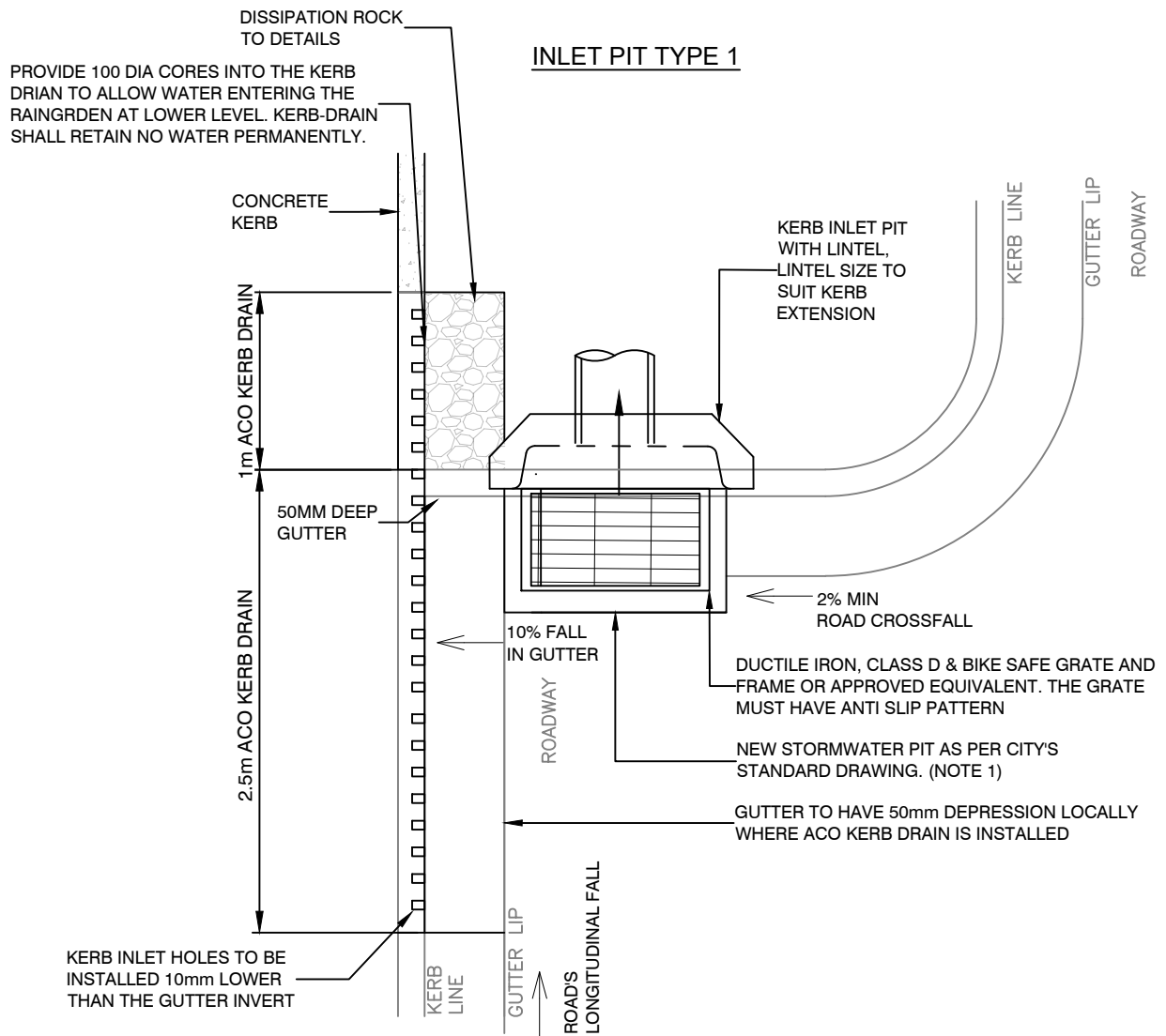


DRAINAGE
Dwg No.
7.2.11



NOTES:

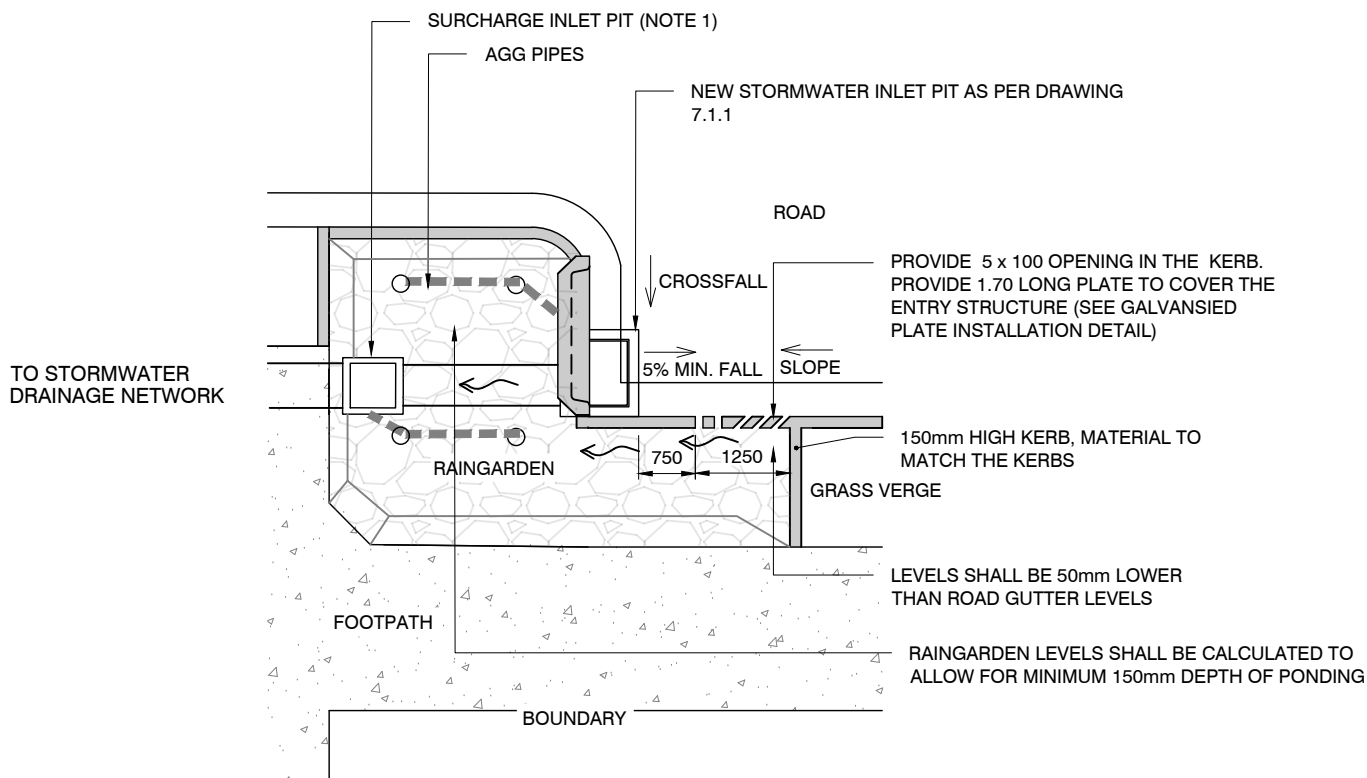
1. THIS DRAWING SHALL BE READ IN CONJUNCTION WITH DRAWING #7.2.1
2. THIS OPTION REQUIRES OBVERT OF THE STORMWATER DRAINAGE PIPE TO BE DEEPER THAN 700 mm.
3. SIZE OF THE BYPASS SHALL BE ADJUSTED TO SUIT THE CATCHMENT SIZE.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



PLAN
1:40

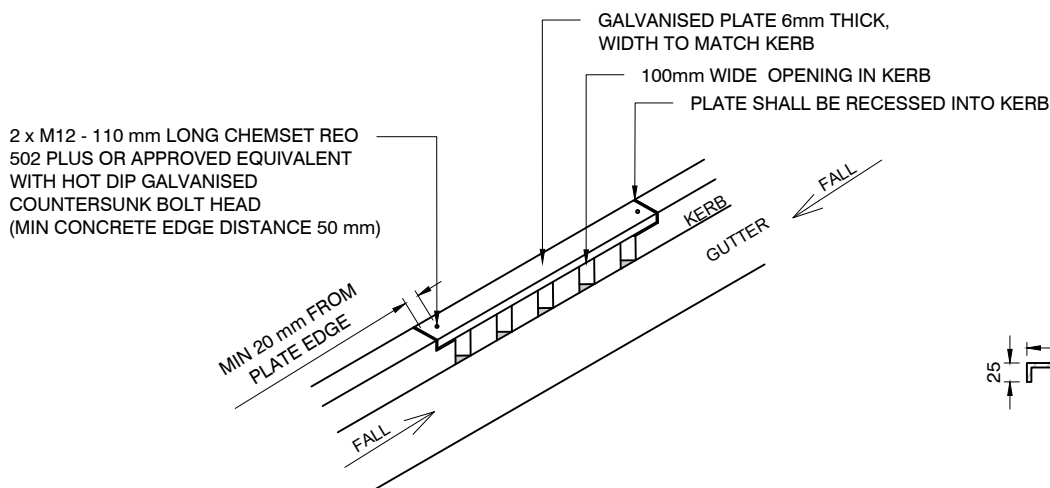
NOTES:

1. STORMWATER PIT SHALL BE CONSTRUCTED AS PER CITY'S STANDARD DRAWINGS. THE PIT SHALL BE CAREFULLY SELECTED TO SUIT SYDNEY STREET CODE & STANDARD SPEC. FROM DRAWINGS #7.1.1 TO 7.1.6.
2. THE ACO KERB DRAIN OR APPROVED EQUIVALENT SHALL BE USED FOR RANGARDEN ENTRY PIT.
3. THIS DETAIL IS WELL SUITED FOR THE AREAS WHERE
 - (i) NO GRASS VERGE EXISTS
 - (ii) THE DRAINAGE PIPES ARE SHALLOWER THAN 1.20m.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.



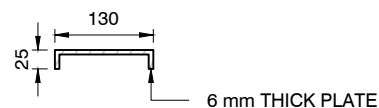
PLAN

1:100



GALVANISED PLATE INSTALLATION DETAIL

1:50



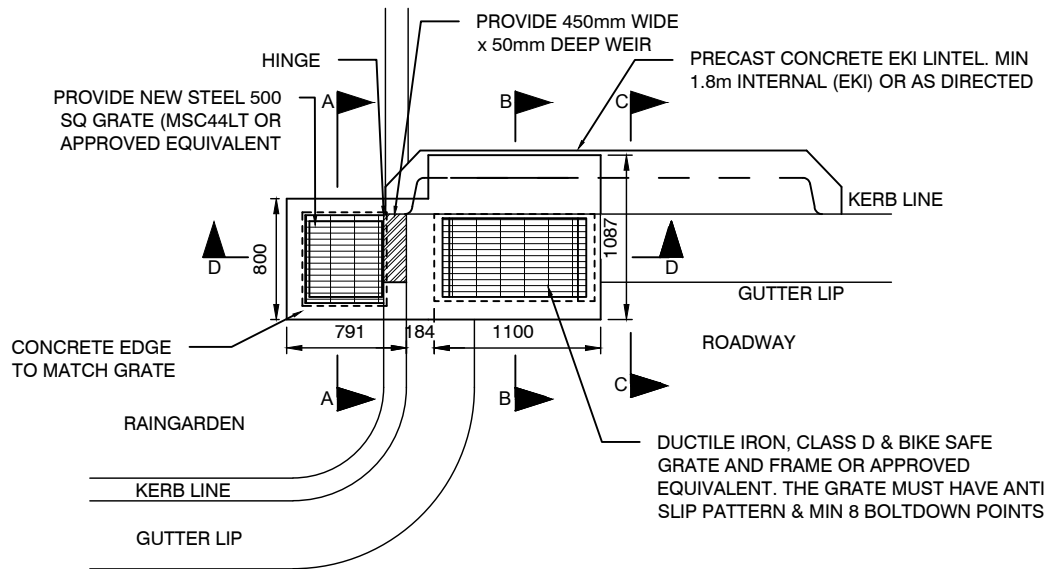
GALVANISED PLATE DETAIL

1:10

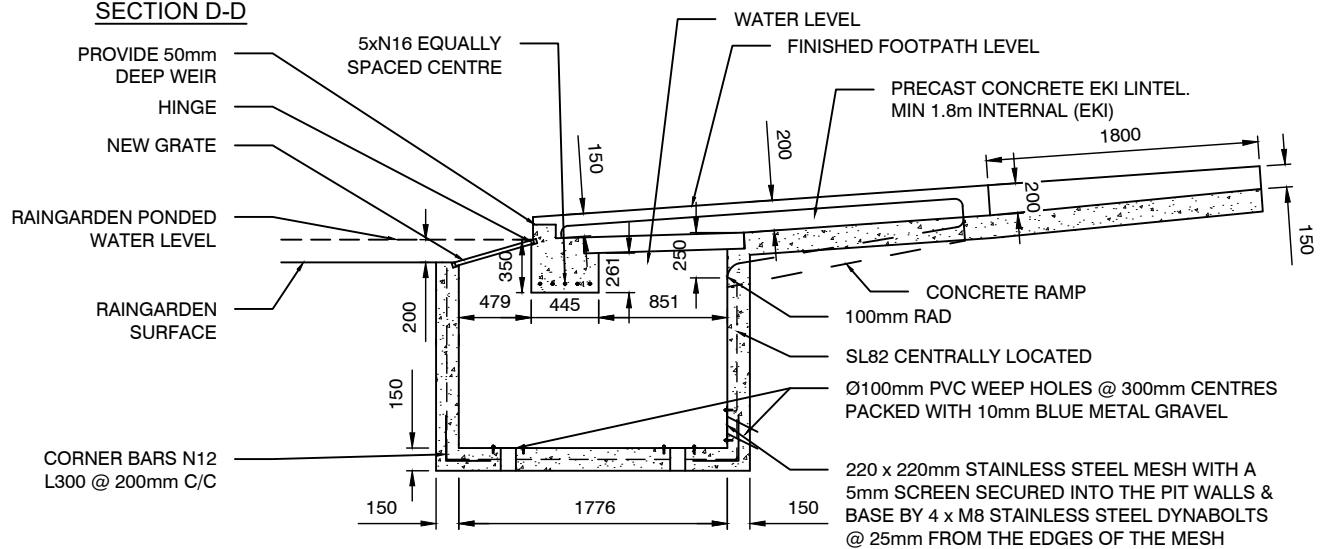
NOTES:

1. RAINGARDEN SHALL BE DESIGNED IN ACCORDANCE WITH SYDNEY STREET TECHNICAL SPECIFICATION PART A4.
2. SURCHARGE PIT MAY BE DELETED UPON APPROVAL. REFER SYDNEY STREET TECHNICAL SPECIFICATION PART A4.
3. THIS OPTION BEST SUITS SMALLER CATCHMENTS WHERE:
 - (i) FOOTPATH HAS A GRASS VERGE.
 - (ii) INVERTS OF THE EXISTING DRAINAGE PIPES ARE SHALLOWER THAN 1.2m.
4. THE DRAINAGE PIT SHALL BE CONSTRUCTED IN ACCORDANCE WITH DRAWINGS #7.1.1 TO #7.1.6.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

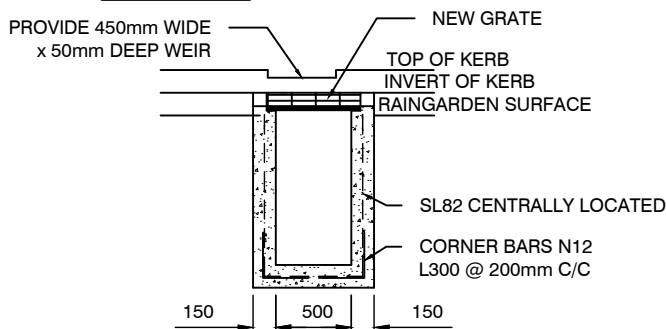
PLAN



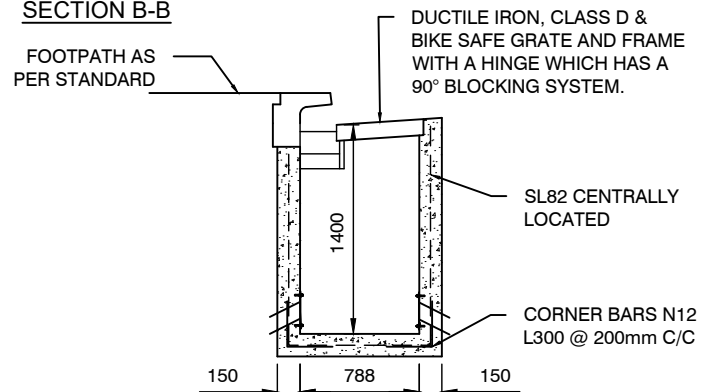
SECTION D-D



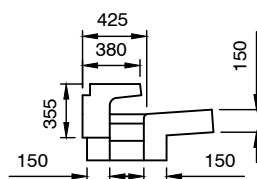
SECTION A-A



SECTION B-B



SECTION C-C



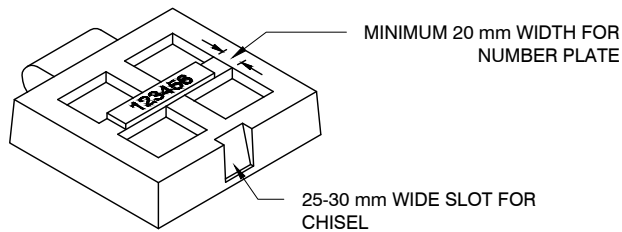
SCALE 1:50

NOTES:

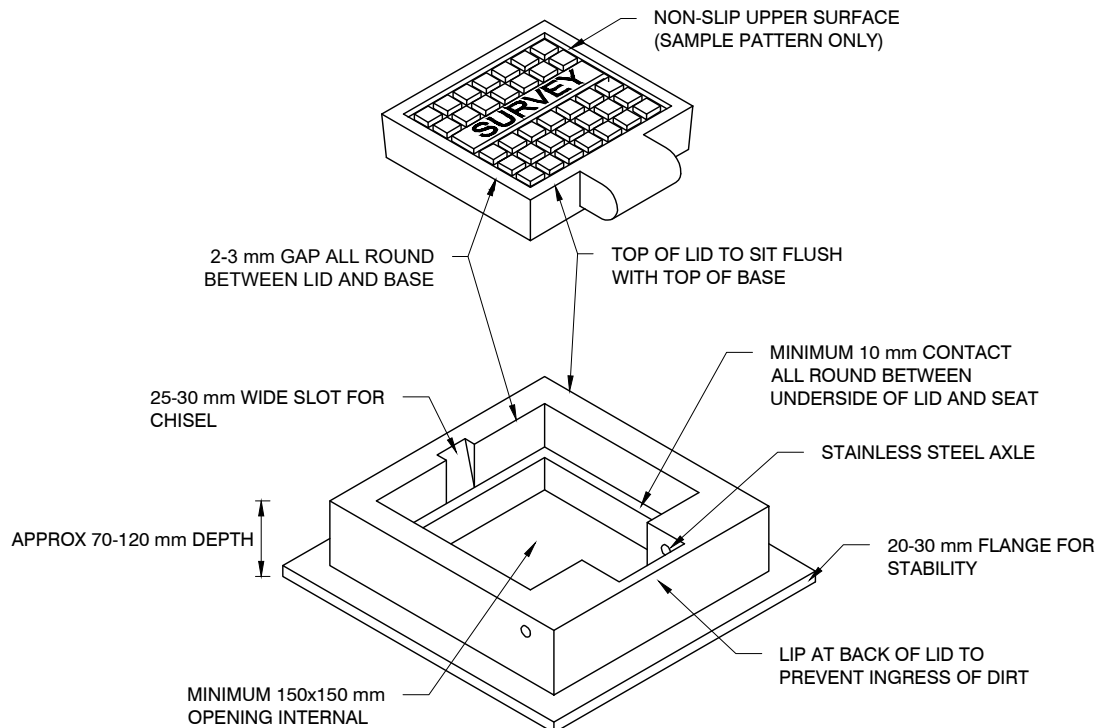
1. COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS TO BE 32MPa.
2. 75mm MINIMUM BENCHING TO HALF PIPE HEIGHT TOTAL BENCHING TO OBVERT OF PIPE.
3. Ø100mm SUBSOIL DRAINAGE PIPE 3.0m LONG WRAPPED IN FABRIC SOCK TO BE PROVIDED IN PIPE TRENCHES ADJACENT TO INLET PIPES.
4. PROVIDE STEP IRONS WHERE PIT IS DEEPER THAN 1.0m AT 300mm CENTRES.
5. PITS OVER 1.5m IN DEPTH TO BE REINFORCED WITH SL82 MESH RETURNED 300mm INTO BASE WITH WALLS 200mm THICK.
6. PITS OVER 2.4m IN DEPTH TO BE DESIGNED BY STRUCTURAL ENGINEER.
7. GRATES SHALL BE BICYCLE SAFE AND HAVE MAXIMUM INLET CAPACITY. ALL GRATES MUST BE APPROVED BY THE CITY'S REPRESENTATIVE.
8. ALL CONCRETE IS TO HAVE A MINIMUM STRENGTH OF 32MPa.
9. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



LID (UNDERSIDE)



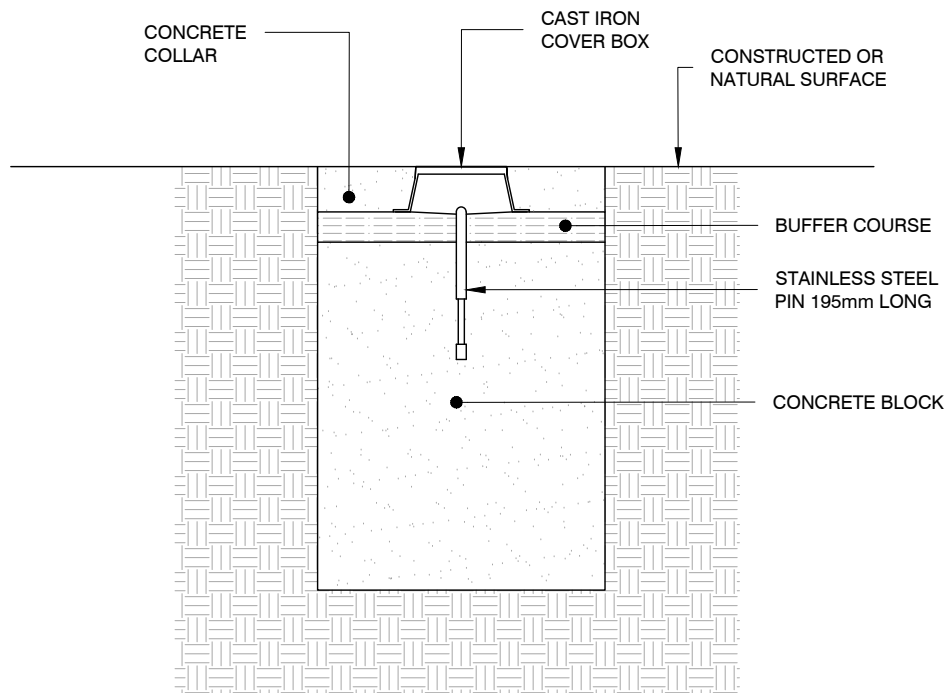
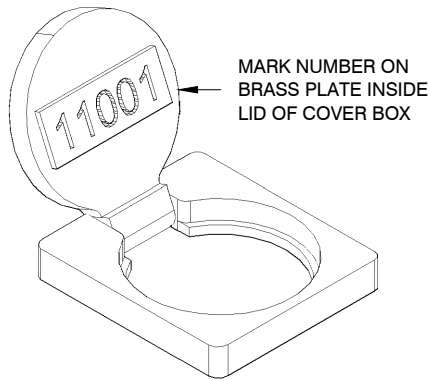
LID (TOPSIDE)



FRAME

SECTION 1:10

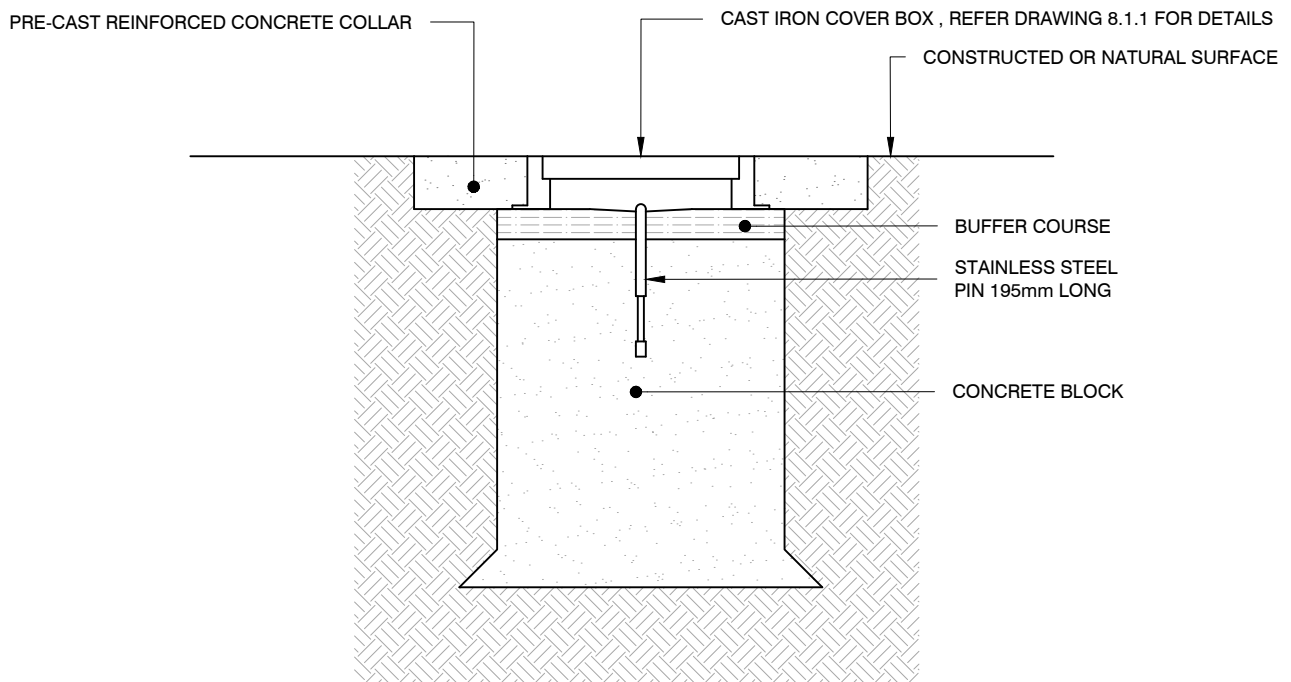
ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



SECTION 1:10

NOTES:

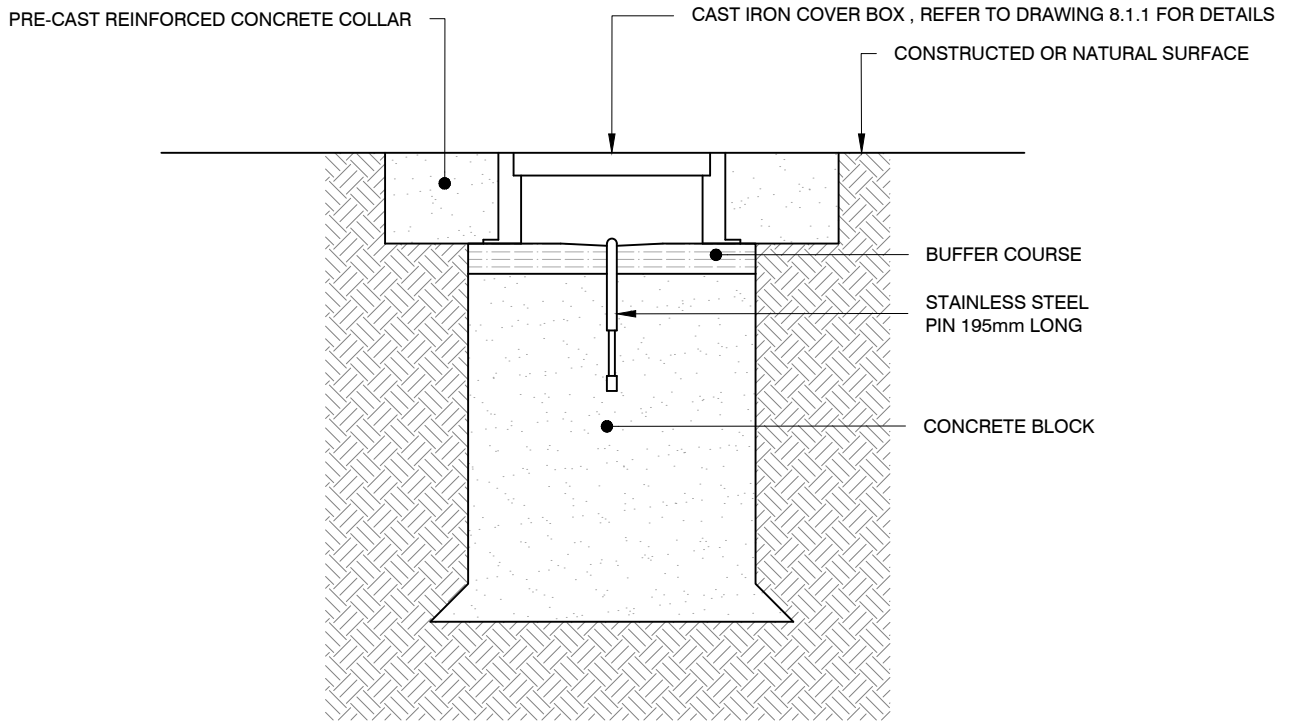
1. MINIMUM SIZE OF CONCRETE 460 mm DEEP BY 380 mm SQUARE AND ENLARGED AT THE BOTTOM. WHERE SOLID ROCK IS MET THE DEPTH MAY BE VARIED.
2. IN LOCALITIES WHERE THE GROUND IS UNSTABLE THE DIMENSIONS MUST BE INCREASED.
3. THE STAINLESS STEEL PIN IS TO PROTRUDE 50 mm ABOVE THE SURFACE OF THE CONCRETE BLOCK.
4. THE BUFFER COURSE IS TO BE A 50 mm LAYER OF CRUSHED BRICK, GRAVEL OR COARSE SAND.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED



SECTION 1:10

NOTES:

1. MINIMUM SIZE OF CONCRETE 460 mm DEEP BY 380 mm SQUARE AND ENLARGED AT THE BOTTOM. WHERE SOLID ROCK IS MET THE DEPTH MAY BE VARIED.
2. IN LOCALITIES WHERE THE GROUND IS UNSTABLE THE DIMENSIONS MUST BE INCREASED.
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4. THE BUFFER COURSE IS TO BE A 50 mm LAYER OF CRUSHED BRICK, GRAVEL OR COARSE SAND.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

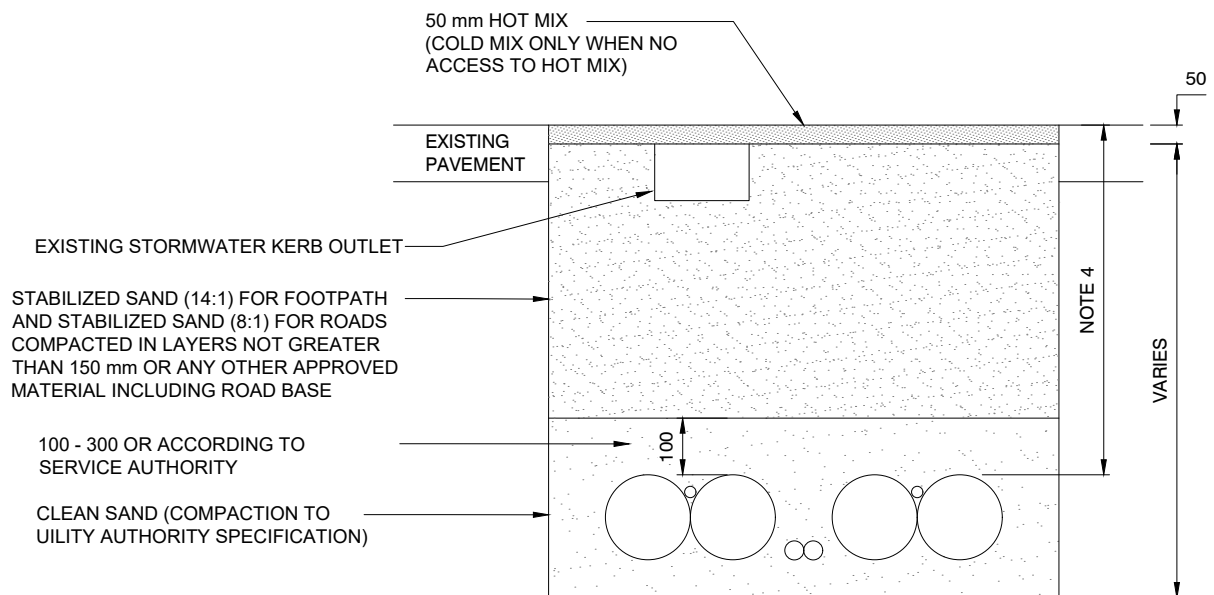


SECTION 1:10

NOTES:

1. MINIMUM SIZE OF CONCRETE 460 mm DEEP BY 380 mm SQUARE AND ENLARGED AT THE BOTTOM. WHERE SOLID ROCK IS MET THE DEPTH MAY BE VARIED.
2. IN LOCALITIES WHERE THE GROUND IS UNSTABLE THE DIMENSIONS MUST BE INCREASED.
3. THE STAINLESS STEEL PIN IS TO PROTRUDE 50 mm ABOVE THE SURFACE OF THE CONCRETE BLOCK.
4. THE BUFFER COURSE IS TO BE A 50 mm LAYER OF CRUSHED BRICK, GRAVEL OR COARSE SAND.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

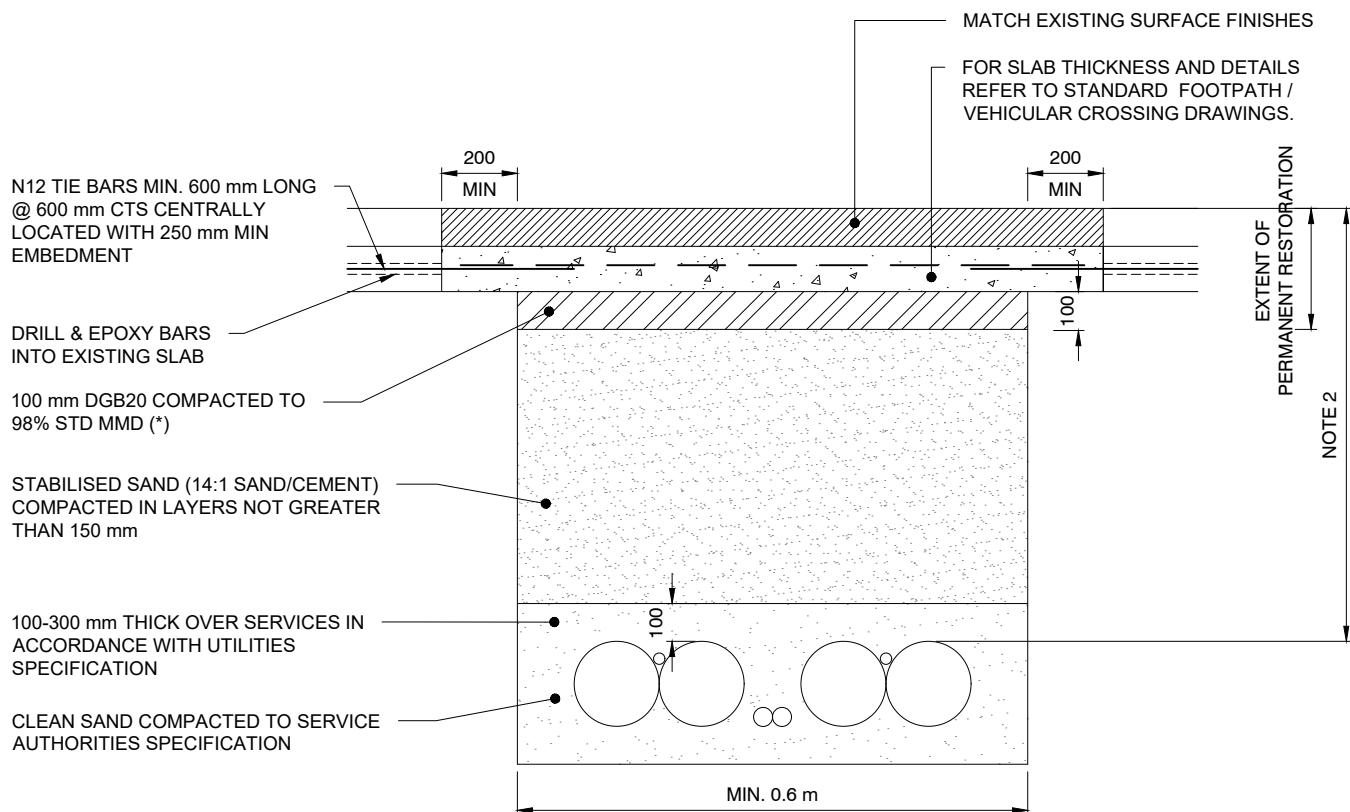
TEMPORARY RESTORATION FOR ALL ROAD AND FOOTPATH



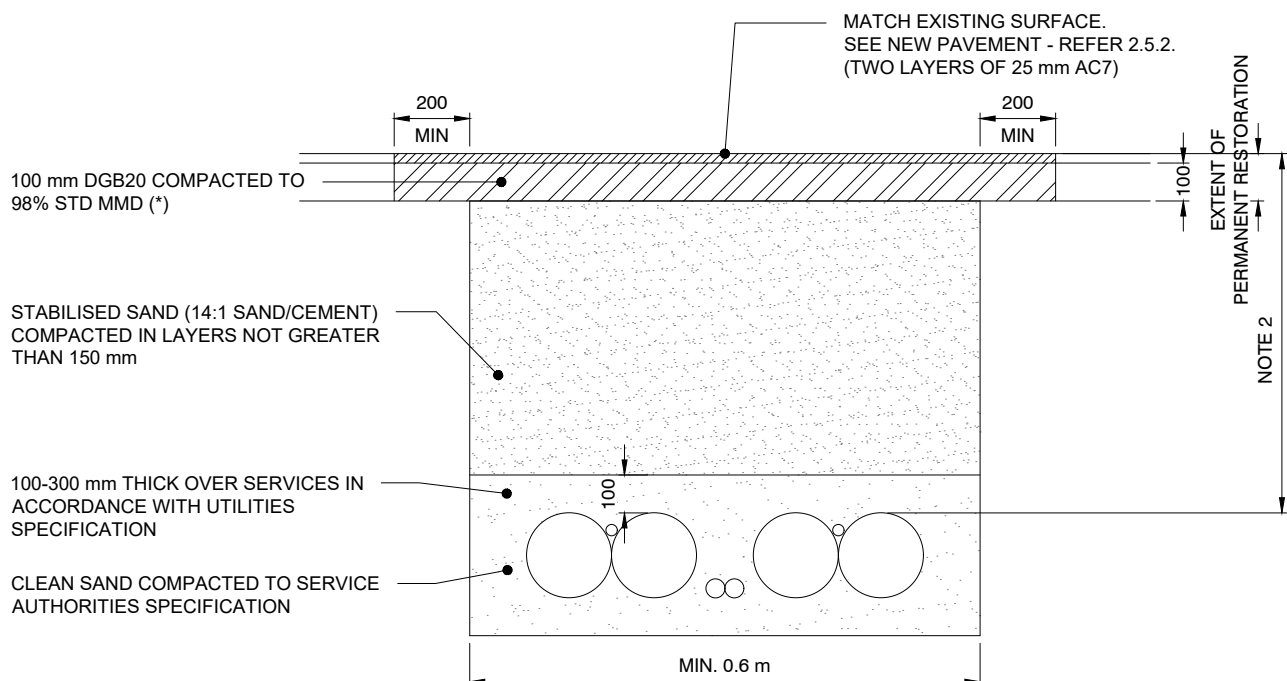
NOTES:

1. MINIMUM RESTORATION WIDTH 0.6 m FOR ASPHALT ROAD AND 1.0 m FOR CONCRETE ROAD
2. MINIMUM RESTORATION WIDTH 0.6 m FOR ASPHALT FOOTPATH AND 0.6 m FOR CONCRETE FOOTPATH
3. REFER TO ANNEXURE A OF SECTION B12 ROAD OPENINGS AND RESTORATION FOR MORE DETAILS.
4. FOR NEW SERVICES MINIMUM 1200 mm CLEARANCE FROM INVERT OF GUTTER.
5. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

FOOTPATH - PERMANENT RESTORATION FOR CONCRETE FOOTPATH



FOOTPATH - PERMANENT RESTORATION FOR FLEXIBLE ASPHALT FOOTPATH

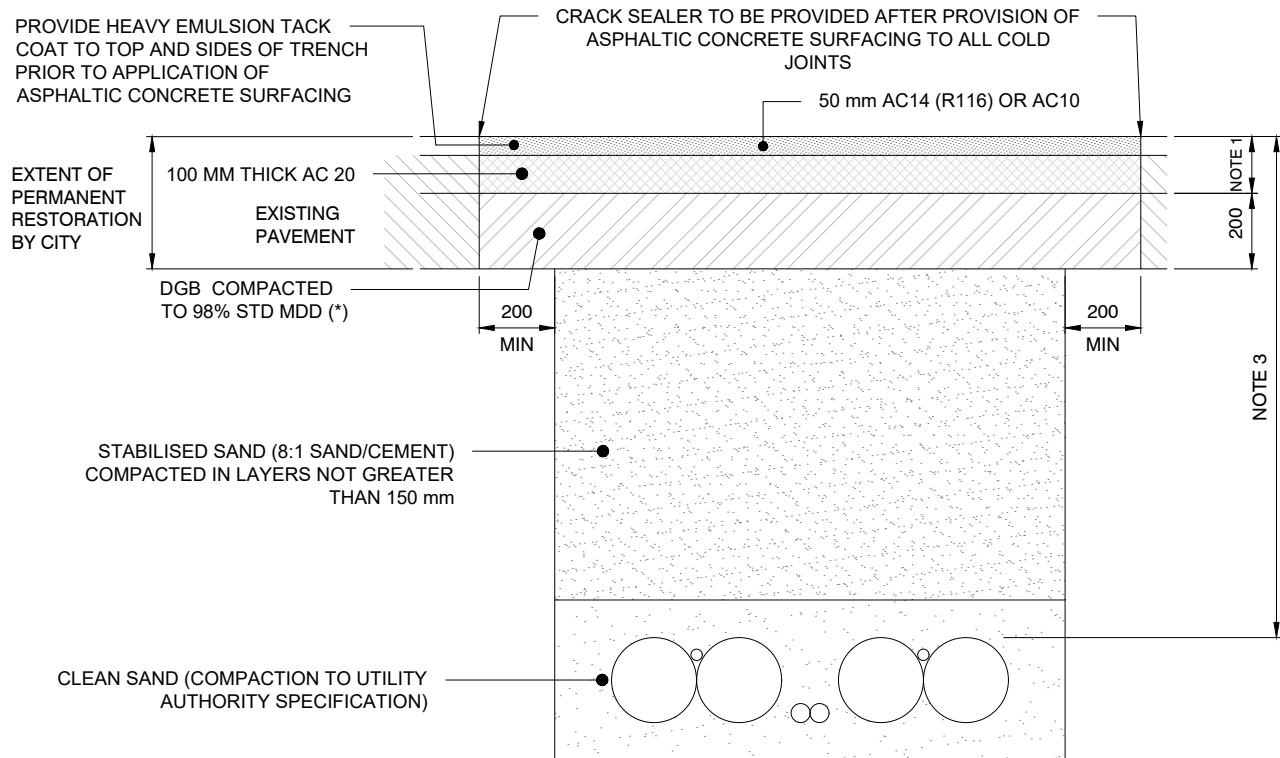


SECTION 1:20

NOTES:

- *1. AREAS LESS THAN 10 m² - IF THE ROAD BASE IS USED INSTEAD OF CEMENT STABILISED SAND, THE TOP 100 mm OF DGB CAN BE SCARIFIED AND RECOMPACTED IF THE MATERIAL IS FOUND TO COMPLY WITH STANDARD
2. FOR NEW SERVICES MINIMUM 600 mm CLEARANCE FROM INVERT OF GUTTER.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

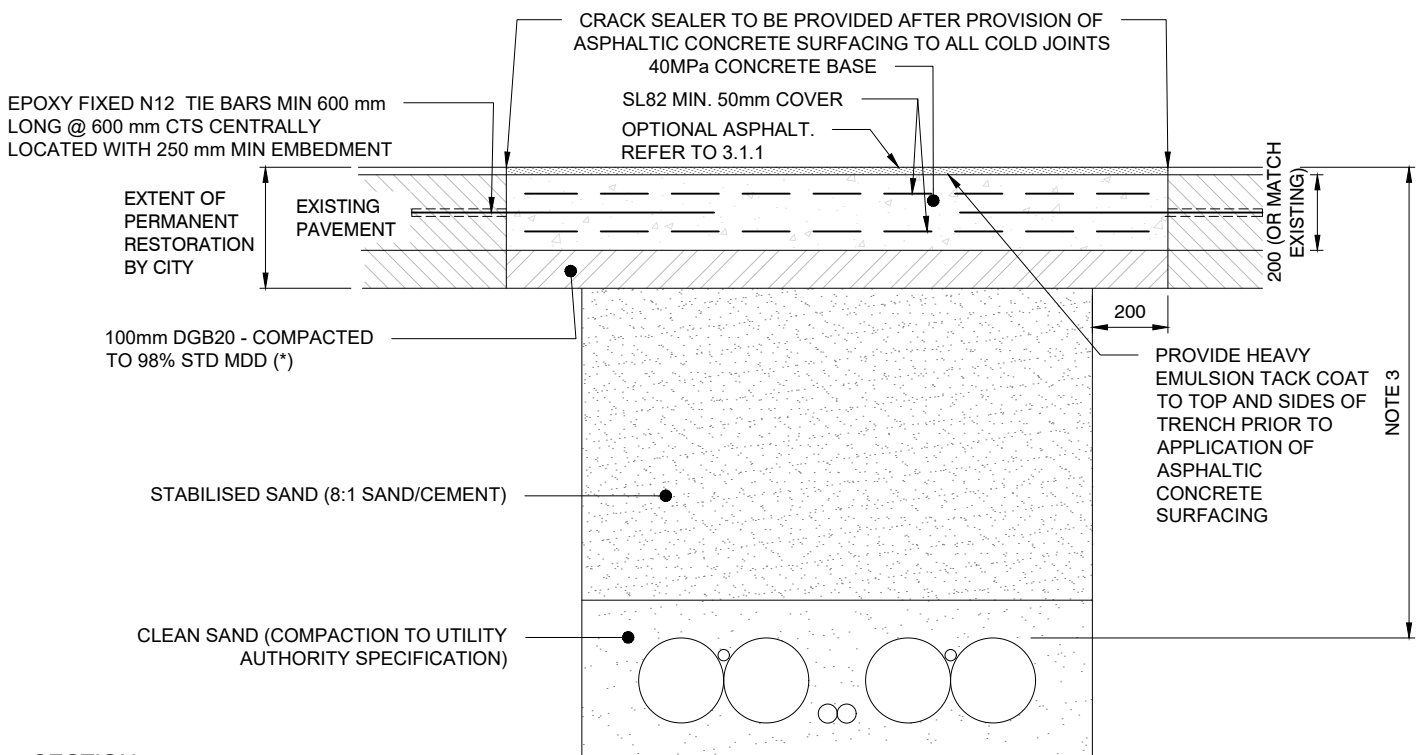
ROAD PAVEMENT - PERMANENT RESTORATION FOR ASPHALT ROAD



NOTE:

1. FOR AREAS LESS THAN 10 m², USE AC14 150 mm IN TWO LAYERS.

ROAD PAVEMENT - PERMANENT RESTORATION FOR CONCRETE ROAD

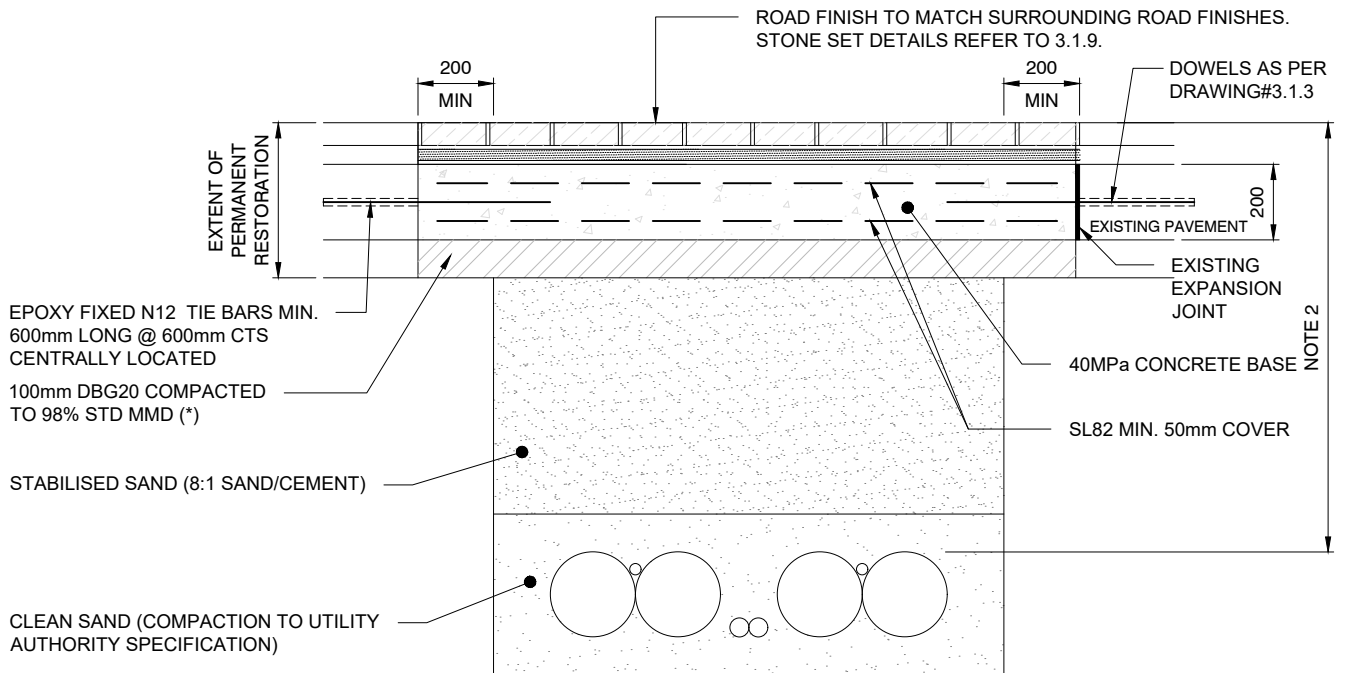


SECTION 1:20

NOTES:

- *1. AREA LESS THAN 10 m² - IF ROAD BASE IS USED INSTEAD OF CEMENT STABILISED SAND AND THE MATERIAL IS FOUND TO COMPLY WITH STANDARD
 - (i) FOR CONCRETE ROADS - THE TOP 100 mm OF DGB CAN BE SCARIFIED AND RECOMPACTED
 - (ii) FOR ASPHALT ROAD - THE TOP 200 mm CAN BE SCARIFIED AND RECOMPACTED IN TWO LAYERS
2. RESTORATION WORKS TO MATCH EXISTING LEVELS OR MINIMUM DIMENSIONS SPECIFIED.
3. FOR NEW SERVICES MINIMUM 1200 mm CLEARANCE FROM INVERT OF GUTTER.
4. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

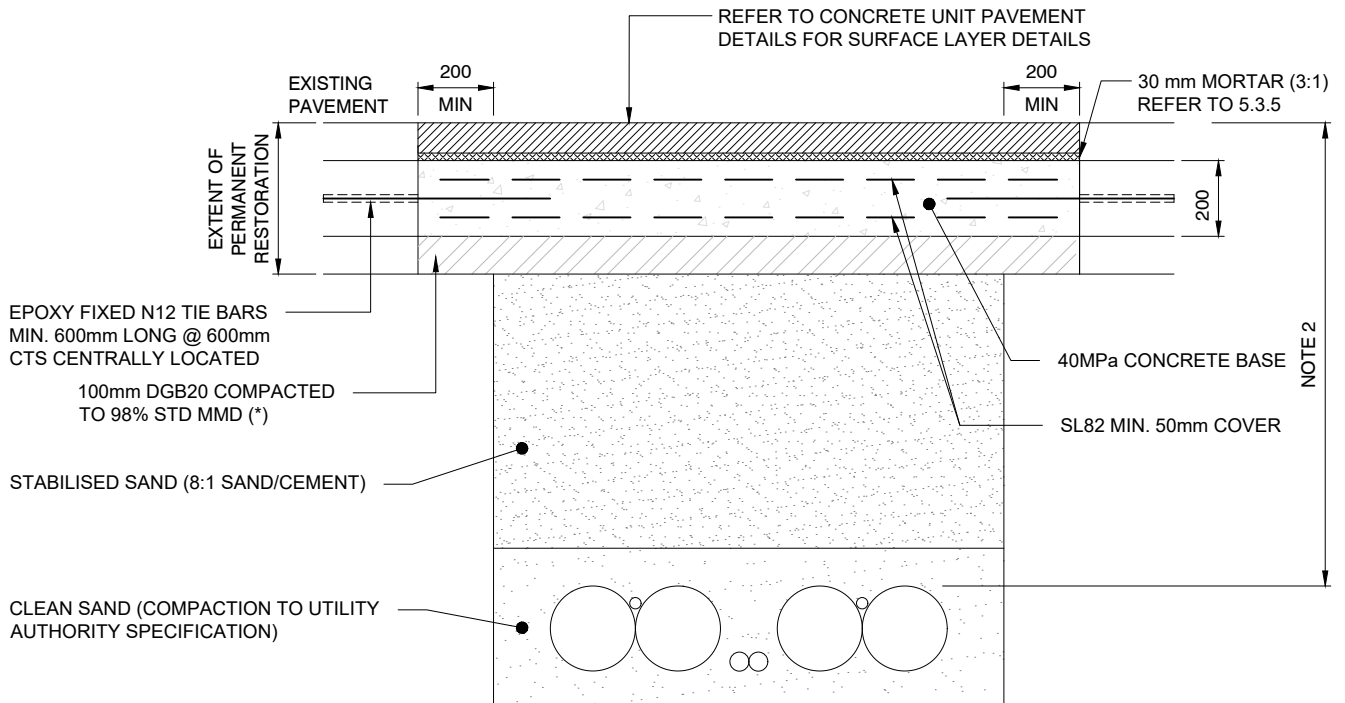
STONE SET ROAD PAVEMENT



NOTES:

1. WHERE RESTORATION ZONE INCLUDES EXISTING EXPANSION JOINTS, THE EXPANSION JOINTS SHALL BE RESTORED AS PER NEW DOWEL'S CONSTRUCTION DETAILS
2. REFER TO DWG 3.1.3 FOR EXPANSION JOINT DETAIL
3. FOR NEW SERVICES MINIMUM 1200 mm CLEARANCE FROM INVERT OF GUTTER.

CONCRETE UNIT ROAD PAVEMENT



SECTION 1:20

NOTES:

- *1. AREAS LESS THAN 10 m² - IF THE ROAD BASE IS USED INSTEAD OF CEMENT STABILISED SAND, THE TOP 100 mm OF DGB CAN BE SCARIFIED AND RECOMPACTED IF THE MATERIAL IS FOUND TO COMPLY WITH STANDARD
2. FOR NEW SERVICES MINIMUM 1200 mm CLEARANCE FROM INVERT OF GUTTER.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED

Revision Register

Drg. No.	Revision Details	Rev. No	Year
C1 Kerb & Gutter			
1.1.1	Mass concrete grade for existing stone revised to 25 Mpa to make it consistent with details of new stone kerb	E	Nov-22
1.1.2	Barrier kerb for cycleway included and concrete grade updated to 32 MPa	F	Jul-25
1.1.7 & 1.1.8	Chamfered lip removed for laybacks adjacent to cycleway	E	Jul-25
1.1.14	For kerb outlet, RHS details updated	F	Jul-25
1.1.15	Footing details updated to avoid differential settlement	E	Nov-22
1.1.16	New wearing course details updated to match existing wearing course thickness & type	E	Nov-22
1.1.17	Stone separator installation details included	-	Jul-25
C2 Footways			
2.1.2-2.1.5, 2.3.10	Class for pit lid/covers in footway updated to C from B	E	Nov-22
2.1.8	Property ownership interface details updated to isolation joint	E	Jul-25
2.1.9	Tree grate and guard details updated	E	Jul-25
2.1.10	Footpath layout with garden beds and tree pits included	-	Jul-25
2.2.4 & 2.3.4	Details for junction with existing concrete slab updated	F	Jul-25
2.2.5 & 2.3.5	Contraction joint / Construction joint and isolation joint details updated	F	Jul-25
2.2.9 & 2.3.8	Paving around poles details updated	F	Jul-25
2.2.10 & 2.3.9	Reinforcement cover details updated	E	Nov-22
2.2.10 & 2.3.9	Paver size details updated	F	Jul-25
2.2.10, 2.3.9, 2.4.6, 2.4.7, 2.4.8, 2.5.5 – 2.5.7 & 2.6.8	Notes update for vertical lip at driveways	F	Jul-25
2.3.10	Compaction rate for DGB under gutter changed to 100% MDD from 98%	E	Nov-22
2.2.4, 2.2.5, 2.2.9, 2.3.4, 2.3.5, 2.3.8, 2.4.3-2.4.5-2.4.8, 2.5.5-2.5.7 & 2.6.8	Sub-base compaction rate changed to std 98% MDD from CBR 4%	E	Nov-22
2.4.6-2.4.8 & 2.5.5-2.5.8	Expansion joint detail removed for brick paving and amended for asphalt paving	E	Nov-22
2.3.12	Tree grate frame detail provided	E	Nov-22
2.4.3, 2.4.4 & 2.4.5	Base of lean mix concrete proposed for Brick paving	F	Jul-25

C: Standard Drawings

Drg. No.	Revision Details	Rev. No	Year
2.6.1-2.6.2	Trip stop details included, CJ and Expansion joints shown in drawing	E	Nov-22
2.6.4	Dummy joint / Tool joint depth update to 25mm or 1/4 th of slab depth	E	Jul-25
2.7.4	Notes updated to include TGSI's may be installed directly on concrete pavers if it complies with luminance contrast requirement set in AS 1428.4.1	E	Nov-22
2.8.1	Joints details provided	E	Nov-22
2.8.1	Paver size updated	F	Jul-25
2.8.2	New drawing for structural details / joint details	-	Nov-22
2.8.2 & 2.8.3	Slab thickness updated and details for CFT with pavers included	A	Jul-25
C3 Roadways			
3.1.3	Dower bar update to min R24 and continuous bottom reinforcement mesh shown at contraction joint	E	Jul-25
3.1.4	Construction joint / junction with existing pavement detail updated	E	Jul-25
3.1.9 & 3.1.11	Ston set installation details updated	E	Jul-25
3.1.10	Paver details updated	E	Jul-25
3.2.2-3.2.5	Concrete slab details updated	E	Jul-25
3.2.5	New drawing for structural details for raised platform (kerb-kerb)	-	Nov-22
C4 Street Furniture			
4.2.1	Tzannes seat footing details updated	E	Nov-22
4.2.1	Mesh overlap details updated	F	Jul-25
4.2.2	Tzannes seat footing details updated	E	Nov-22
4.2.2	Seat installation height requirements updated	F	Jul-25
4.3.1	Bin enclosure footing details updated	E	Nov-22
4.3.1	Mesh details updated	F	Jul-25
4.3.2	New Drawing - Bin enclosure details over existing asphalt footpath / grass surface	-	Nov-22
4.3.3	New Drawing - Bin enclosure details over existing concrete footpath	-	Nov-22
4.4.1 & 4.4.2	Bollard details updated	E	Jul-25
4.5.1	Bubbler installation details updated	E	Jul-25
C5 Public Domain Lighting			
5.1.3 & 5.1.4	Switch board details updated.	E	Jul-25
5.1.3 & 5.1.7	Plinth height amended and detail of paving around switch boards included	E	Jul-25
5.1.6	Lighting pit arrangement details updated to specify that Class B infill pit lids may be used within the park for non-trafficable park area	E	Nov-22

C: Standard Drawings

Drg. No.	Revision Details	Rev. No	Year
5.1.7	Plinth height amended for granite paving and note added for concrete/ asphalt paving	E	Nov-22
C6 Public Domain Lighting			
6.2.2	Notes updated to specify sign post installation shall be carried out using Loc-socket	E	Jul-25
C7 Stormwater Assets			
7.1.3	Structural details updated	E	Nov-22
7.1.5	Structural details updated	E	Nov-22
7.1.6	Structural details updated	E	Nov-22
7.1.7	Trimmer bars & downpipe connection details updated	E	Nov-22
7.1.10	V-notch shown at the base slab	F	Jul-25
7.1.12	Structural details updated	E	Nov-22
7.1.13	Structural details updated	E	Nov-22
7.1.15			Nov-22
7.1.16	Drawings removed from standard drawings	-	Nov-22
7.1.17			Nov-22
7.1.18	Property connection details updated to include option of RHS	F	Jul-25
7.2.2	Filter media specs removed from drawing and reference provided to tech specs	E	Nov-22
7.2.3	Filter media specs removed from drawing and reference provided to tech specs	E	Nov-22
7.2.4	Filter media specs removed from drawing and reference provided to tech specs	E	Nov-22
7.2.5	Terrace raingarden details updated to timber retaining wall instead of bluestone kerb	E	Nov-22
7.2.6	Retaining wall details updated from bluestone kerb to timber retaining wall	E	Nov-22
7.2.8	Gutter bridge details / dimensions updated	E	Nov-22
7.2.11	Structural details updated, calming basin details updated, specific RL's removed and timber retaining wall and benching details shown	E	Nov-22
7.2.12	Calming basin details updated	E	Nov-22
7.2.12	Galvanised wire mesh removed at inlet	F	Jul-25

